

# SPECIFICATIONS:

A.A.S.H.O. Standard Specifications for Highway Bridges 1961 with Interim Specifications, 1961, 1962, 1963 & 1964.

## CONTRACT:

State of Maine, State Highway Commission Standard Specifications for Highways and Bridges, Revision of January 1956 and Supplemental Specifications of February 1960.

## LIVE LOADING

H-20-44 (Modified for Interstate)

## ALLOWABLE STRESSES

Concrete (n=10) ~ f<sub>c</sub> = 1200 p.s.i.  
Reinforcing Steel, Int. Grade ~ f<sub>s</sub> = 20,000 p.s.i.  
Structural Steel ~ f<sub>s</sub> = 20,000 p.s.i. (A.S.T.M. Designation A-36).

## CONCRETE CLASSIFICATION

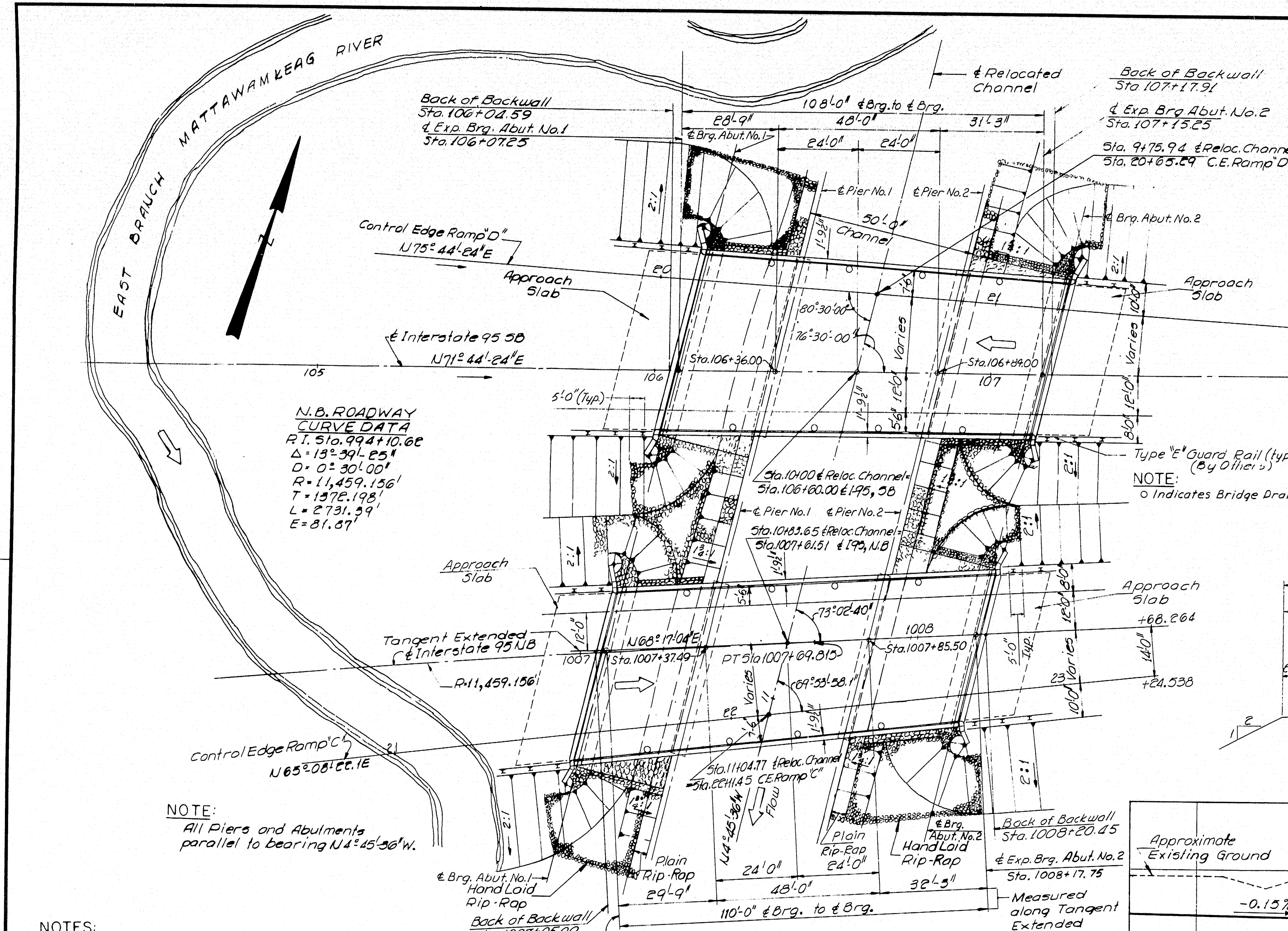
All Concrete shall be Class "A".

## FOUNDATIONS

Abutments 10BP42 End Bearing Piles (Capacity 37 Tons)  
Piers 10BP42 End Bearing Piles (Capacity 37 Tons)

## HYDRAULIC DATA

A = 48.1 Sq. Miles  
S = 22.85 Ft./Mile  
Q<sub>50</sub> = 3435 c.f.s.

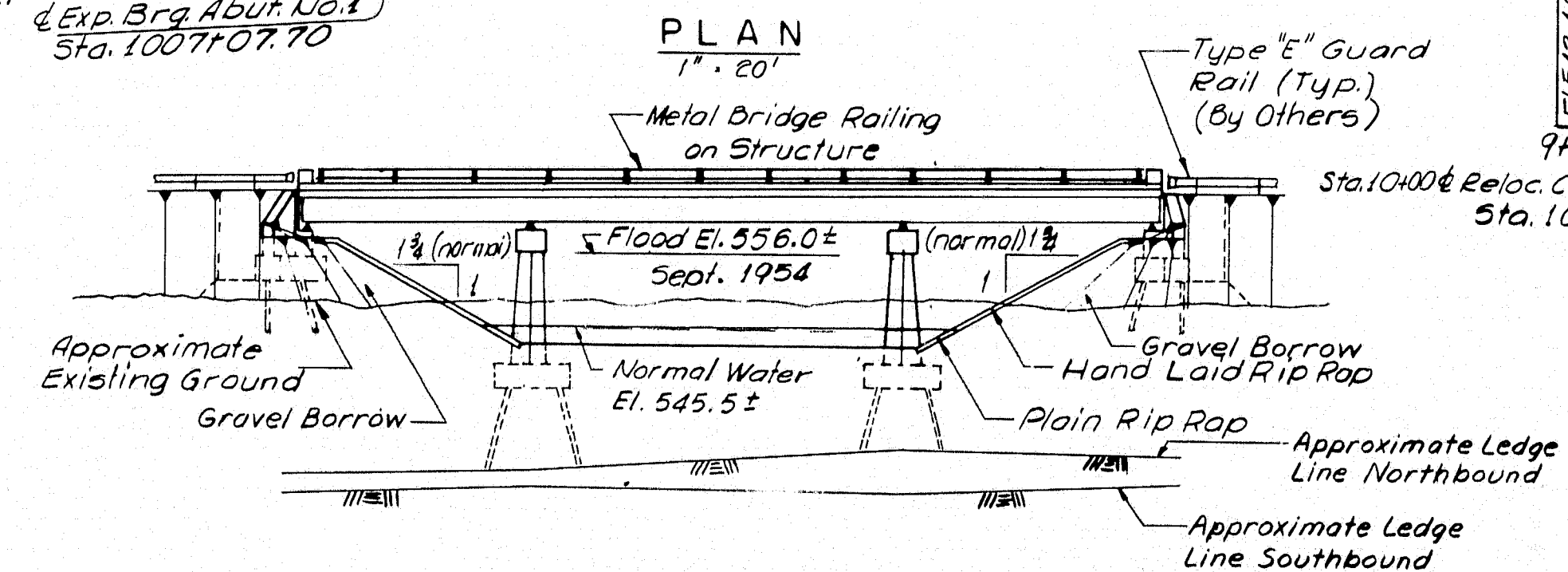


NOTE:  
All Piers and Abutments parallel to bearing N4°45'36"W.

- NOTES:
- All fill within the limits shown on the profile sheet shall be placed by the controlled density method.
  - Size of stone in gravel borrow through which abutment piles are driven should not exceed 6 inches and concentrations of stones in the area shall be avoided.
  - Place gravel borrow to elevation of abutment footings before driving piles.

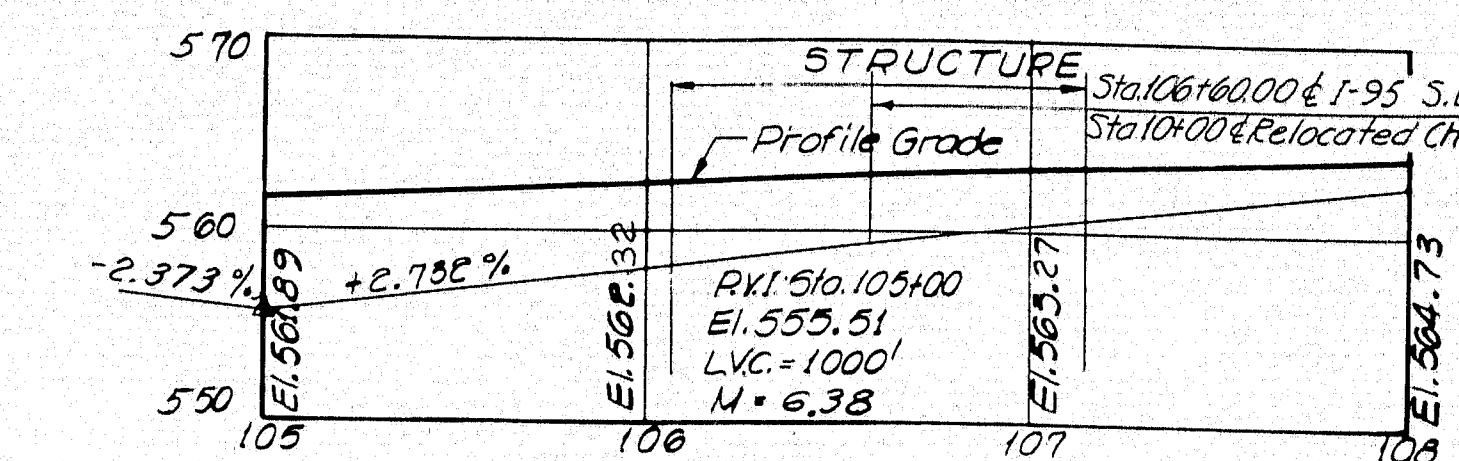
### INDEX OF SHEETS

- GENERAL PLAN & QUANTITIES
- FOUNDATION SURVEY, SB.
- FOUNDATION SURVEY, NB.
- ABUTMENT NO. 1 SB.
- ABUTMENT NO. 2 SB. & APPROACH SLAB
- ABUTMENT NO. 1 NB. & APPROACH SLAB
- ABUTMENT NO. 2 NB.
- PIERS SB.
- PIERS NB.
- STRUCTURAL STEEL & BLOCKING SB.
- STRUCTURAL STEEL & BLOCKING NB.
- SUPERSTRUCTURE SB.
- SUPERSTRUCTURE NB.
- SLOPE PROTECTION
- REINFORCING STEEL



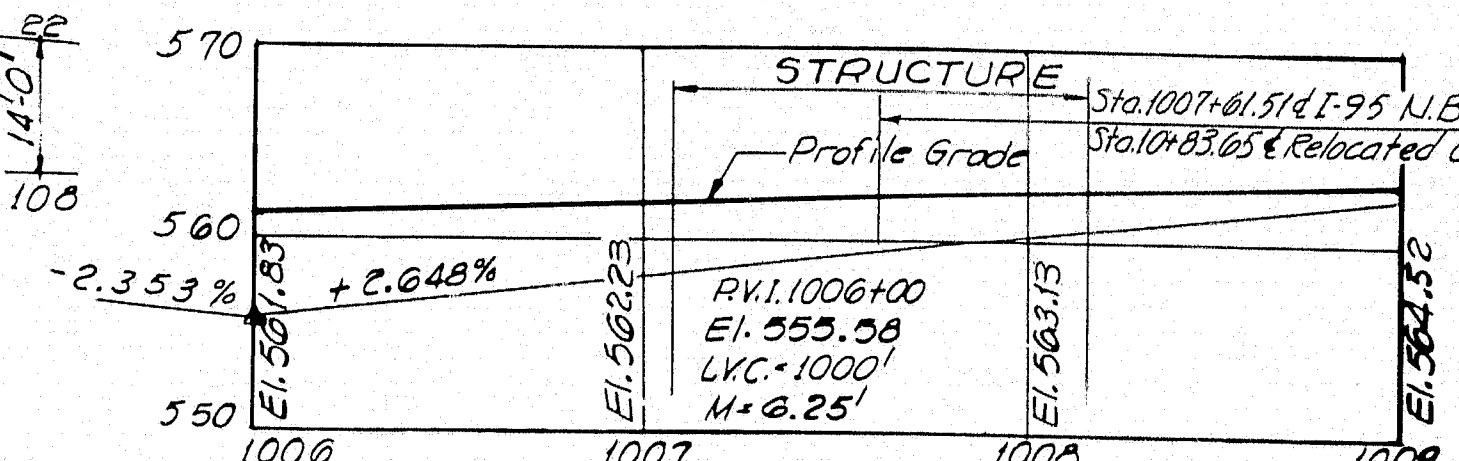
### ELEVATION

STANDARD DETAIL SHEETS	
BD101-64	BEARING PEDESTALS
BD103-64	BEAM SPLICES
BD104-64	DIAPHRAGMS, ARMORED JOINT
	SHEAR CONNECTORS, DRAIN
BD107-64	STEEL RAIL
BD108-64	ALUMINUM RAIL



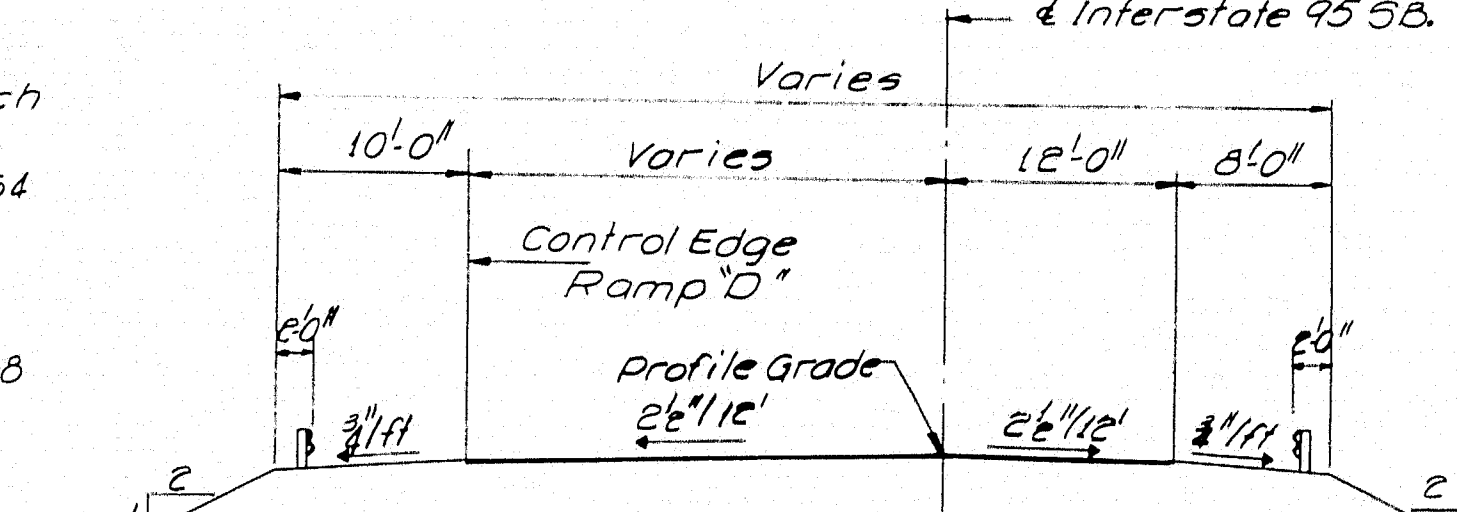
### PROFILE - INTERSTATE 95 S.B.

Hor: 1" = 50'  
Vert: 1" = 10'



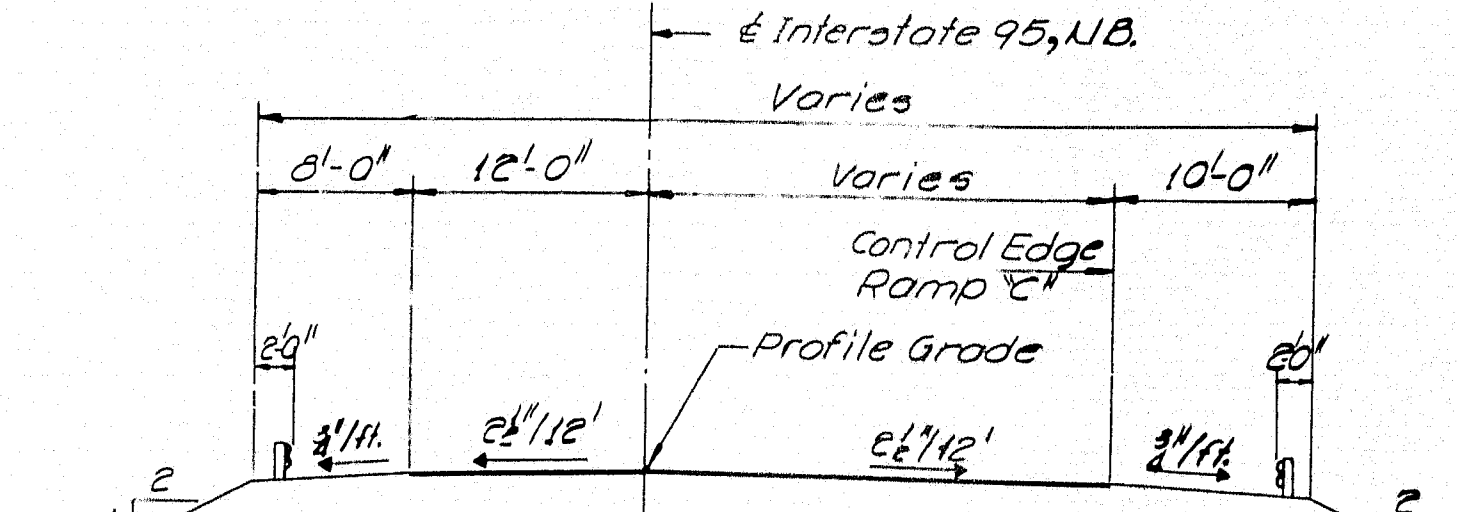
### PROFILE - INTERSTATE 95 N.B.

Hor: 1" = 50'  
Vert: 1" = 10'



### SECTION - INTERSTATE 95 SB

Hor: 1" = 50'  
Vert: 1" = 10'



### SECTION - INTERSTATE 95 NB.

ESTIMATE OF QUANTITIES			
ITEMS	DESCRIPTION	UNIT	SB Quant. NB Quant.
204-14	Structural Earth Excavation, Piers	Cu. Yds.	660 650
205-12	Gravel Borrow (I.P.M.)	Cu. Yds.	795 710
701-33	Port. Cem. Conc., Abuts. & Ret. Walls	Cu. Yds.	220 212
701-35.2	Port. Cem. Conc., Piers (Mattawamkeag River Bridge)	Cu. Yds.	339 325
701-40	Port. Cem. Conc., Rdwy. & Sawk. Slabs on Steel Bridge	Cu. Yds.	145 147
701-54	Port. Cem. for Riprap Grout	Bbls.	9 9
702-103.2	Structural Steel, Fab. & Del. (Mattawamkeag River Bridge)	L.S.	L.S. L.S.
702-104.2	Structural Steel, Erection (Mattawamkeag River Bridge)	L.S.	L.S. L.S.
702-105.2	Structural Steel, Field Painting (Mattawamkeag River Bridge)	L.S.	L.S. L.S.
705-13	Reinforcing Steel, Delivered	Lbs.	80,700 78,100
705-14	Reinforcing Steel, Placing	Lbs.	80,700 78,100
705-16	Steel H-Beam Piles, 40 lbs./ft.	Lin. Ft.	1,987 1,889
803-7	Cofferdam, Pier No. 1 SB. (Mattawamkeag River Bridge)	L.S.	L.S. —
803-8	Cofferdam, Pier No. 2 SB. (Mattawamkeag River Bridge)	L.S.	L.S. —
803-9	Cofferdam, Pier No. 1 NB. (Mattawamkeag River Bridge)	L.S.	L.S. —
803-10	Cofferdam, Pier No. 2 NB. (Mattawamkeag River Bridge)	L.S.	L.S. —
803-8	Bridge Rail	Lin. Ft.	210 213
807-11	Epoxy Resin Surface Sealant	Sq. Yds.	132 130
901-24	Vertical Bridge Curb - Type I	Lin. Ft.	218 227
901-25	Vertical Bridge Curb - Circular - Type I	Lin. Ft.	12 12
907-9	Plain Riprap	Cu. Yds.	55 60
907-10	Hand Laid Riprap	Cu. Yds.	280 285

Estimated Weight of Structural Steel including drains is 112,400 lbs. S.B. and 113,700 lbs. N.B. Total 226,100 lbs.

## NOTE:

Curing Box for Oakfield-Smyrna Road Bridges shall be used for these bridges.

DESIGN - IS DETAIL D.A.T.  
TRACE - PRN  
CHECK - PRN

BRIDGE NO. SURVEY - PLOT

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

INTERSTATE 95  
OVER

RELOCATED EAST BRANCH  
MATTAWAMKEAG RIVER

IN THE TOWN OF  
OAKFIELD

AROOSTOOK COUNTY

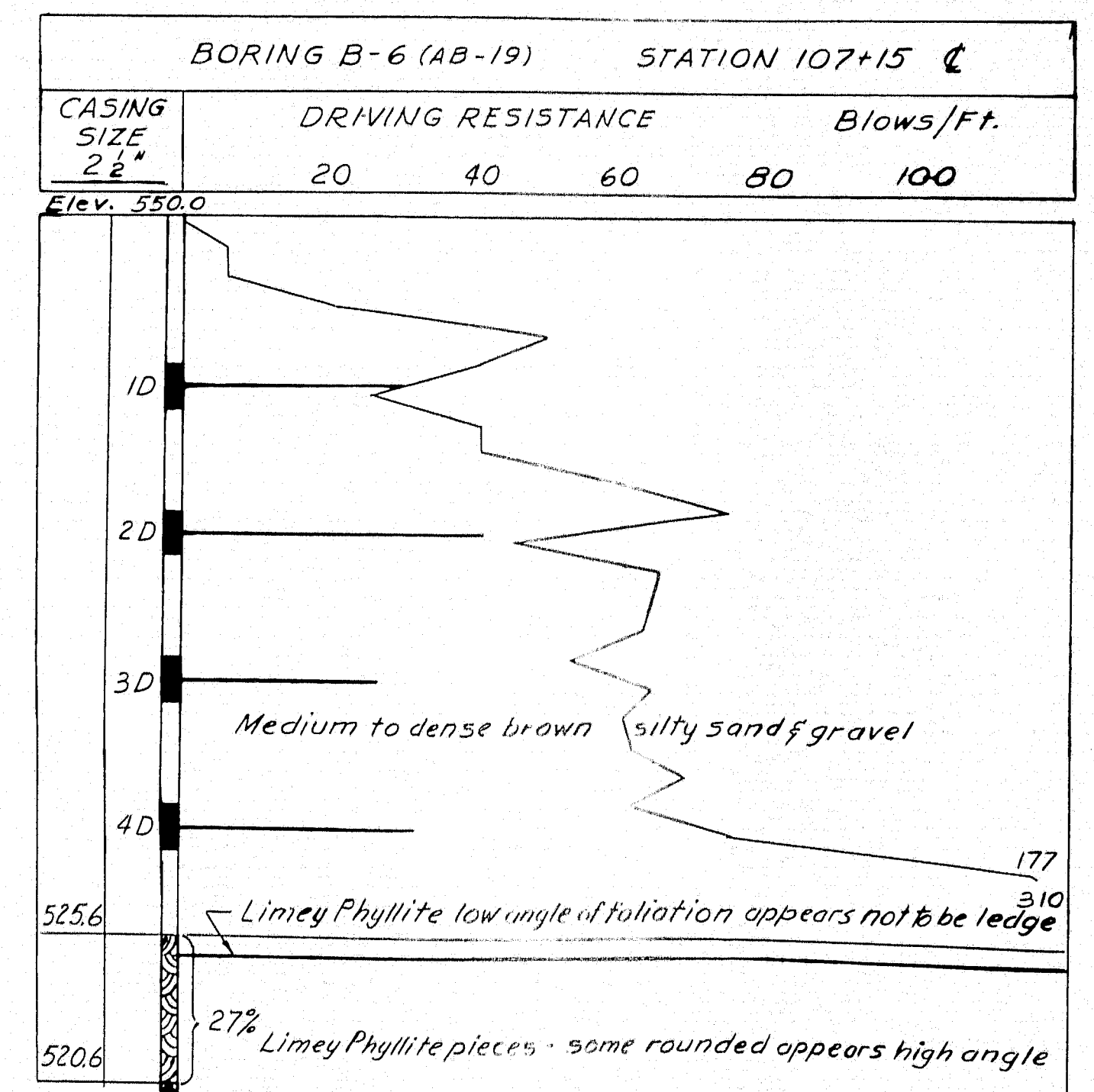
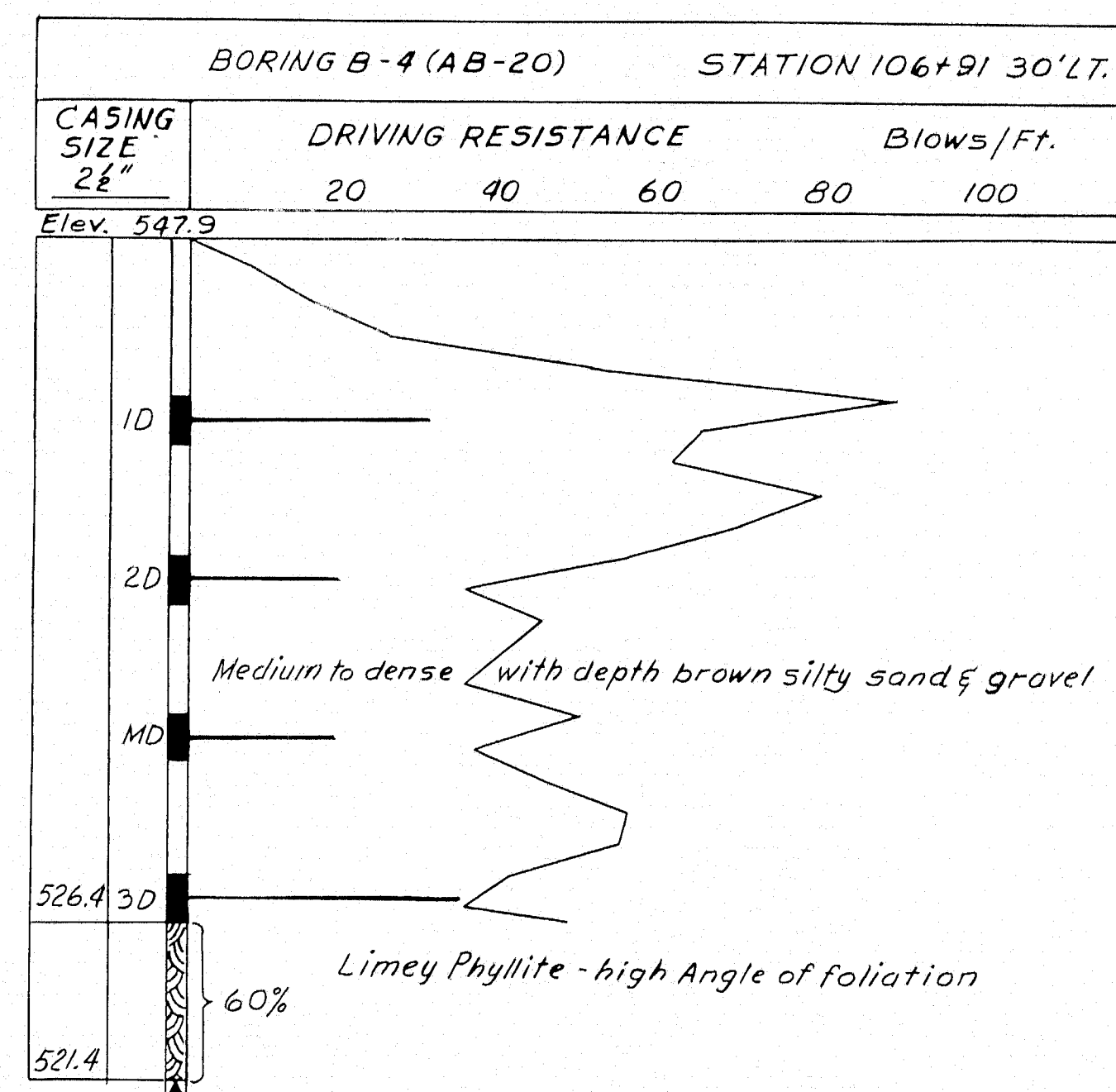
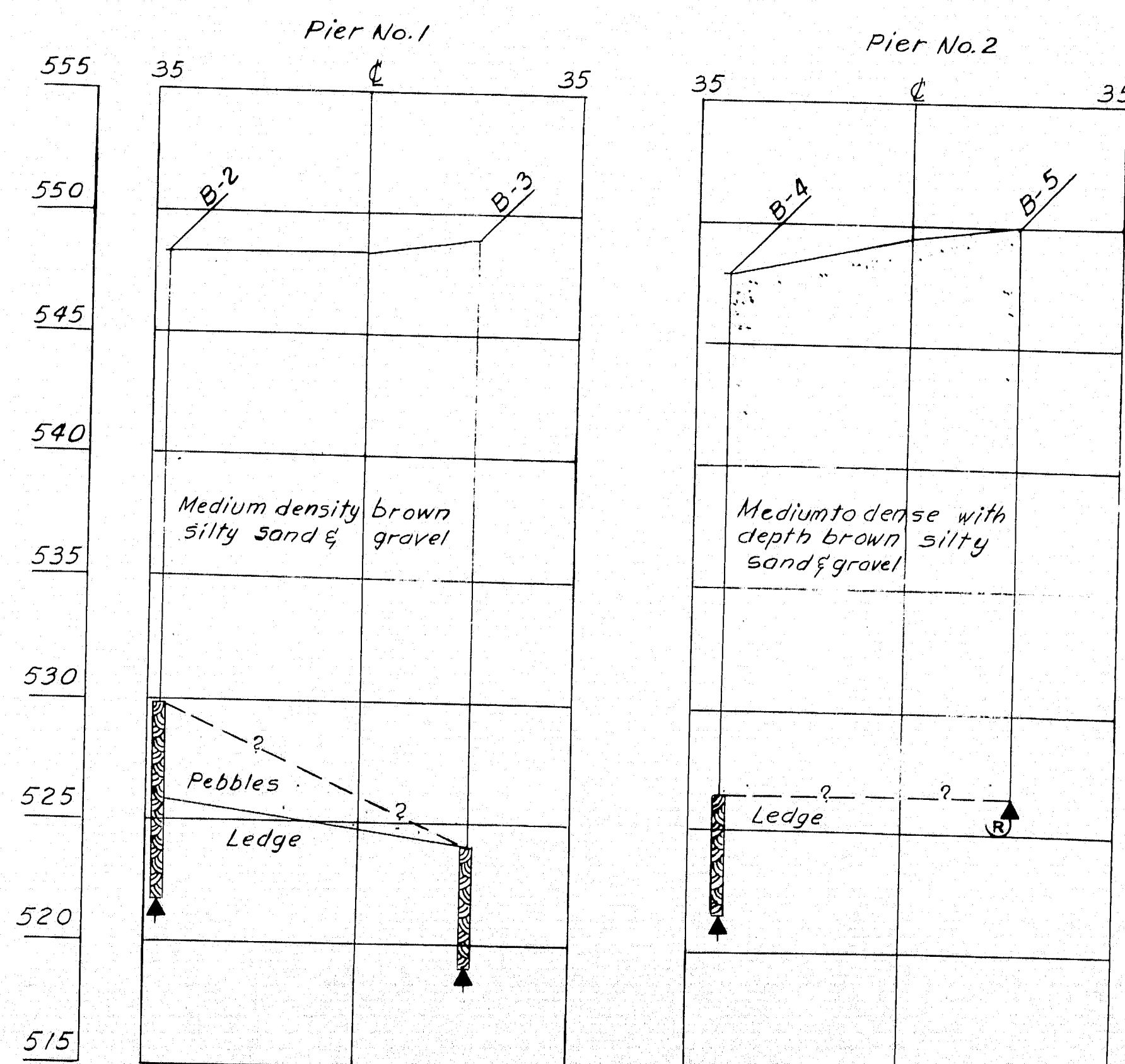
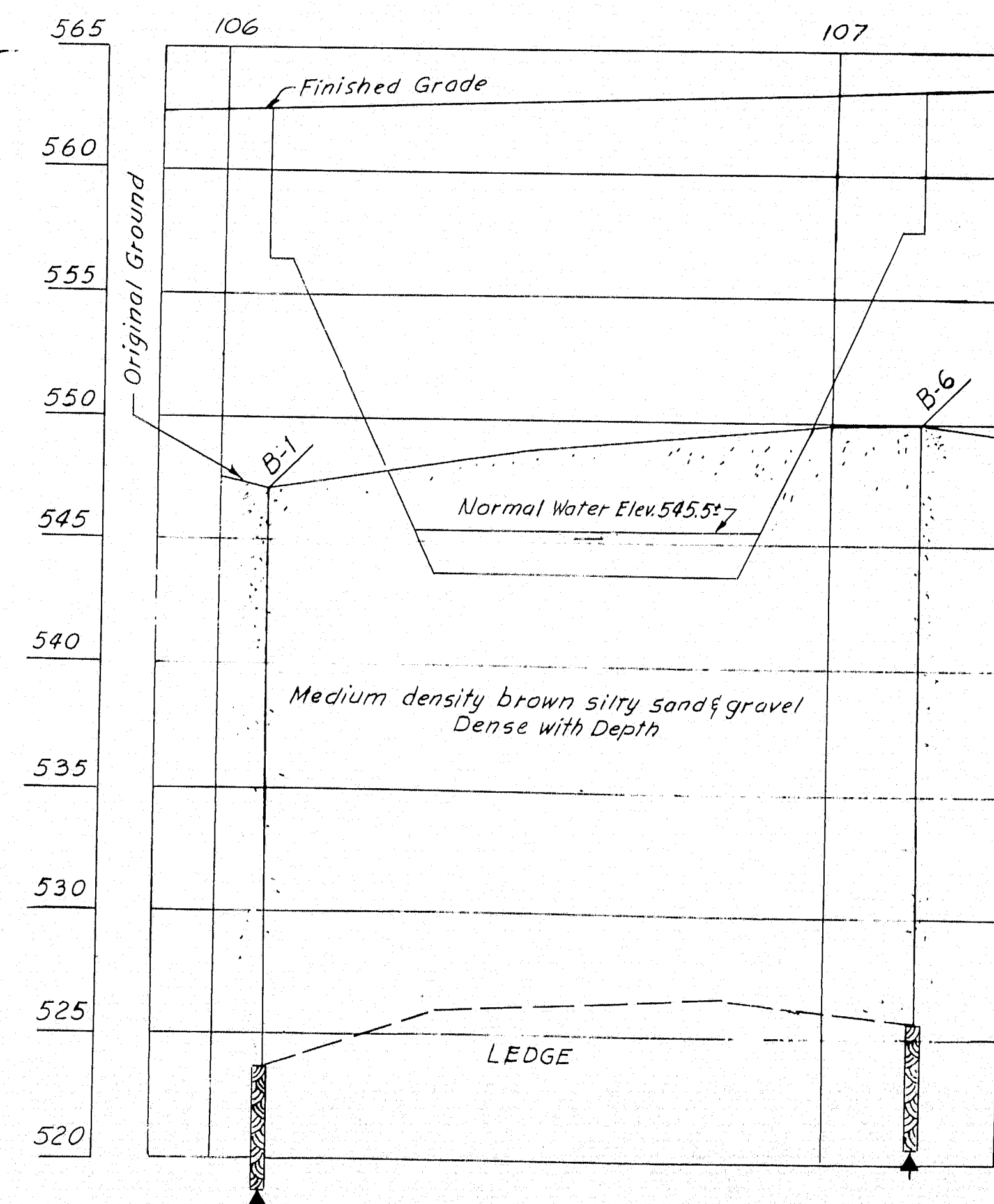
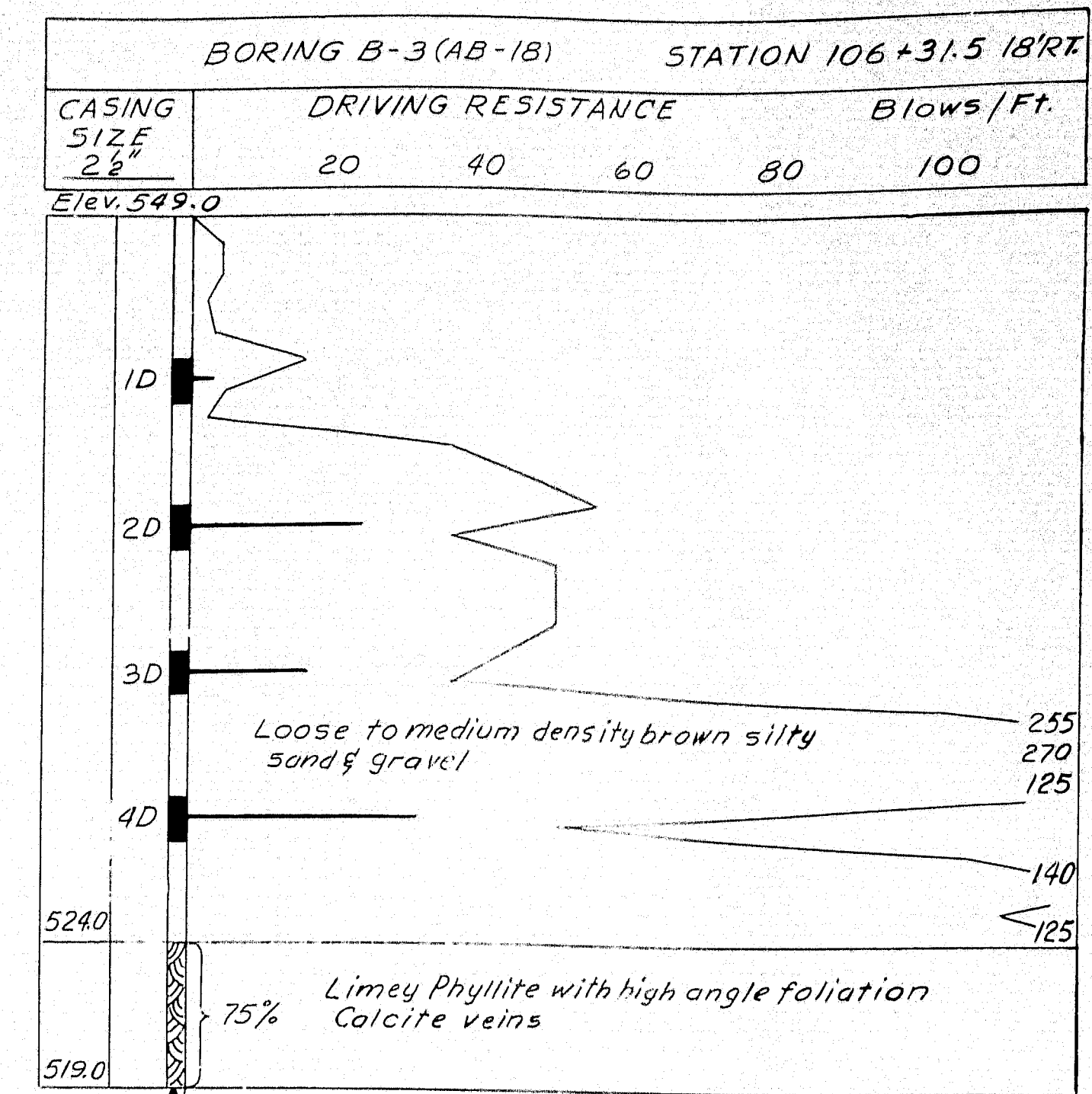
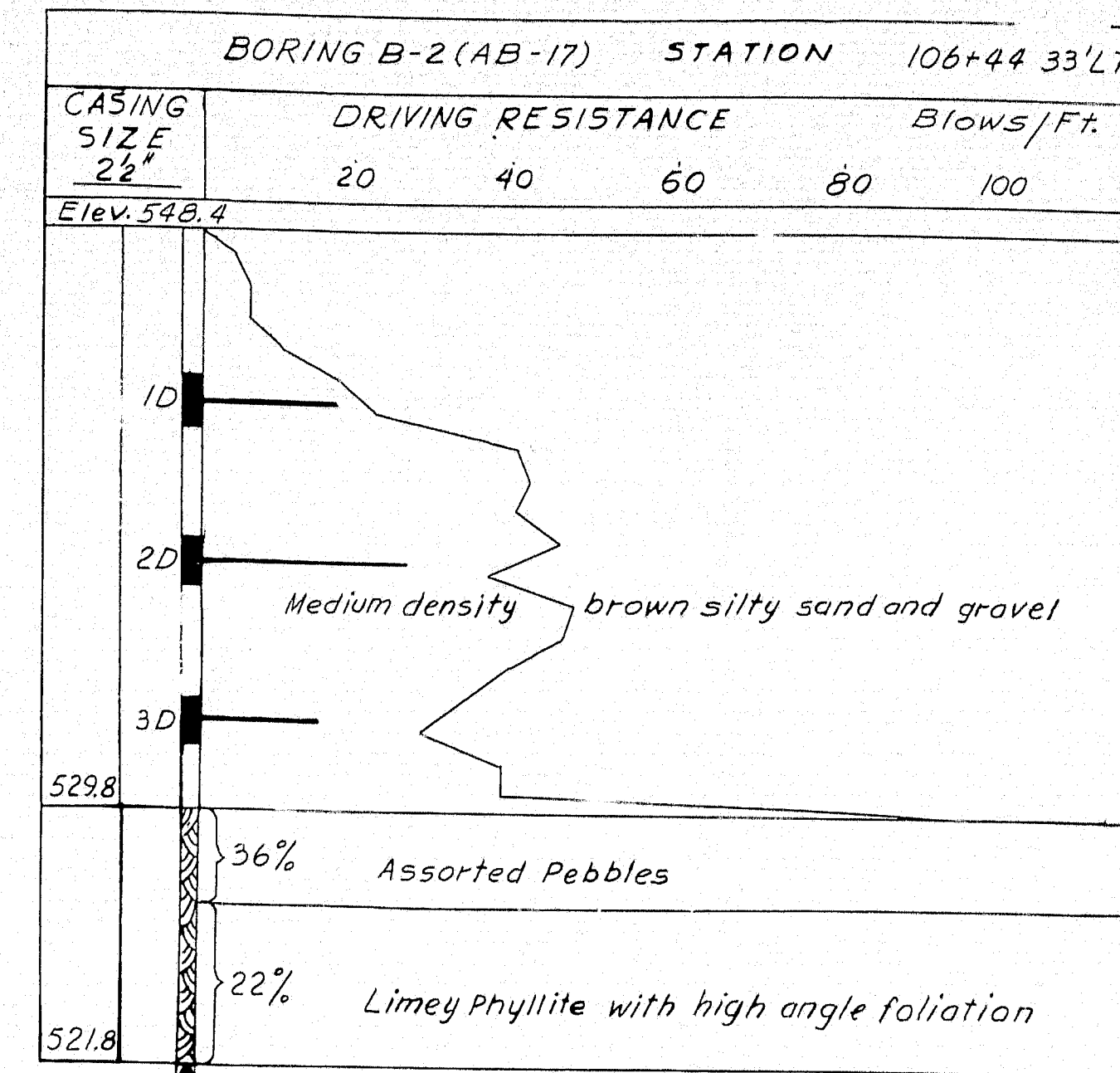
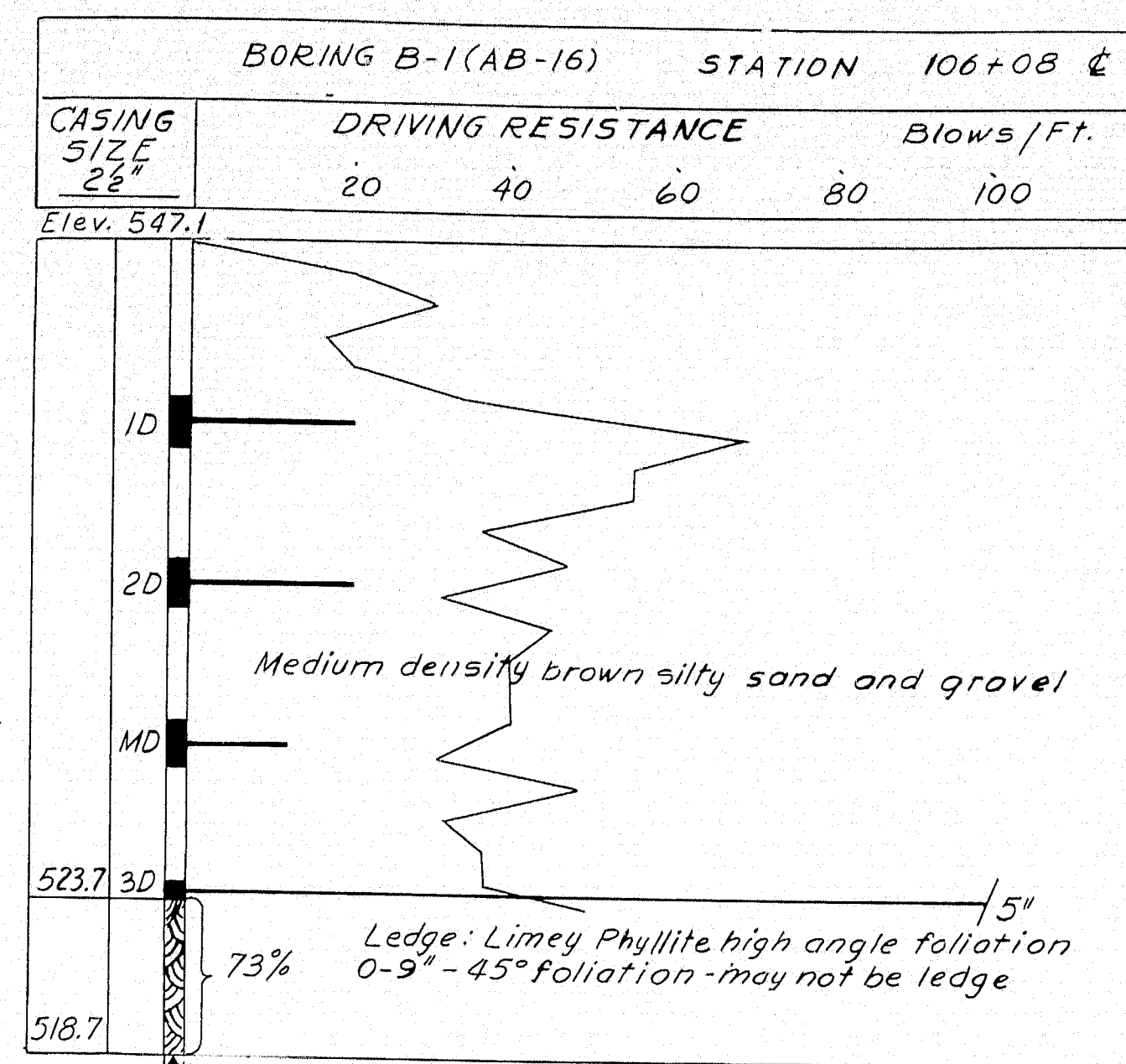
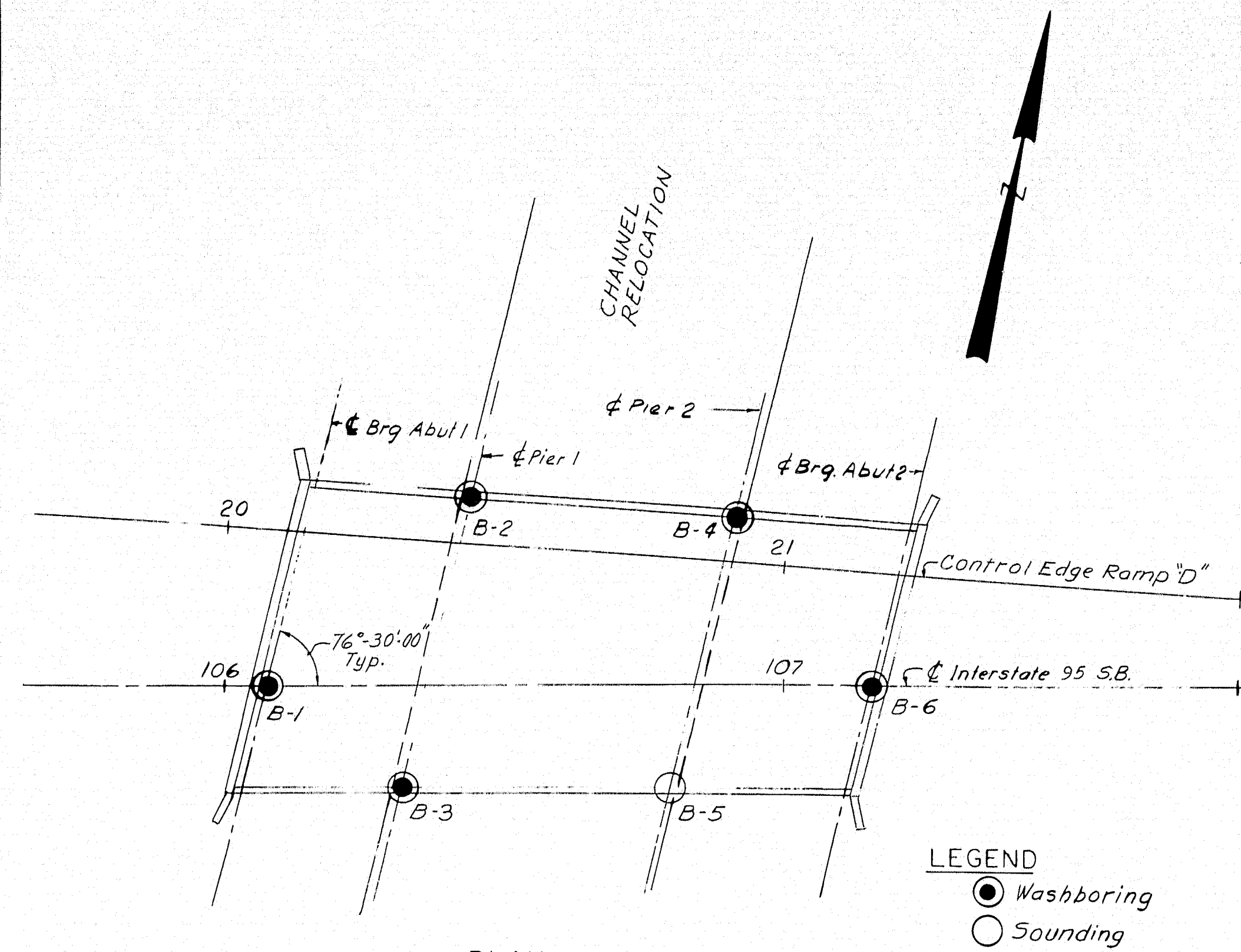
GENERAL PLAN AND QUANTITIES

SHEET 1 OF 16 AUGUSTA, MAINE FEBRUARY 1965

M-2255 DYER BROOK OAKFIELD (12)

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
NEW YORK BOSTON KANSAS CITY





- NOTES:
- Number of blows required to drive extra heavy casing one foot with 400 ft. lbs of energy per blow.
  - Location of sample, or sample attempt.
  - 5.8H Sampler #1290's unsuccessful sample attempt and type of sampler
  - Number of blows required to drive spoon or tubing one foot with 350 ft. lbs. of energy per blow.
  - Bottom of boring (may not be bottom of soil strata)
  - Refusal of drill rods or casing (may not be ledge)
  - 7 1/2" Locations cored by diamond bit and per cent recovery of rock.

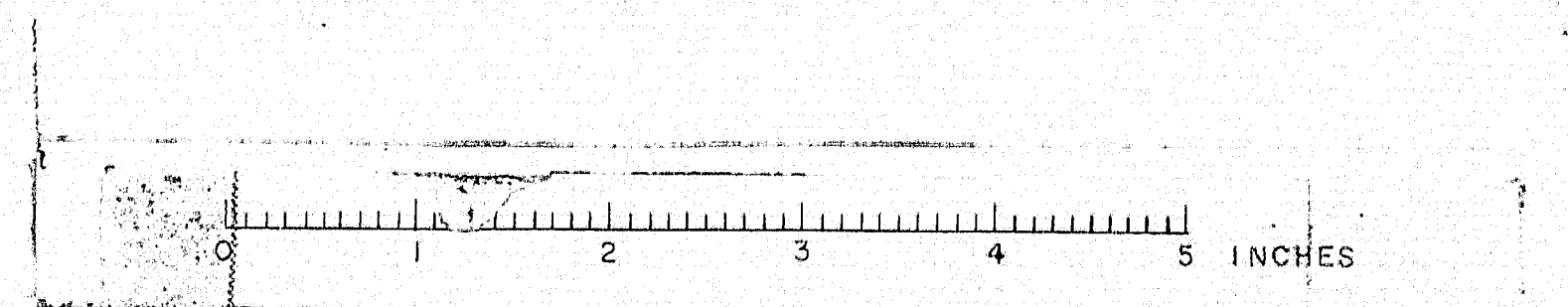
HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
NEW YORK BOSTON KANSAS CITY

DESIGN - TRACE - CHECK - V.A.V. DETAIL - R.F. BRIDGE NO. SURVEY - PLOT -

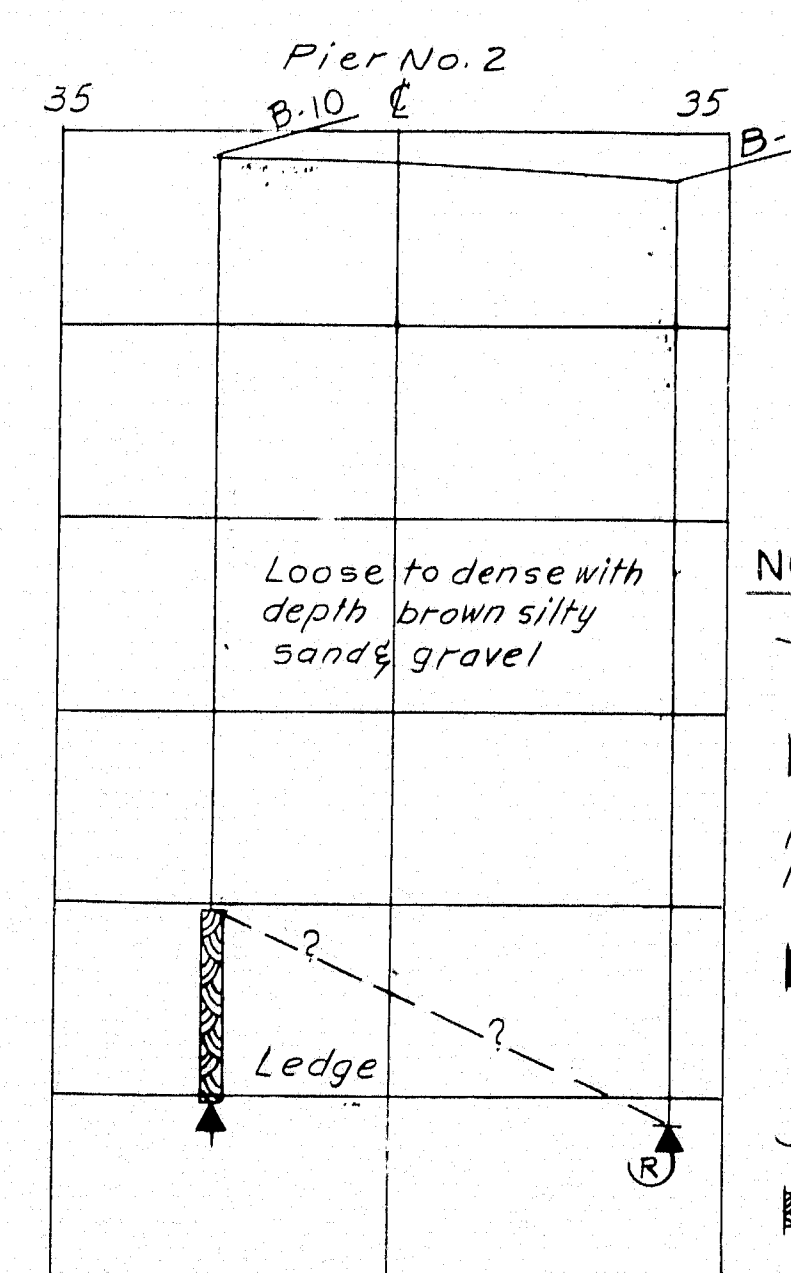
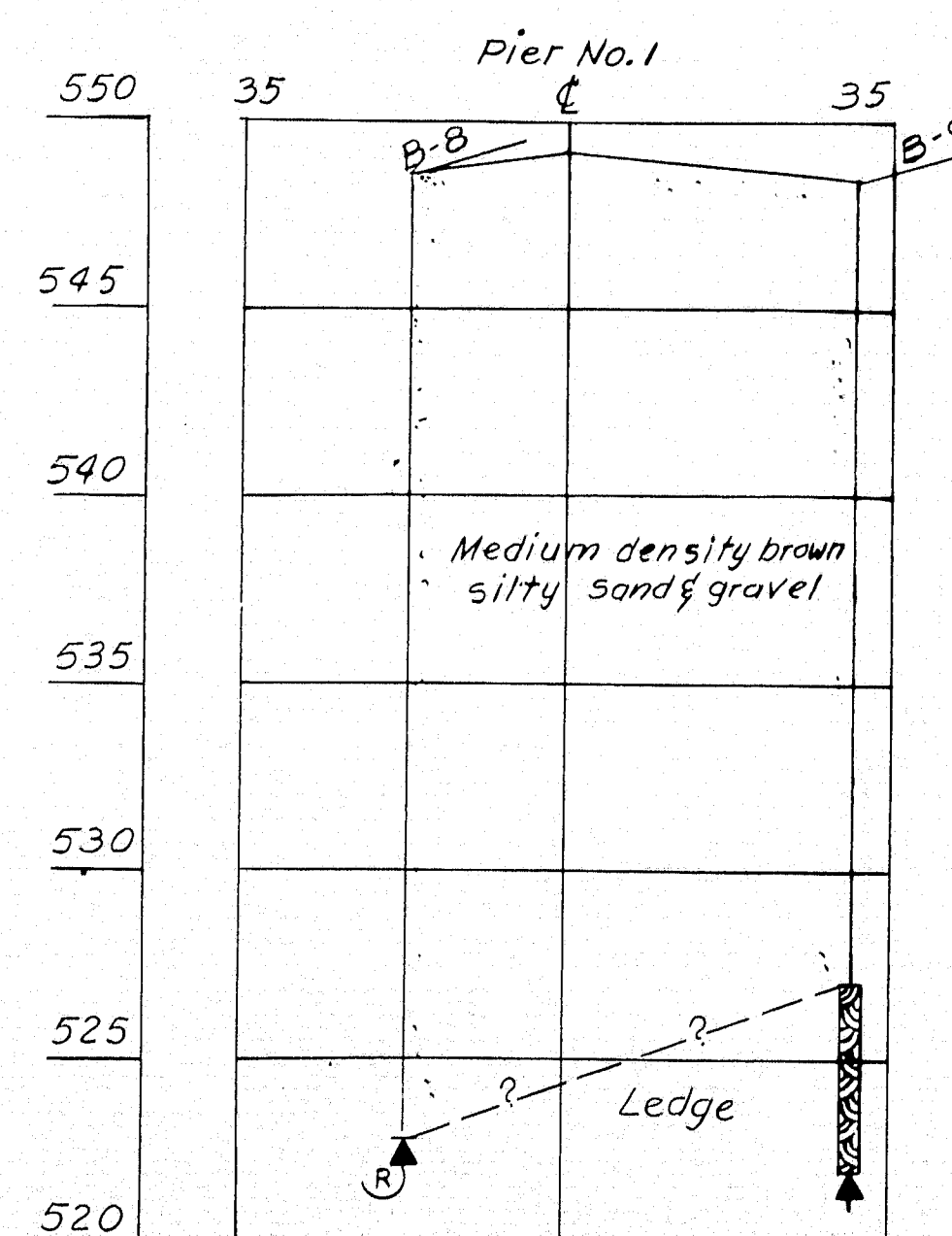
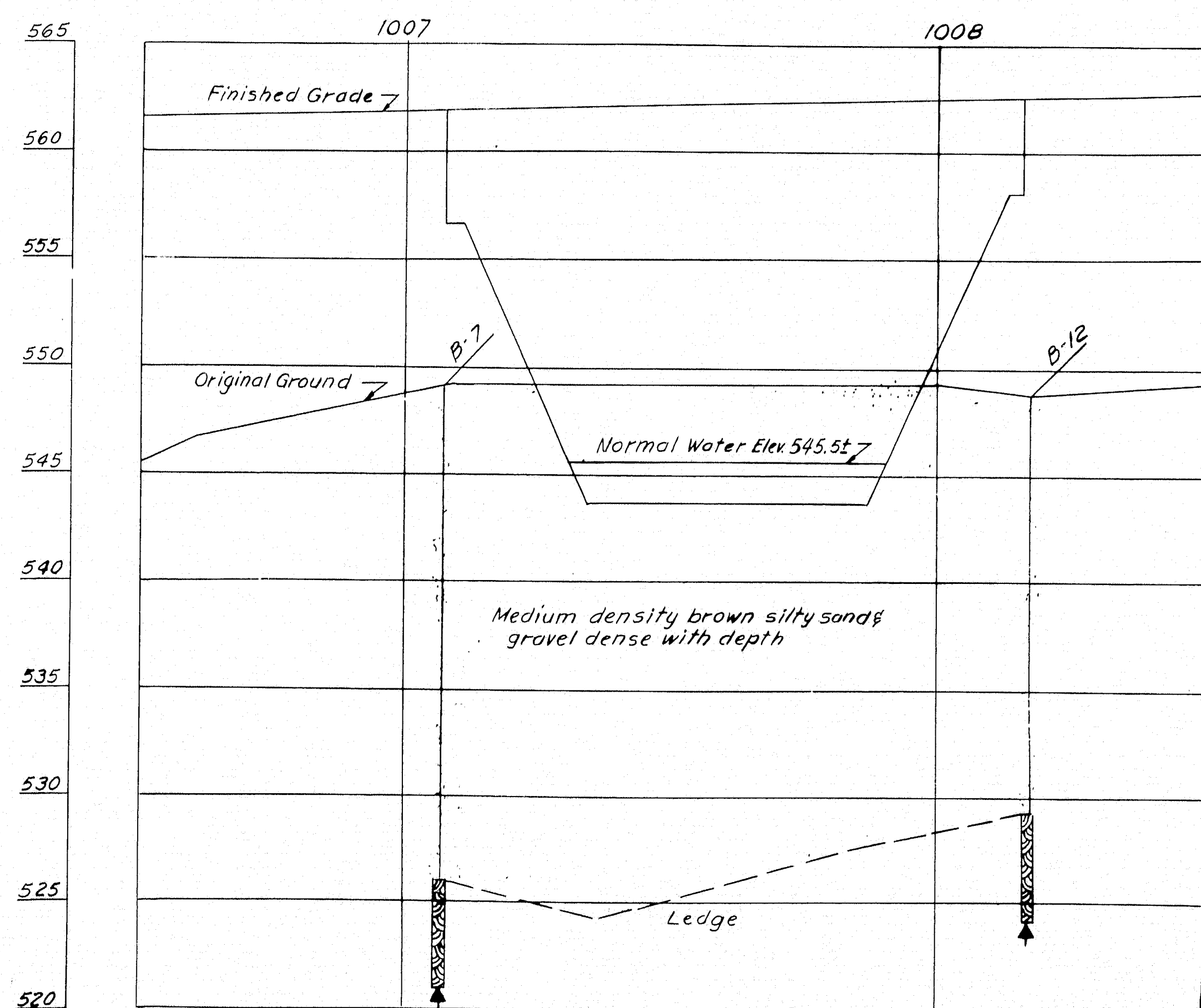
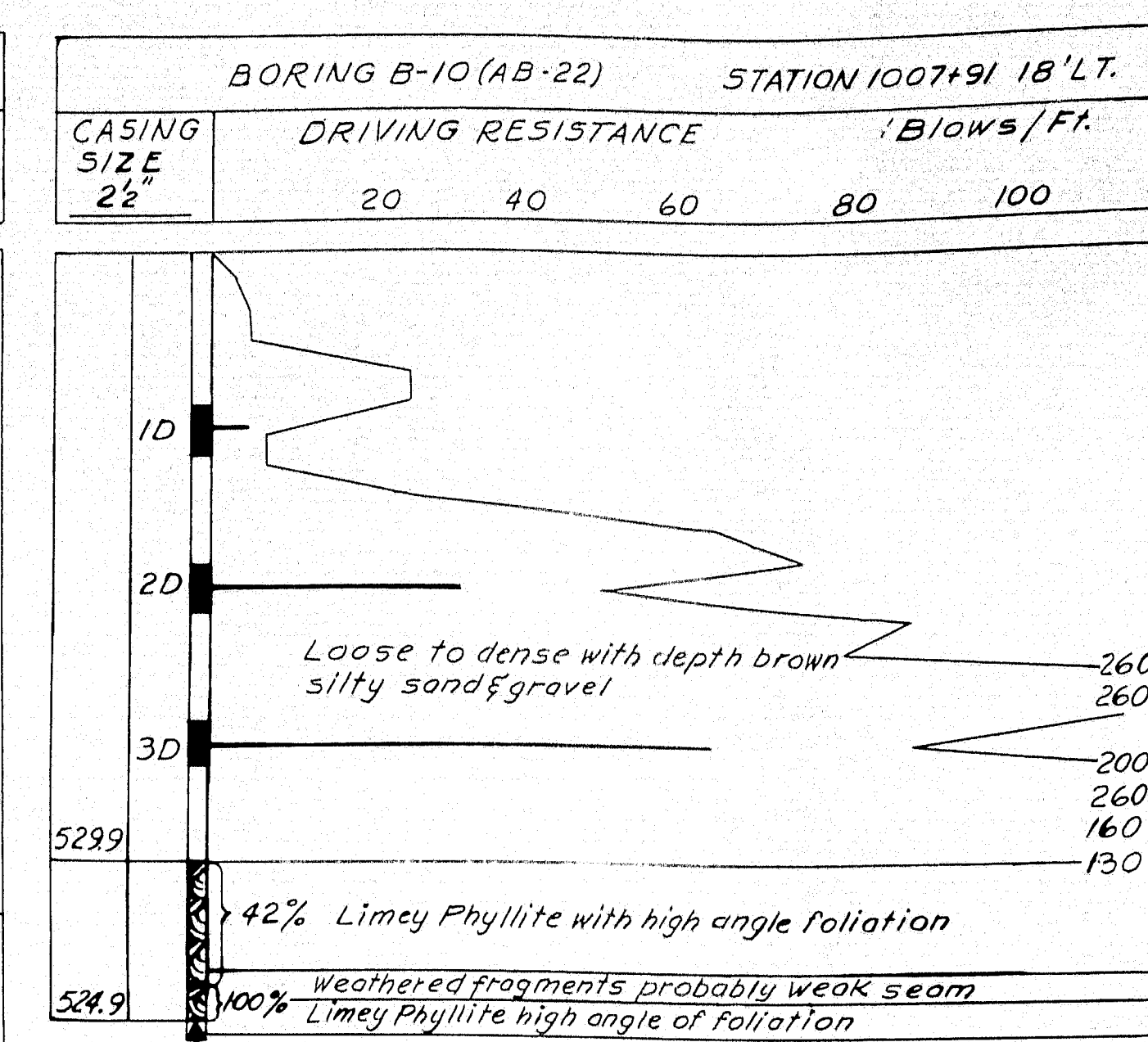
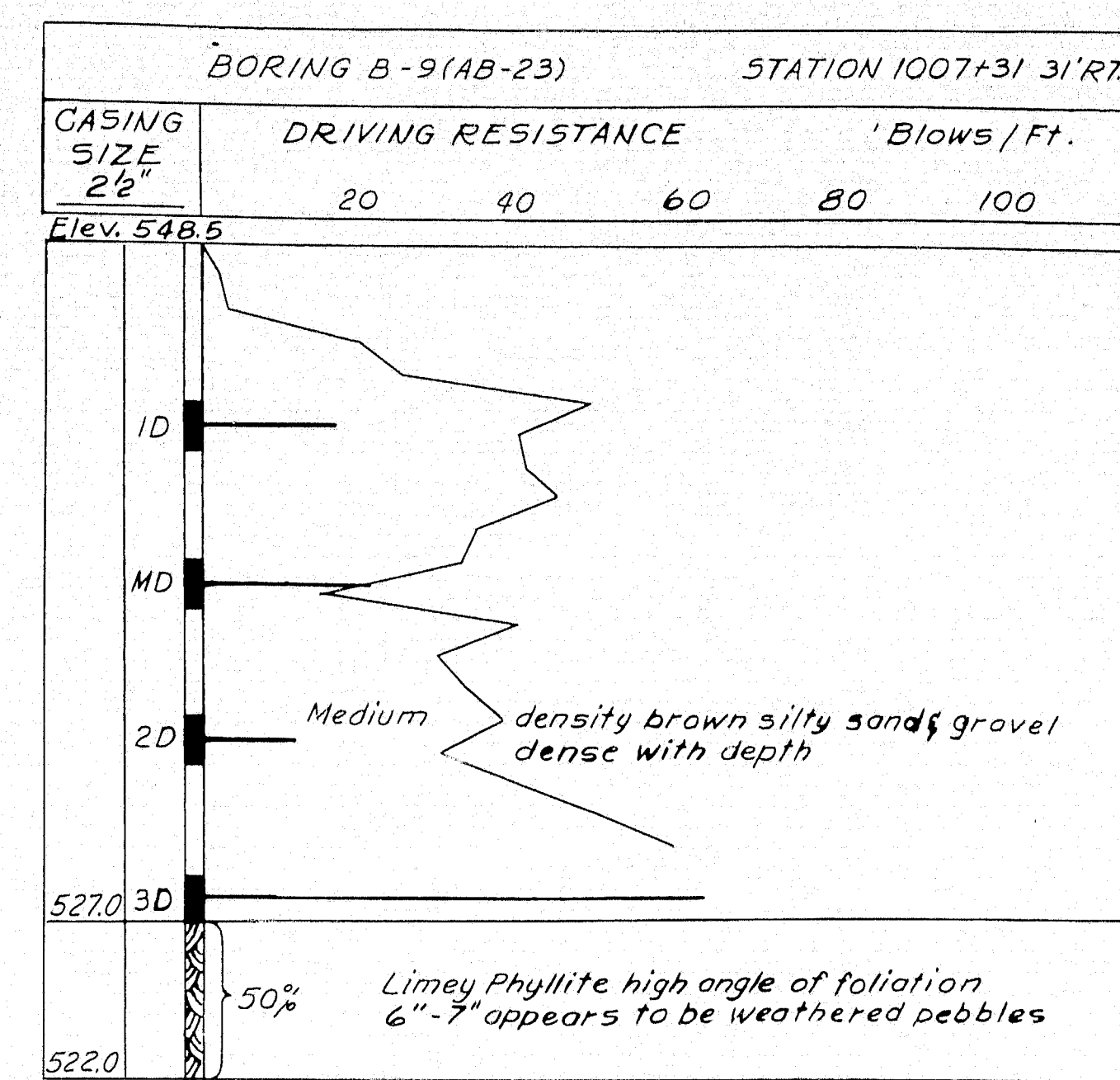
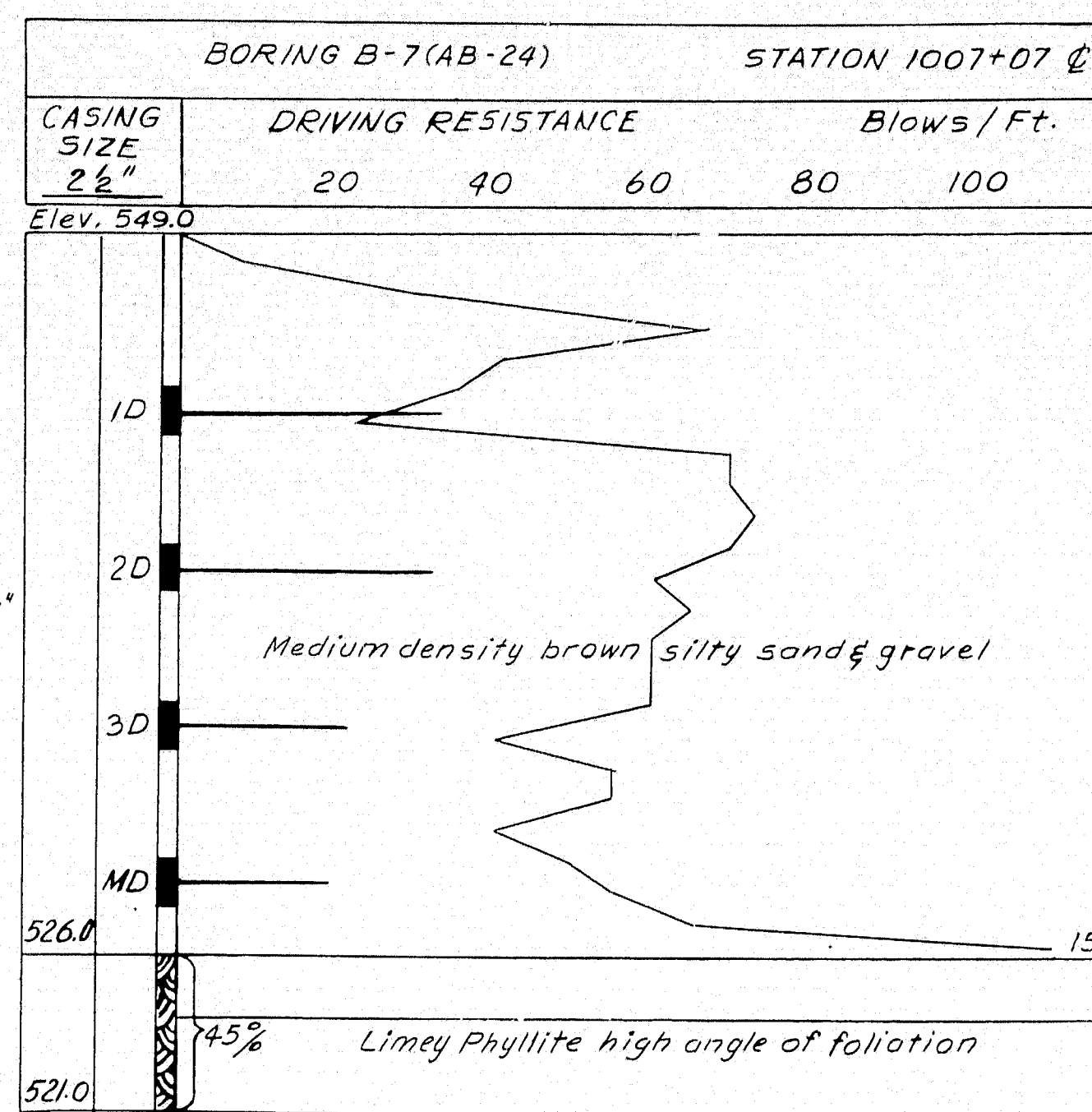
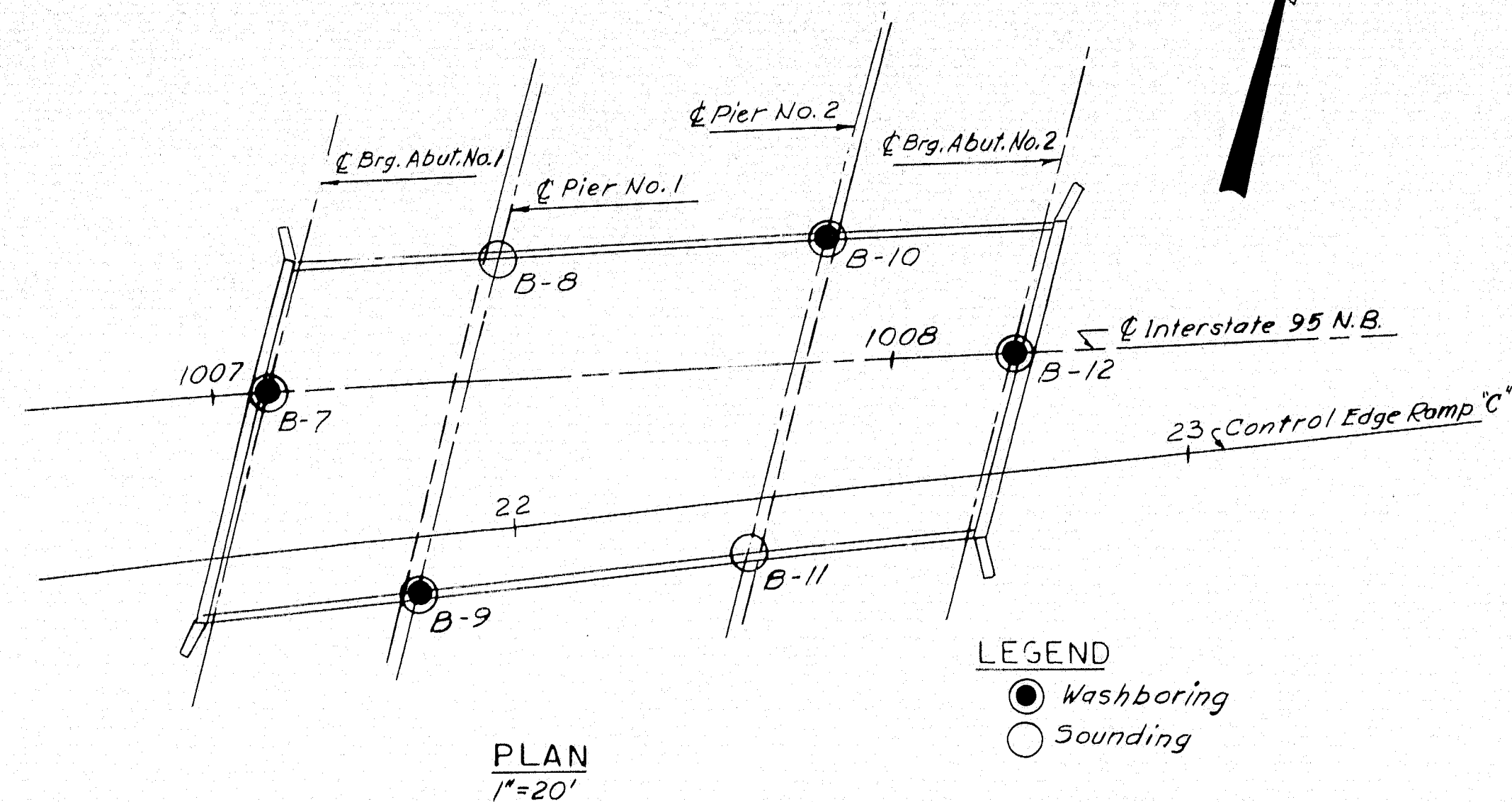
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

INTERSTATE 95 S.B.  
OVER  
RELOCATED EAST BRANCH  
MATTAWAMKEAG RIVER  
IN THE TOWN OF  
OAKFIELD  
ARROOSTOOK COUNTY  
FOUNDATION SURVEY

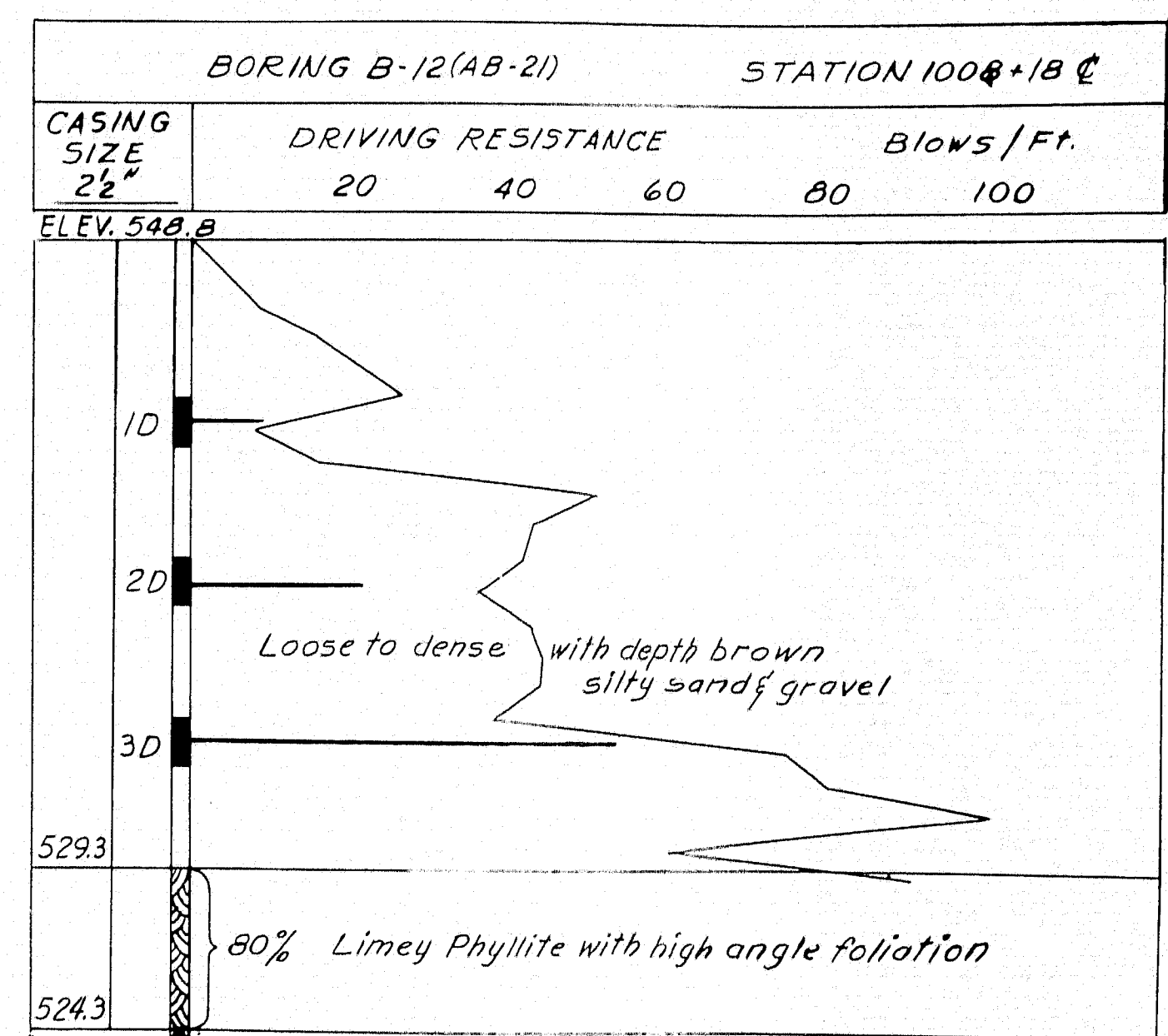
SHEET 2 OF 16 AUGUSTA, MAINE FEBRUARY 1965  
DYER BROOK OAKFIELD (12)







- NOTES:
- Number of blows required to drive extra heavy casing one foot with 400 ft. lbs. of energy per blow.
  - Location of sample or sample attempt.
  - 10 S.H. Sampler #1290's
  - MD unsuccessful sample attempt and type of sampler
  - Number of blows required to drive spoon or tubing one foot with 350 ft. lbs. of energy per blow.
  - Bottom of boring (may not be bottom of soil strata)
  - Refusal of drill rods or casing (may not be ledge)
  - 7% Locations cored by diamond bit and per cent recovery of rock.



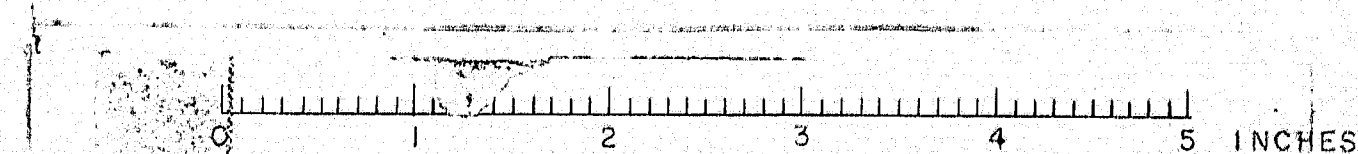
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CONSULTING ENGINEERS  
NEW YORK BOSTON KANSAS CITY

DESIGN-TRACE-CHECK-V.A.V. DETAIL-R.F. BRIDGE NO. SURVEY-PLOT

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
INTERSTATE 95 NB  
OVER  
RELOCATED EAST BRANCH  
MATTAWAMKEAG RIVER  
IN THE TOWN OF  
OAKFIELD  
ARROOSTOOK COUNTY  
FOUNDATION SURVEY

SHEET 3 OF 16 AUGUSTA, MAINE FEBRUARY 1965

M-2257 DYER BROOK OAKFIELD (12)

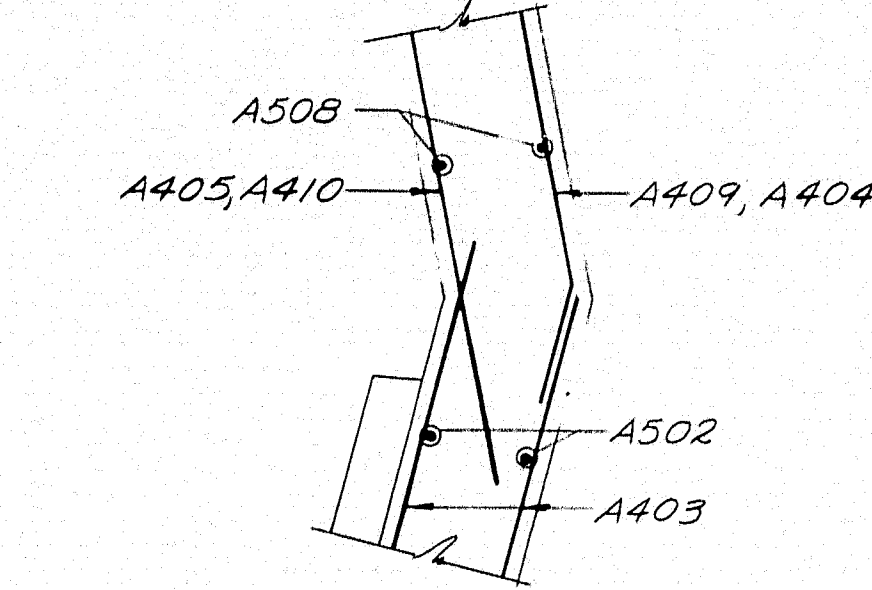




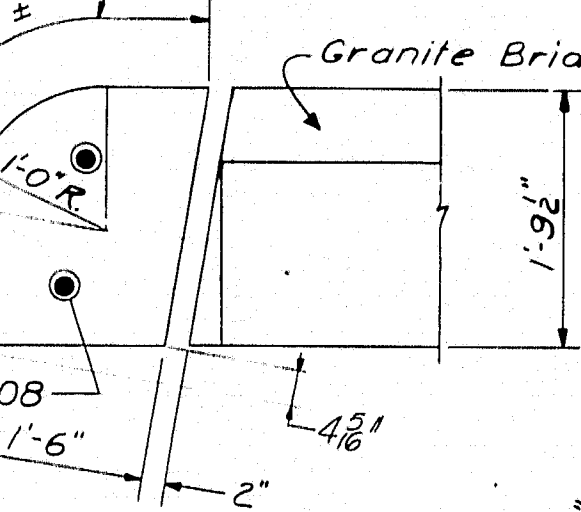
**NOTE:**

Cover the vertical construction joints on the backside with 2 layers of heavy roofing 10" wide. Bond the layers together and to the concrete with a suitable grade of roofing cement. Recess the vertical areas to be covered 1/4". Paint vertical construction joints with a suitable grade of asphalt paint to break bond.

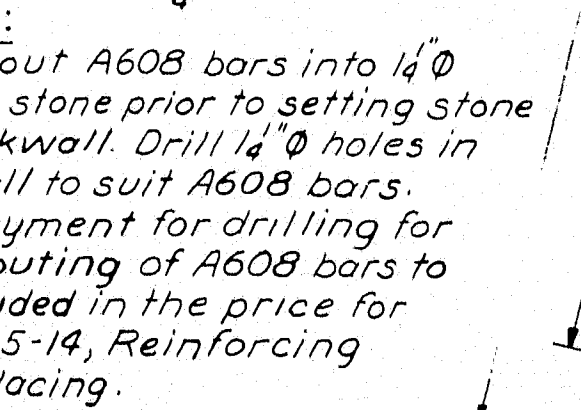
**SECTION D-D**  
2" = 1'-0"



**DETAIL "A"**  
3" = 1'-0"



**DETAIL "B"**  
3" = 1'-0"



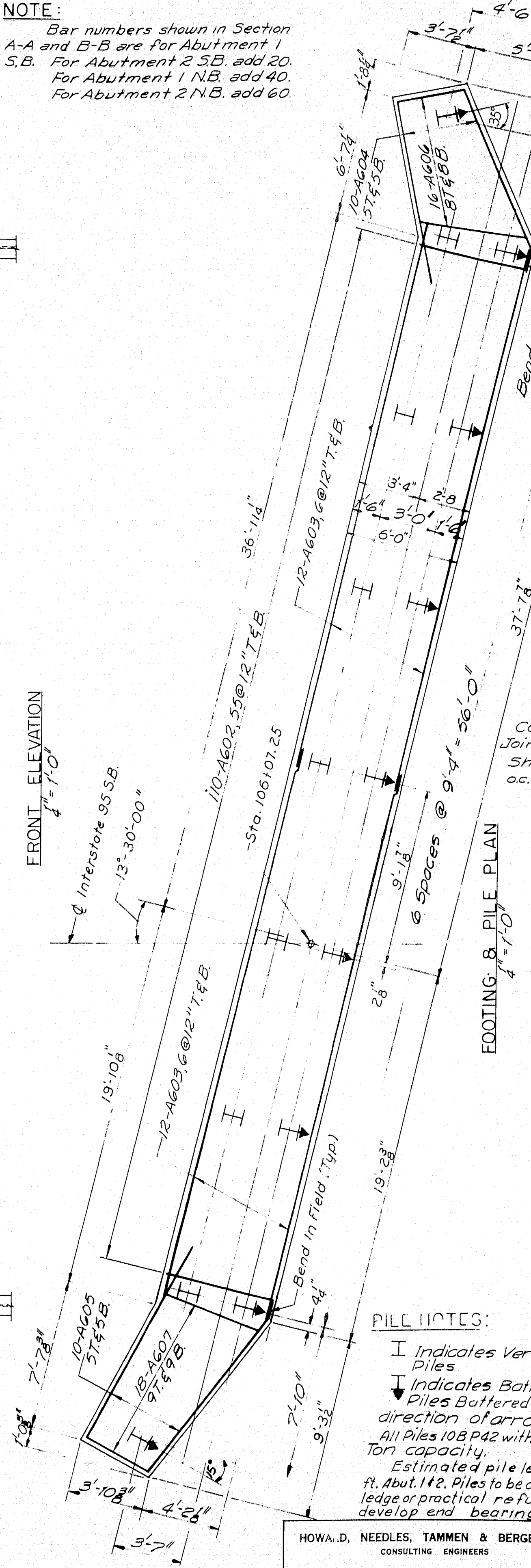
**NOTES:**

1. Grout A608 bars into 14" holes in stone prior to setting stone on backwall. Drill 14" holes in backwall to suit A608 bars.
2. Payment for drilling for and grouting of A608 bars to be included in the price for Item 705-14, Reinforcing Steel, Placing.
3. Granite blocks shall be placed in position after or at the same time as curb on bridge is positioned.

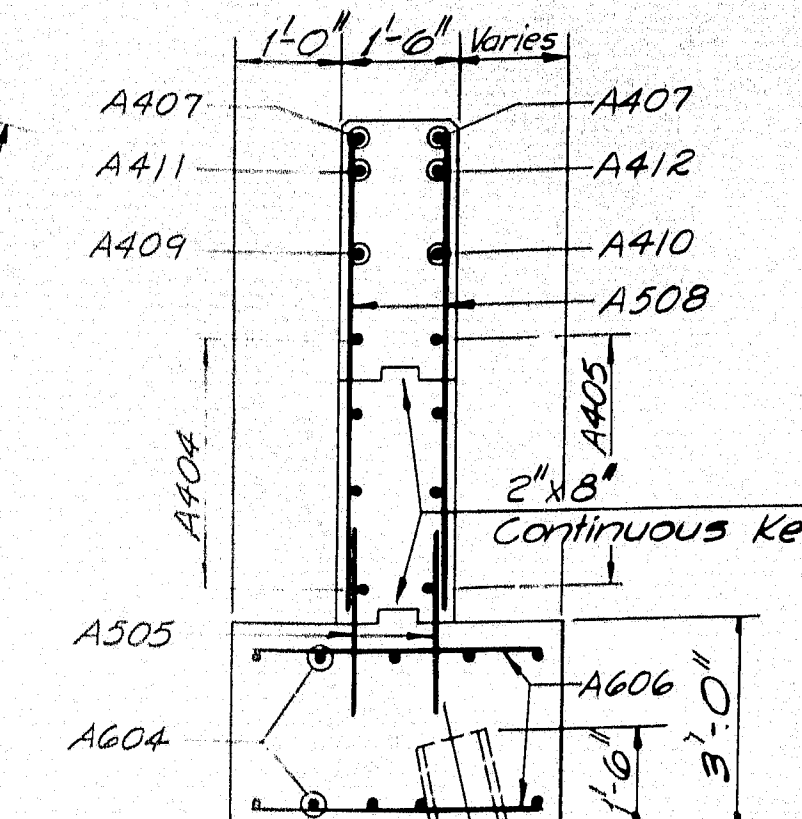
**NOTE:**

Bar numbers shown in Section A-A and B-B are for Abutment 1 S.B. For Abutment 2 S.B. add 20. For Abutment 1 N.B. add 40. For Abutment 2 N.B. add 60.

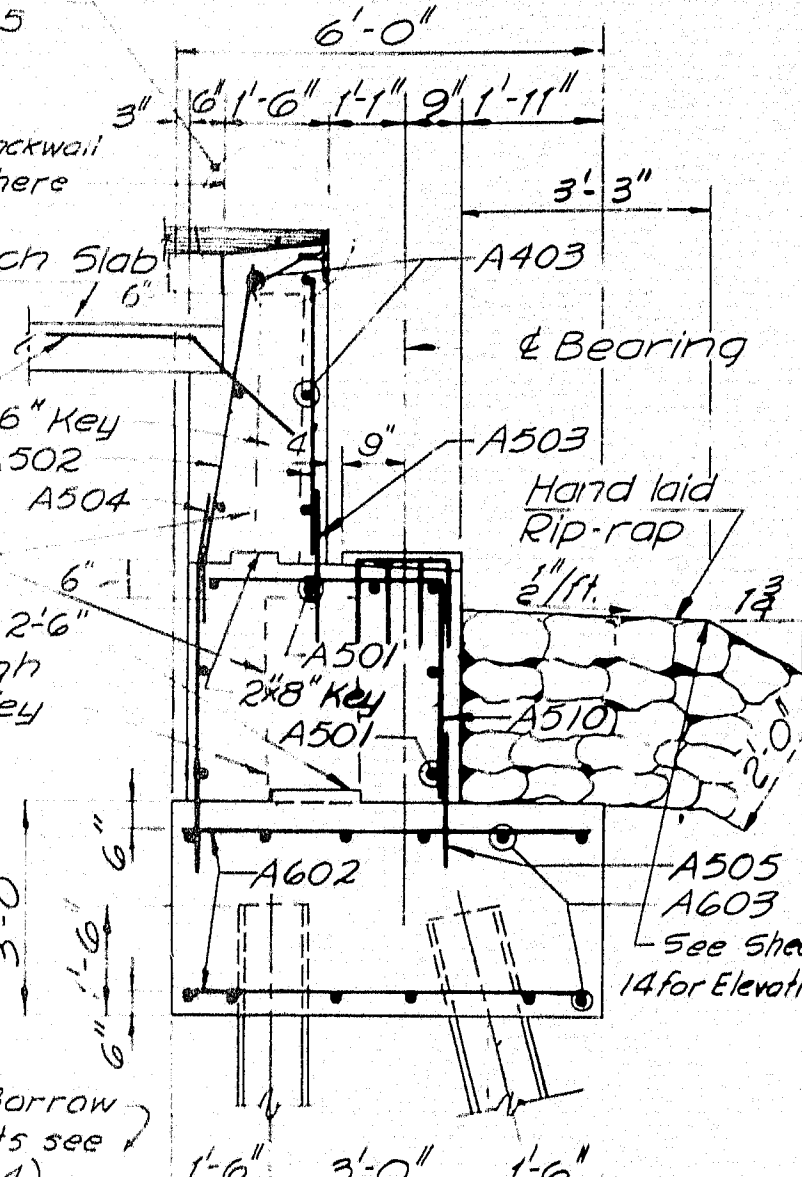
**FRONT ELEVATION**  
4" = 1'-0"



**SECTION B-B**  
3" = 1'-0"



**SECTION A-A**  
3" = 1'-0"



**GENERAL NOTES:**

1. For Approach Slab details see Sheet 5.
2. Paint Bridge Seal, face of Backwall, and 1'-0" below top of Slope Protection on face and ends of Breast Wall with Gray Epoxy Resin Surface Sealant.
3. Dress bearing areas 1" larger all around than the masonry plates to exact elevations shown.
4. Reinforcing steel to have 1" minimum cover unless otherwise shown.
5. Place reinforcing to clear Anchor Bolts.

⊙ n.f. denotes rear face, ⊕ denotes far face, ⊕ f denotes each face.

**PILE NOTES:**

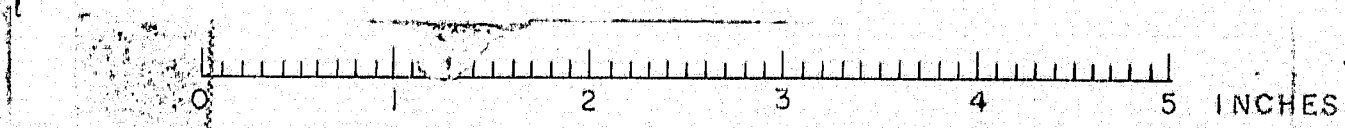
- ⊥ Indicates Vertical Piles
- ↘ Indicates Battered Piles Battered 3:12 in direction of arrow.
- All Piles 108 P42 with 37 Ton capacity.
- Estimated pile length 34 ft. Abut. 142. Piles to be driven to ledge or practical refusal to develop end bearing.

DESIGN-G.H. DETAIL-G.V. TRACE-CHECK-S.M.	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 S.B. OVER	
RELOCATED EAST BRANCH MATTAWAKEAG RIVER	
IN THE TOWN OF OAKFIELD	
ARROOSTOOK COUNTY ABUTMENT NO. 1	
SHEET 4 OF 16 AUGUSTA, MAINE FEBRUARY 1965	

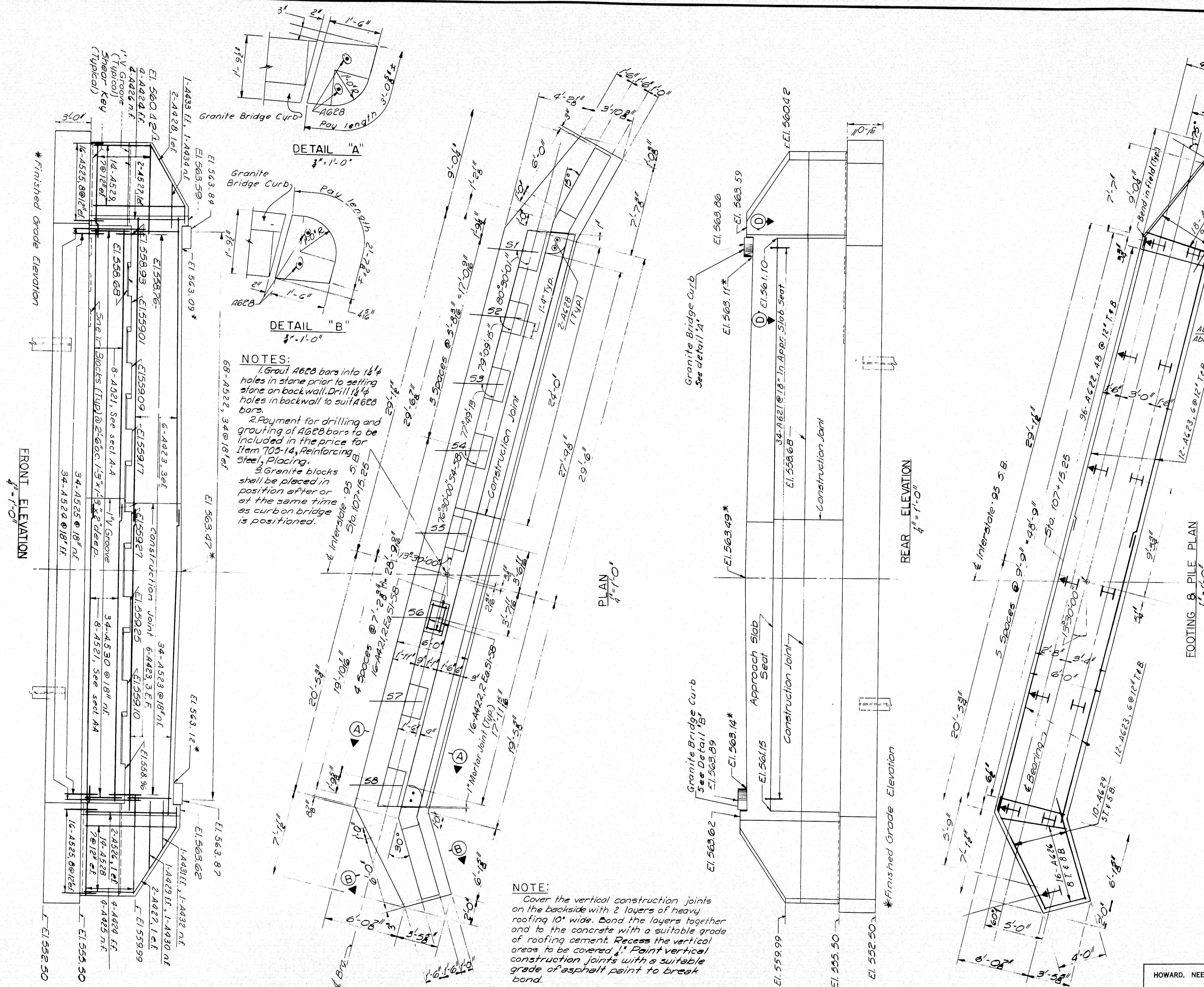
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NEW YORK BOSTON KANSAS CITY

M-2258

DYER BROOK OAKFIELD (12)







**DETAIL "A"**  
3'-1'-0"

**DETAIL "B"**  
3'-1'-0"

**NOTES:**  
1. Grout A628 bars into 1 1/2" holes in stone prior to setting alone on backwall. Drill 1 1/2" holes in backwall to suit A628 bars.  
2. Payment for drilling and grouting of A628 bars to be included in the price for Item 705-14, Reinforcing Steel, Placing.  
3. Granite blocks shall be placed in position after or at the same time as curb on bridge is positioned.

**DETAIL "C"**  
1'-1'-0"

**SECTION C-C**  
1'-1'-0"

**APPROACH SLAB DETAILS**  
ABUTMENT NO. 2 SHOWN - ABUTMENT NO. 1 SIMILAR

**NOTE:**  
For details of Armored Joint see Sheet BD104-64

**NOTE:**  
For Sections A-A and B-B see Sheet 4.  
For General Notes and Pile Notes see Sheet 4.

**DESIGN - G.H. TRACE - CHECK - P.R.N.**

**DETAIL - G.V. SURVEY - PLOT -**

**BRIDGE NO. 95 S.B. OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER IN THE TOWN OF OAKFIELD ARROOSTOOK COUNTY**

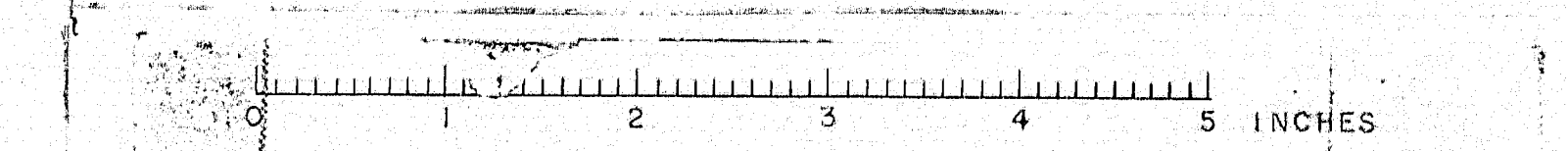
**ABUTMENT NO. 2 & APPROACH SLAB**

**SHEET 5 OF 16 AUGUSTA, MAINE FEBRUARY 1965**

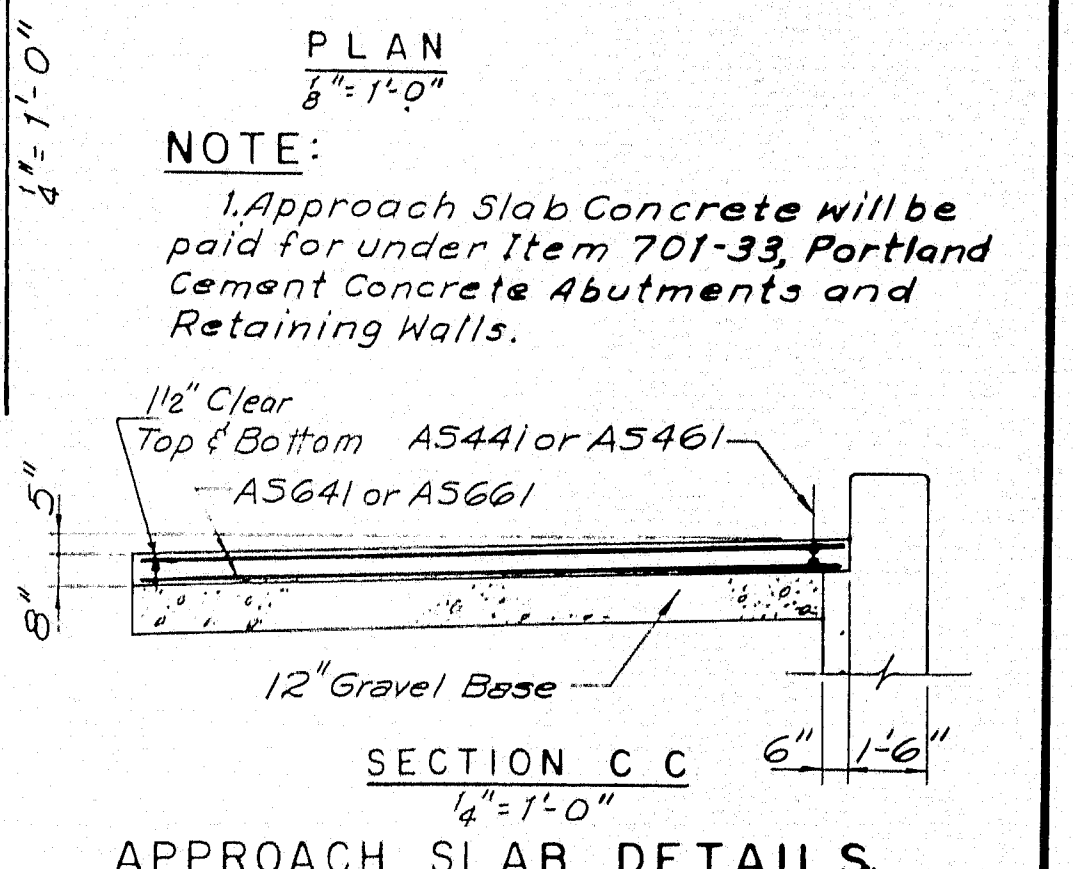
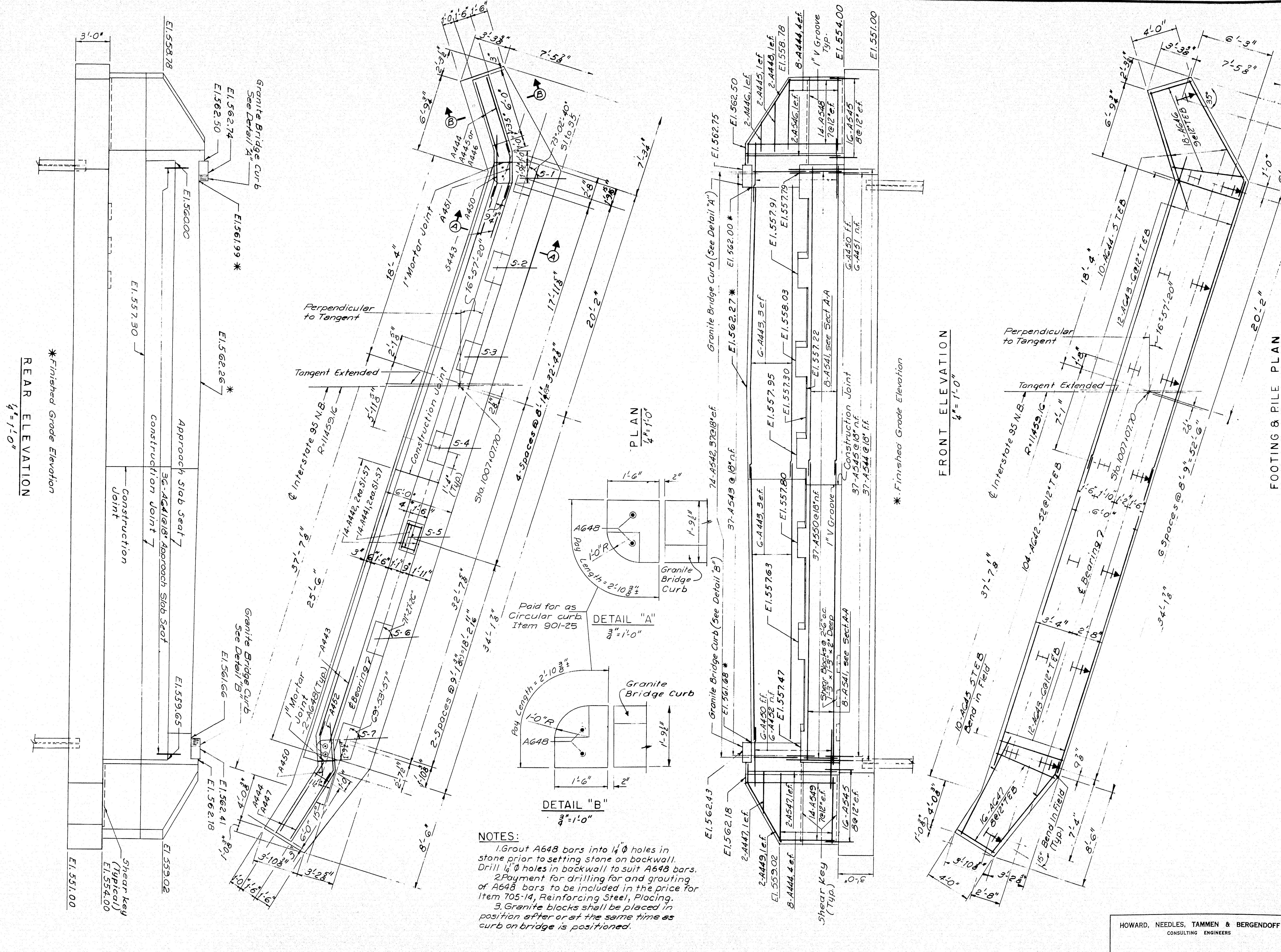
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**NEW YORK BOSTON KANSAS CITY**

**M-2259 DYER BROOK OAKFIELD (12)**



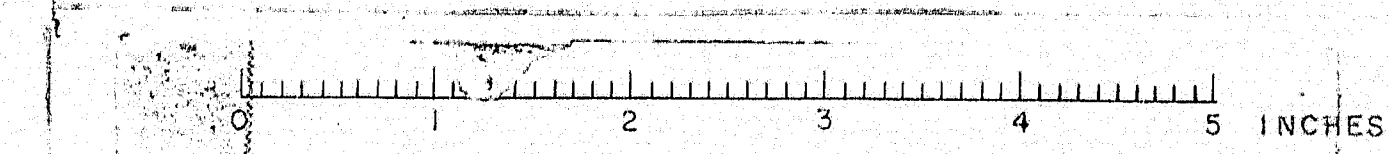




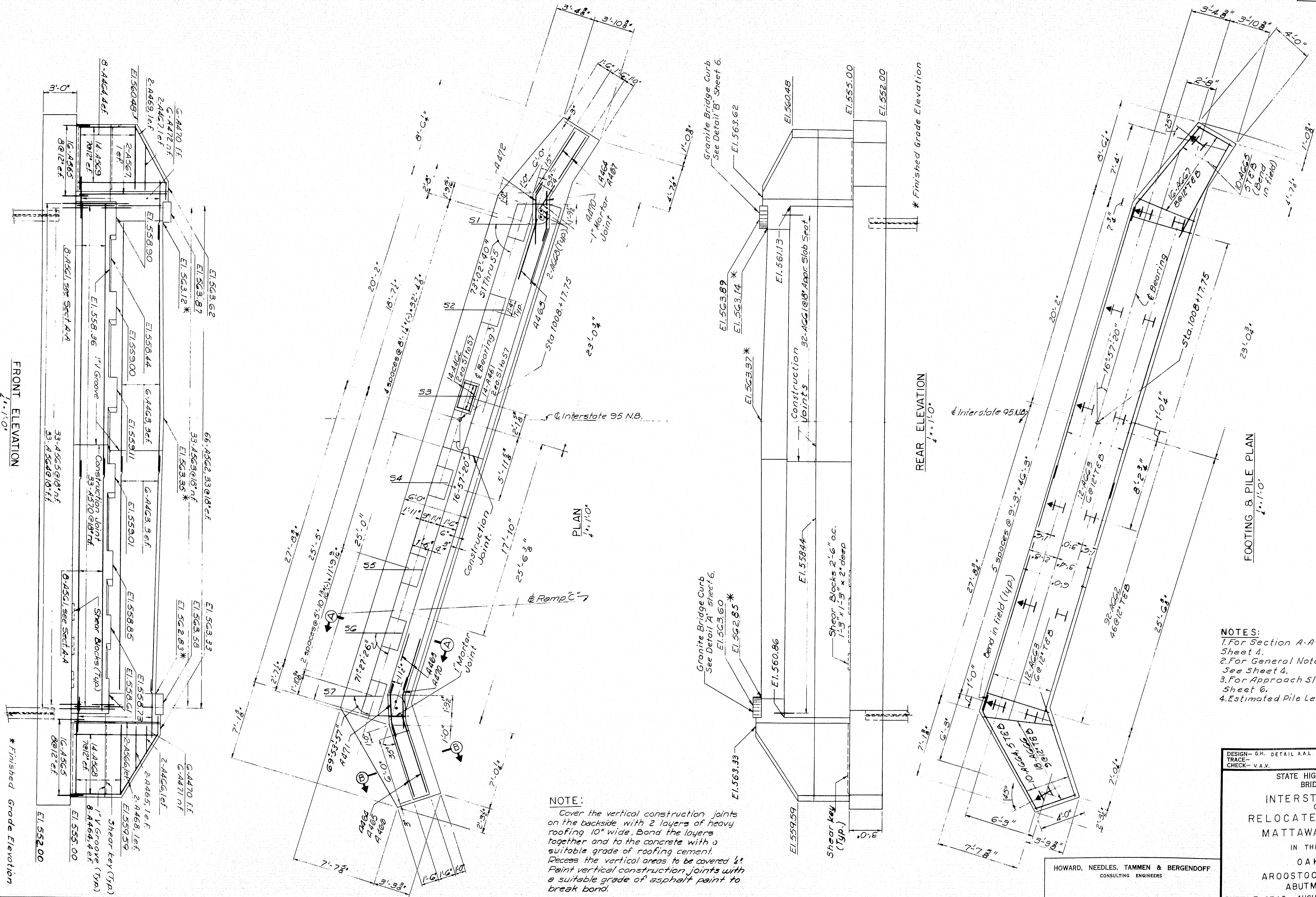
**NOTES:**

1. For Section A-A and B-B see sheet no. 4.
2. For General Notes and Pile Notes see sheet no. 4.
3. Estimated pile length 31 feet.
4. Cover the vertical construction joints on the backs with 2 layers of heavy roofing 10 wide. Bond the layers together and to the concrete with a suitable grade of roofing cement. Recess the vertical areas to be covered 4". Paint vertical construction joints with a suitable grade of asphalt paint to break bond.

DESIGN-G.H. DETAIL-G.E.C. BRIDGE NO. 95 N.B.  
TRACE-CHECK-V.A.V. SURVEY-PLOT-  
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
INTERSTATE 95 N.B.  
OVER  
RELOCATED EAST BRANCH  
MATTAWKEAG RIVER  
IN THE TOWN OF  
OAKFIELD  
ARROOSTOOK COUNTY  
ABUTMENT NO. 1 & APPROACH SLAB  
SHEET 6 OF 16 AUGUSTA, MAINE FEBRUARY 1965  
M-2260 DYER BROOK OAKFIELD (12)





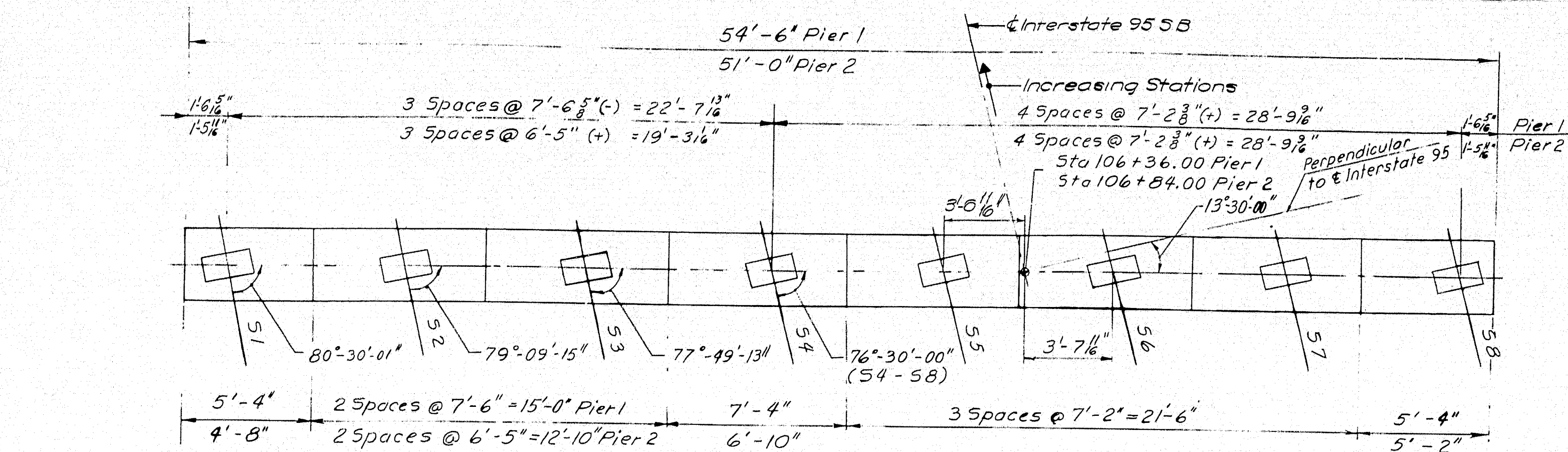


DESIGN - G.H. DETAIL A.A.L. TRACE - CHECK - V.A.V.	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 N.B. OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER	
IN THE TOWN OF OAKFIELD ARROOSTOOK COUNTY ABUTMENT NO. 2	
SHEET 7 OF 16 AUGUSTA, MAINE FEBRUARY 1965	

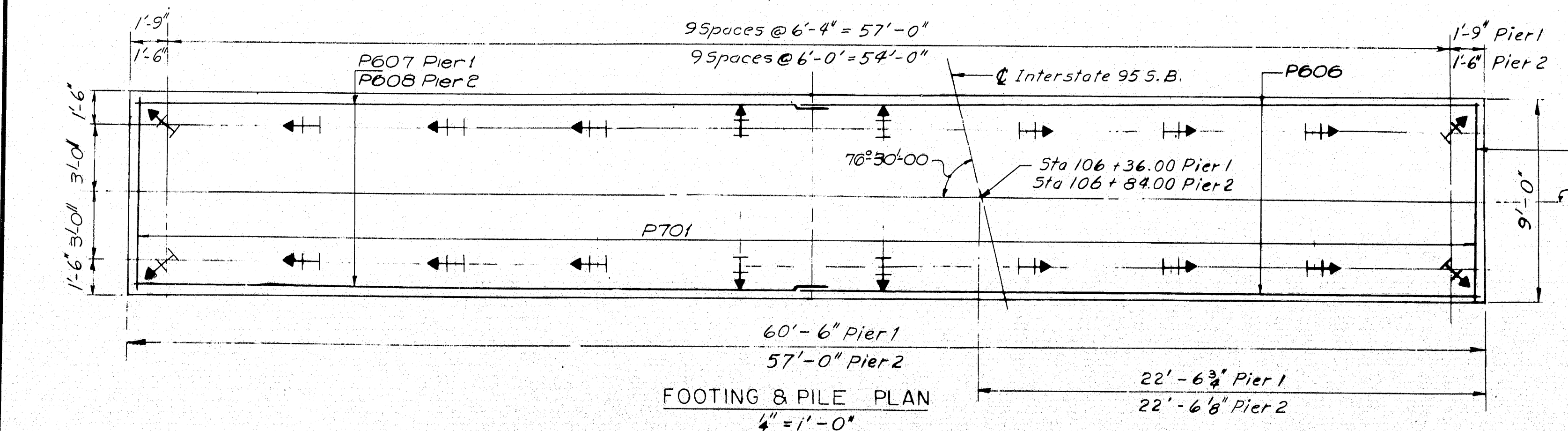
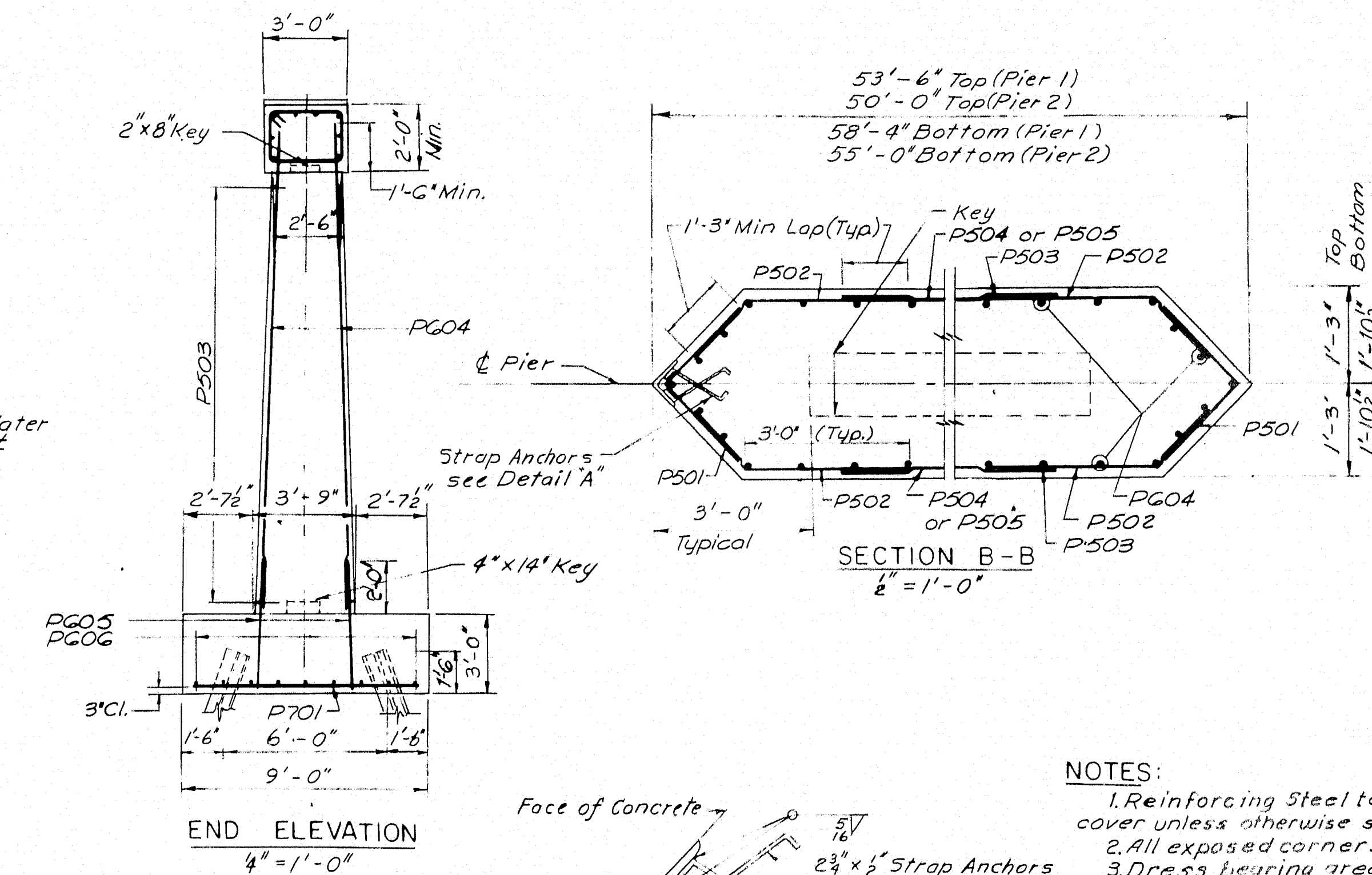
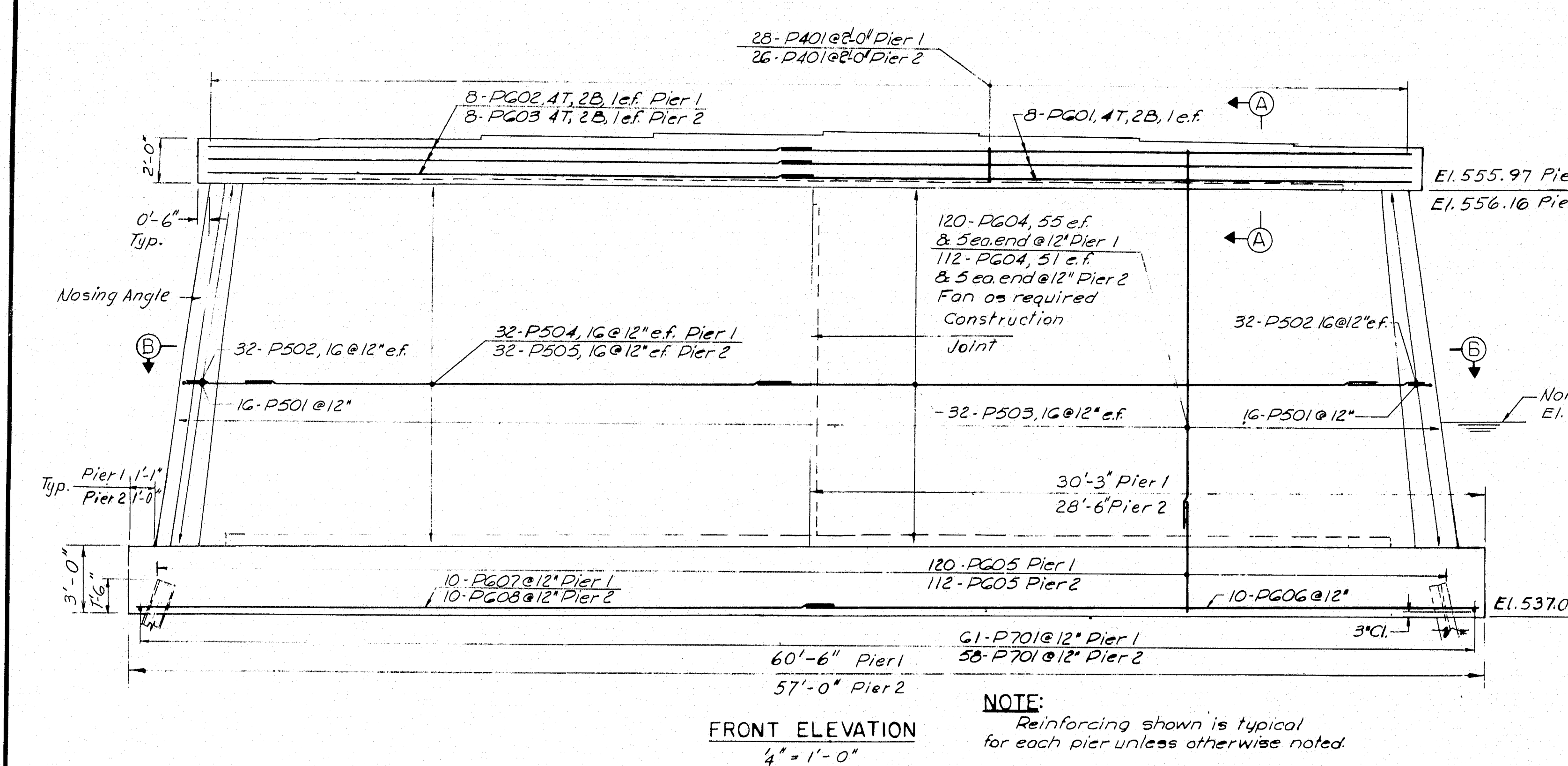
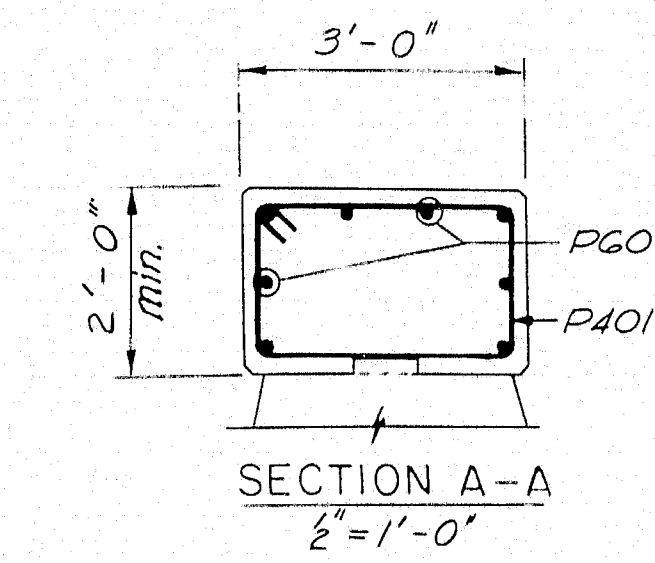
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NEW YORK BOSTON KANSAS CITY

M-2261 DYER BROOK OAKFIELD (12)





BEARING ELEVATIONS		
BEAM	PIER 1	PIER 2
51	557.97	558.16
52	558.08	558.25
53	558.20	558.34
54	558.31	558.44
55	558.42	558.54
56	558.40	558.52
57	558.26	558.38
58	558.13	558.24



NOTE:  
Reinforcing shown is typical for each pier unless otherwise noted.

- PILE NOTES:
1. Indicates vertical Piles
  2. Indicates Batter Pile, battered 3:12 in direction of arrow.
  3. All piles 10B P42 Capacity 37 tons
  4. Estimated Pile Length: 23 feet
  5. Piles to be driven to ledge or practical refusal to develop end bearings.

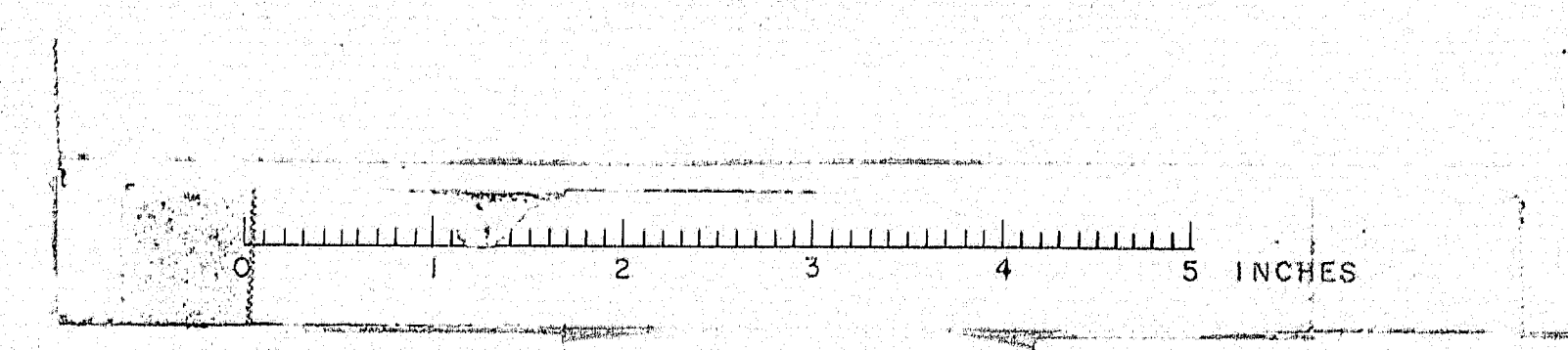
DETAIL A  
1/4" = 1'-0"

- NOTES:
1. Reinforcing Steel to have 2" minimum cover unless otherwise shown.
  2. All exposed corners to have 1" chamfer.
  3. Dress bearing areas 1" larger all around, than masonry plates to exact elevations shown.
  4. Place reinforcing to clear anchor bolts.
  5. E.R. - denotes each face.

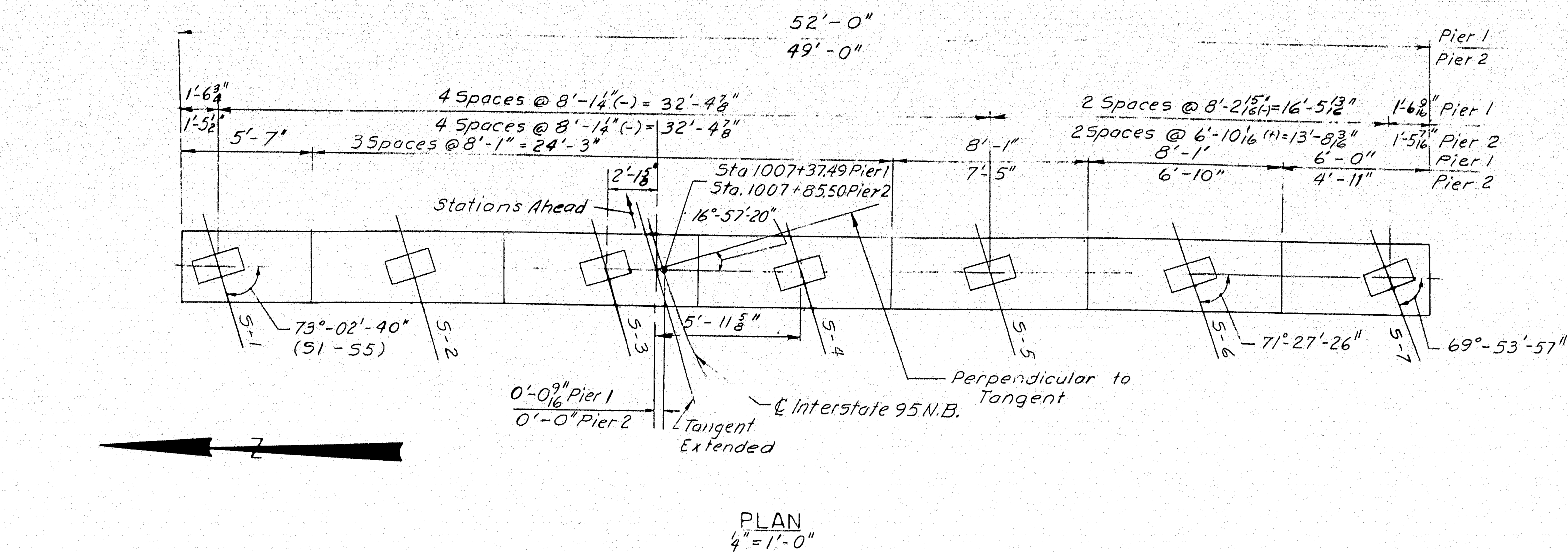
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CONSULTING ENGINEERS  
NEW YORK BOSTON KANSAS CITY

DESIGN - E. F. T. OF 12/11/95	BRIDGE NO. SURVEY - PLOT -
TRACE - P. R. P.	
CHECK - P. R. P.	
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 S.B. OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER IN THE TOWN OF OAKFIELD AROSTOOK COUNTY PIERS	
SHEET 8 OF 16 AUGUSTA, MAINE FEBRUARY 1965 DYER BROOK OAKFIELD (12)	

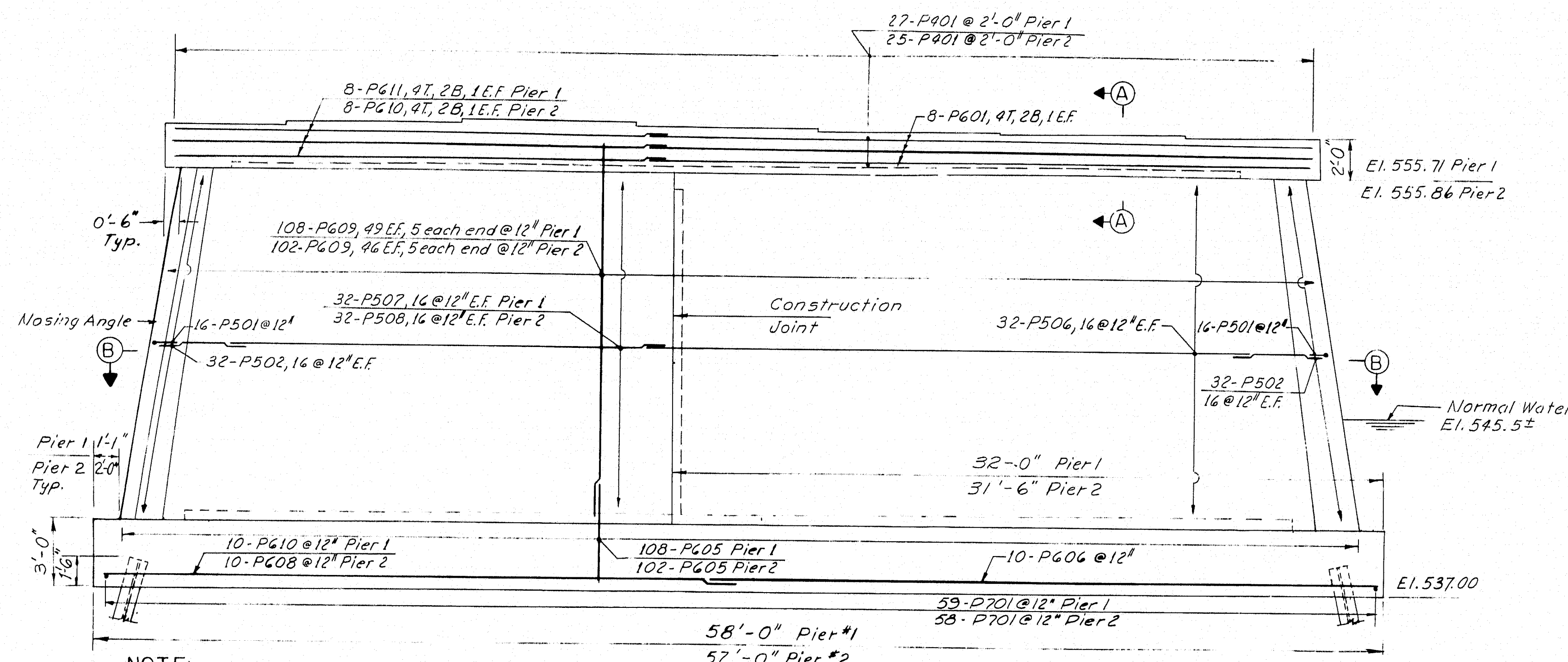
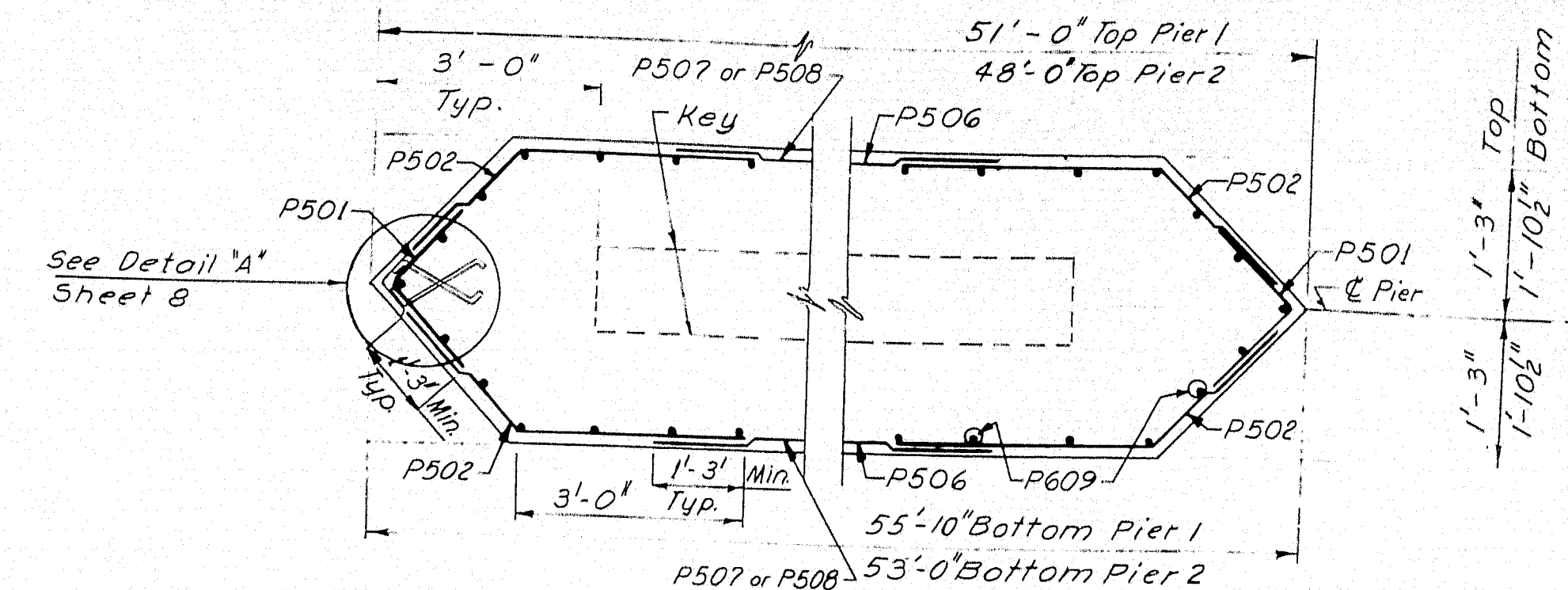
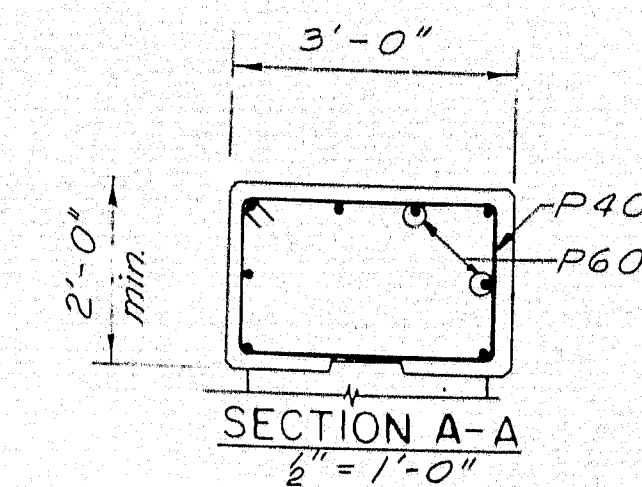
M-2262



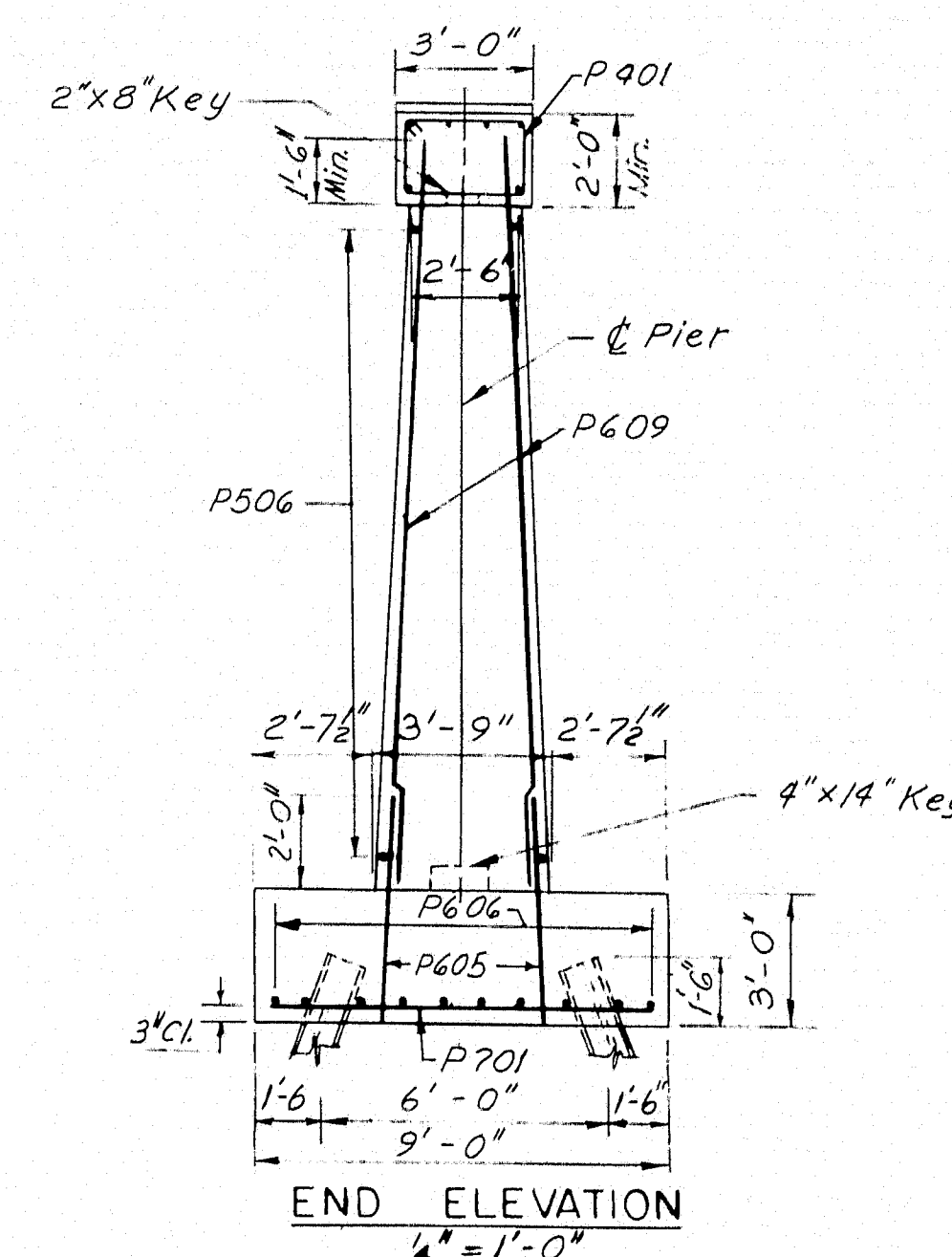
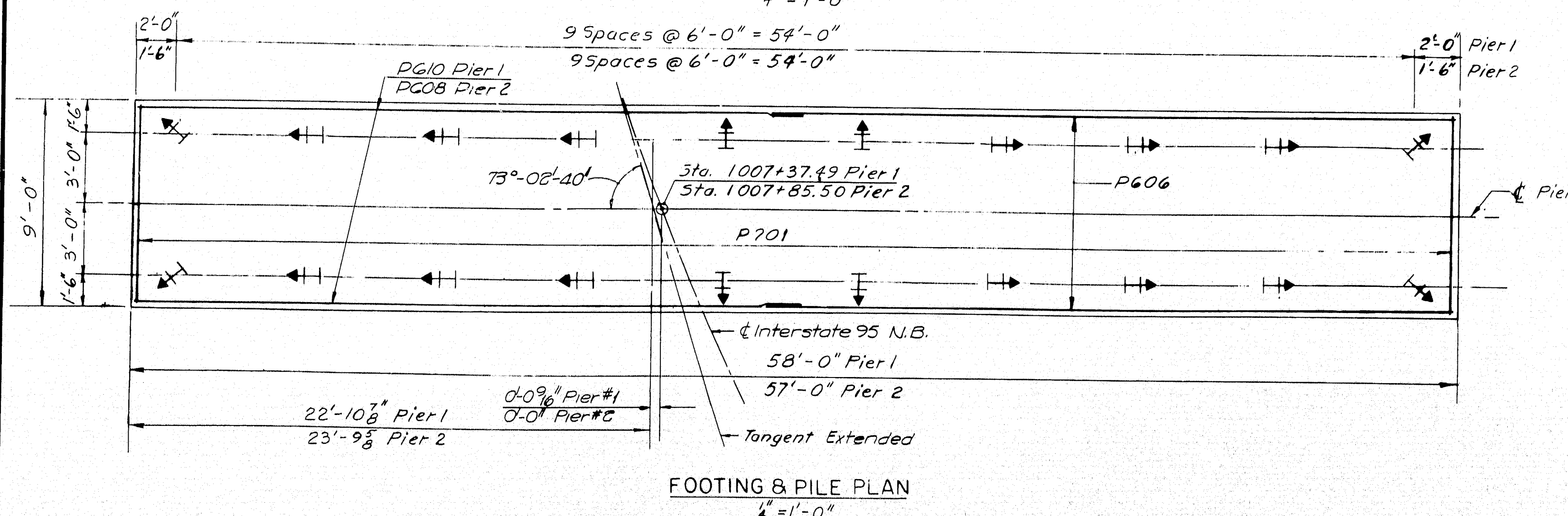




BEAM	PIER 1	PIER 2
S1	558.03	558.16
S2	558.15	558.27
S3	558.26	558.38
S4	558.18	558.29
S5	558.02	558.13
S6	557.87	557.99
S7	557.71	557.86



NOTE:  
Reinforcing shown is typical for each pier unless otherwise noted.



- NOTES:
1. Reinforcing steel to have 2" minimum cover unless otherwise shown.
  2. All exposed corners to have 1" chamfer.
  3. Dress bearing areas 1" larger all around, than masonry plates to exact elevations shown.
  4. Place reinforcing to clear anchor bolts.
  5. E.F. denotes each face.

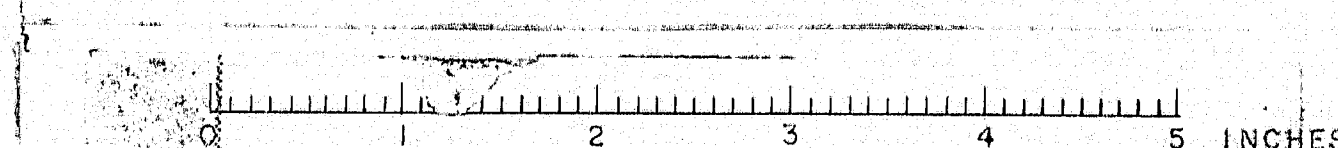
#### PILE NOTES:

1. I Indicates vertical piles.
2. I Indicates Batter Pile, battered 3:12 in direction of arrow.
3. All piles 10BP42 capacity = 37 Tons.
4. Estimated Pile Length: 23 feet.
5. Piles to be driven to ledge or practical refusal to develop end bearing.

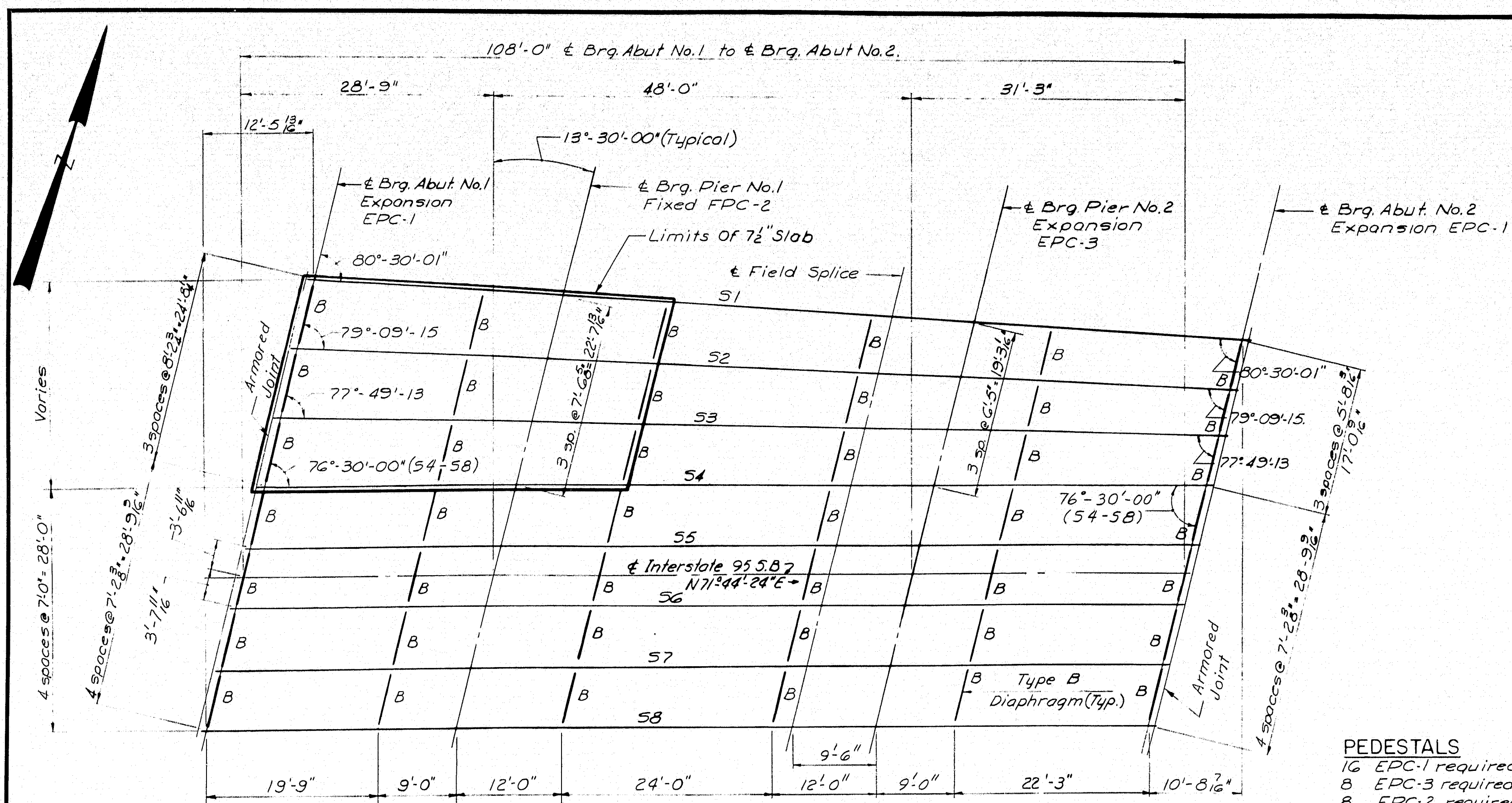
DESIGN - E.F.K. DETAIL-R.F.	BRIDGE NO.
TRACE - P.R.N.	SURVEY - PLOT
STATE HIGHWAY COMMISSION	
BRIDGE DIVISION	
INTERSTATE 95 N.B.	
OVER	
RELOCATED EAST BRANCH	
MATTAWAMKEAG RIVER	
IN THE TOWN OF	
OAKFIELD	
AROSTOOK COUNTY	
PIERS	
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS	SHEET 9 OF 16
NEW YORK BOSTON KANSAS CITY	AUGUSTA, MAINE FEBRUARY 1965

M-2263

DYER BROOK OAKFIELD (12)

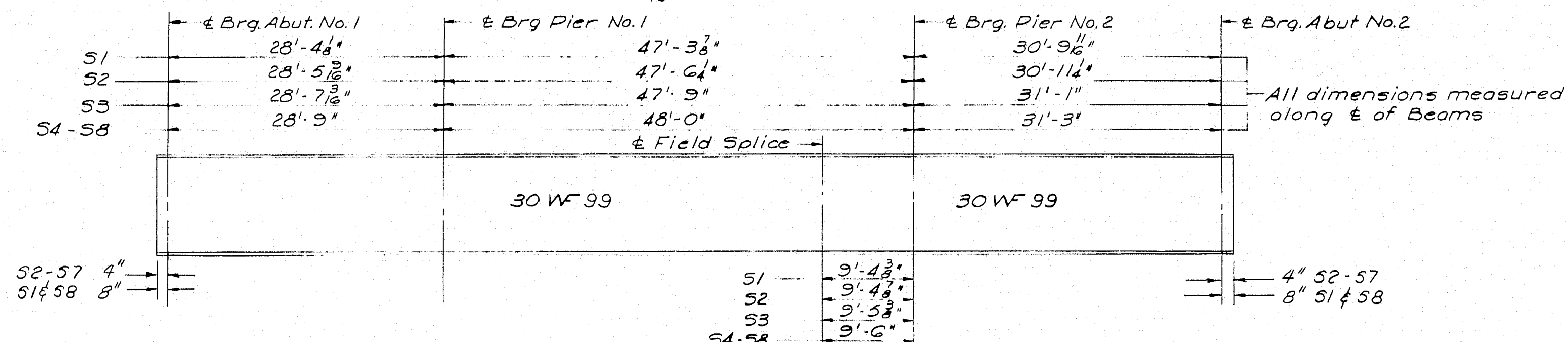






ERECTION DIAGRAM

1" = 10'

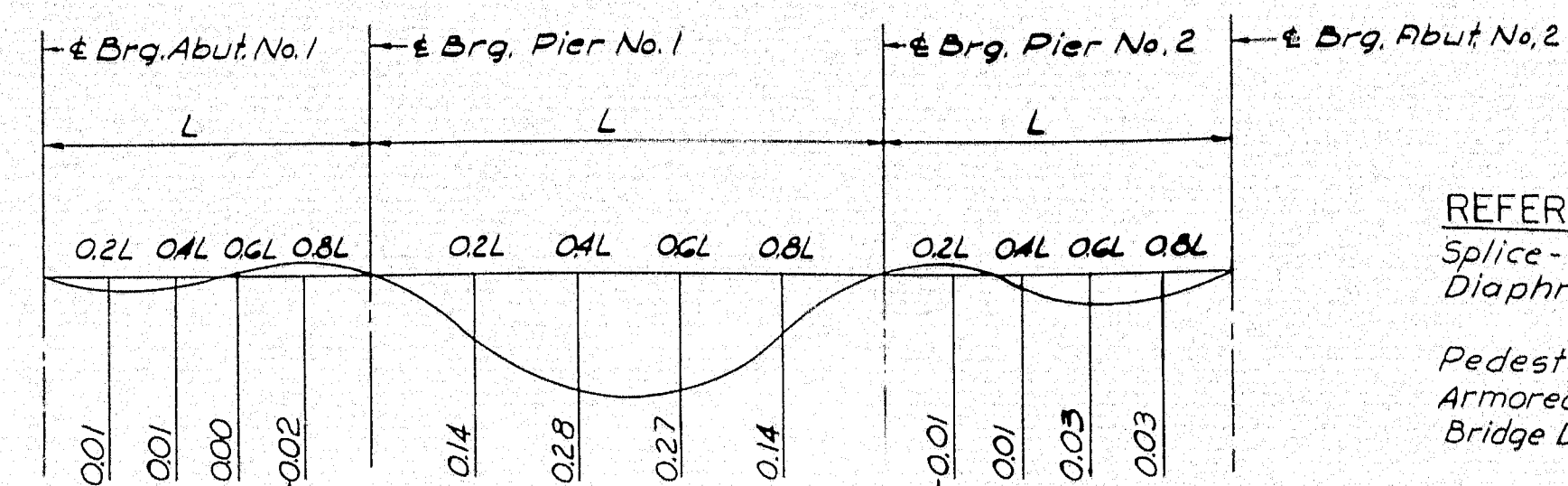


TYPICAL STRINGER ELEVATION

All dimensions horizontal

BOTTOM OF SLAB ELEVATIONS AT BLOCKING POINTS

	Span No. 1	Span No. 2	Span No. 3	Span No. 4
	Span No. 1	Span No. 2	Span No. 3	Span No. 4
Line	Span No. 1	Span No. 2	Span No. 3	Span No. 4
Line 1	561.03	561.08	561.14	561.19
Line 2	561.15	561.20	561.25	561.31
Line 3	561.28	561.33	561.37	561.42
Line 4	561.40	561.45	561.49	561.54
Line 5	561.55	561.60	561.64	561.69
Line 6	561.54	561.58	561.63	561.67
Line 7	561.40	561.45	561.49	561.54
Line 8	561.27	561.31	561.36	561.40
Span No. 1	5'-8 3/4"	11'-5 1/4"	17'-1 1/4"	22'-10 1/4"
Span No. 2	5'-8 3/4"	11'-5 1/4"	17'-1 1/4"	22'-10 1/4"
Span No. 3	5'-8 3/4"	11'-5 1/4"	17'-1 1/4"	22'-10 1/4"
Span No. 4	5'-8 3/4"	11'-5 1/4"	17'-1 1/4"	22'-10 1/4"



DEAD LOAD DEFLECTION DIAGRAM

ALL DEFLECTIONS IN INCHES

NOTE:  
No Shop camber required.  
Natural mill camber to be placed up.

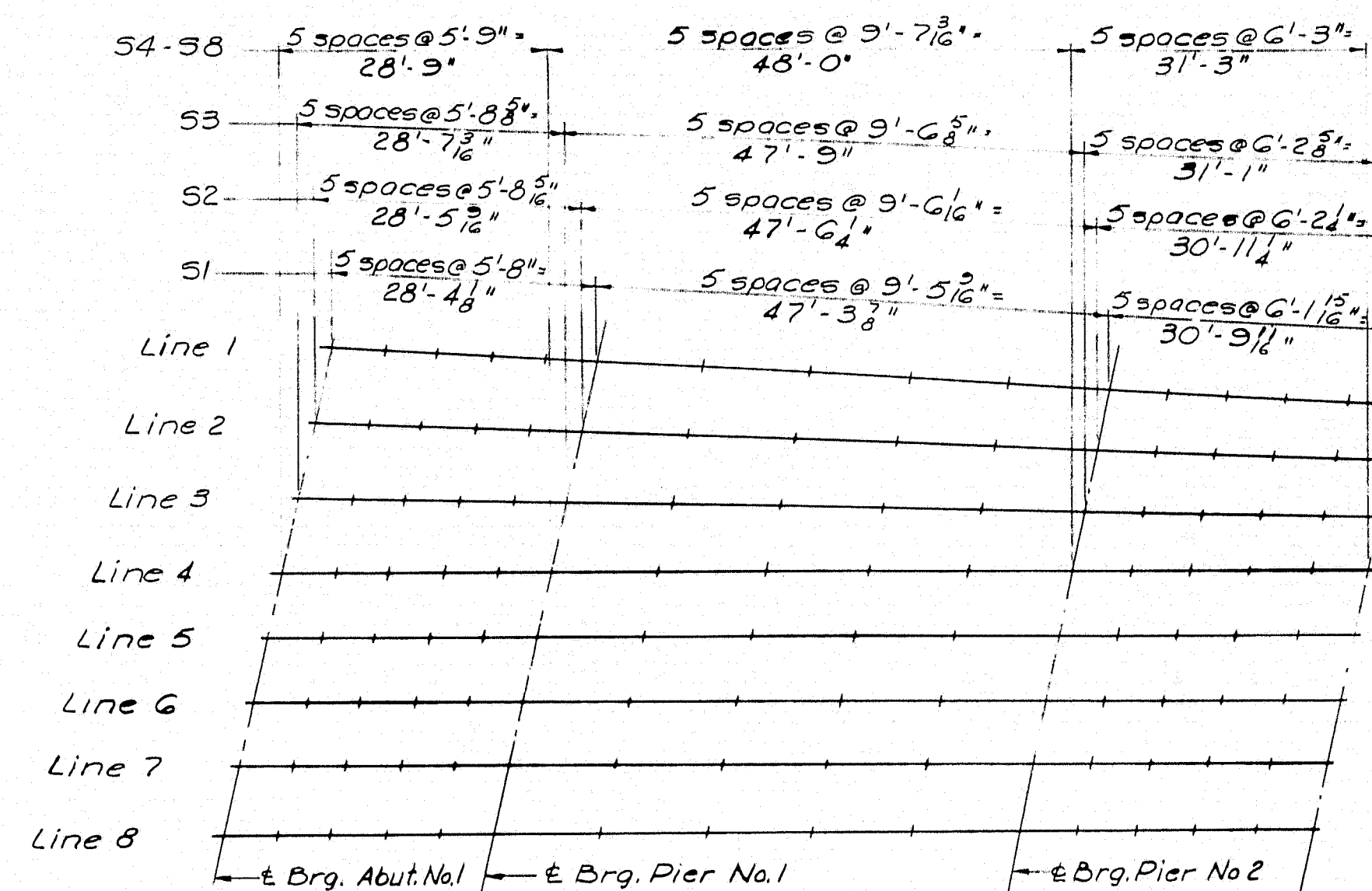
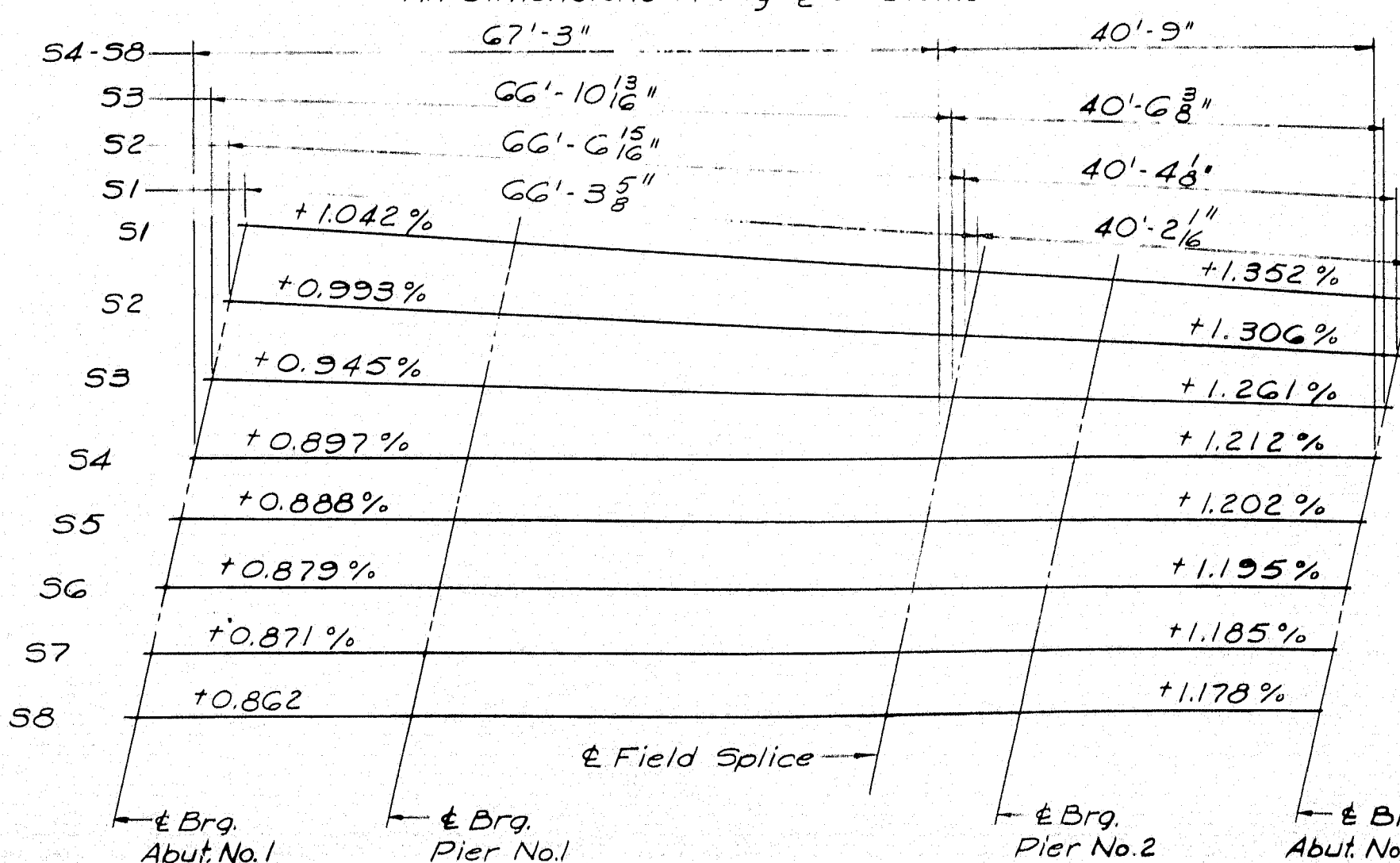


DIAGRAM OF BLOCKING POINTS

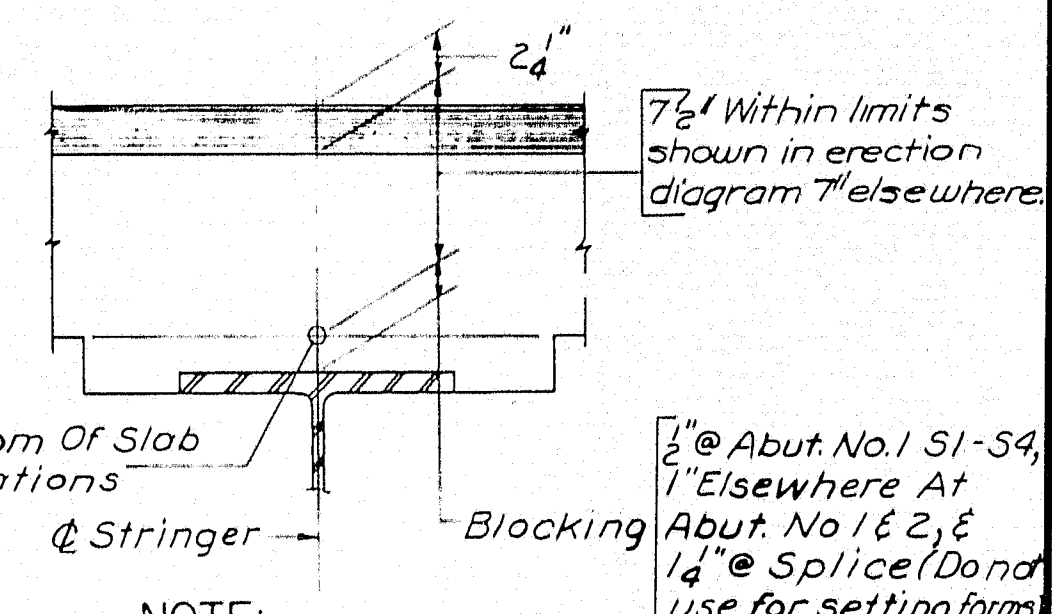
All Dimensions Along & of Beams



BEAM GRADES

REFERENCE  
Splice - See Standard Details BD 103-64  
Diaphragms - See Standard Details BD 104-64  
Pedestals - See Standard Details BD 101-64  
Armored Joint - See Standard Details BD 104-64  
Bridge Drain - See Standard Details BD 104-64

SPECIFICATIONS  
Fabrication and Erection: State of Maine  
Standard Specifications, Highway and  
Bridges, Revision of Jan. 1956 and  
Supplemental Specifications of Feb. 1960  
Design and Detail: A.A.S.H.O. Standard  
Specifications for Highway Bridges of 1961 and  
Interim Specifications of 1961, 1962, 1963 & 1964.  
Materials: Except as otherwise noted  
on the Standard Details, all materials  
shall conform to A.S.T.M. Designation A-36



BLOCKING DETAIL

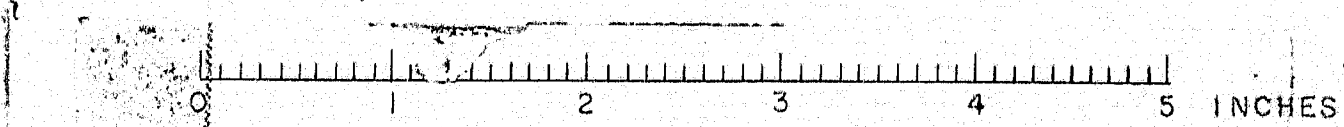
NOTE:  
To compensate for dead load deflections  
as well as possible irregularities in beams,  
set the bottom of slab elevation at the  
points indicated before any of the slab  
formwork is started.

BLOCKING DETAIL

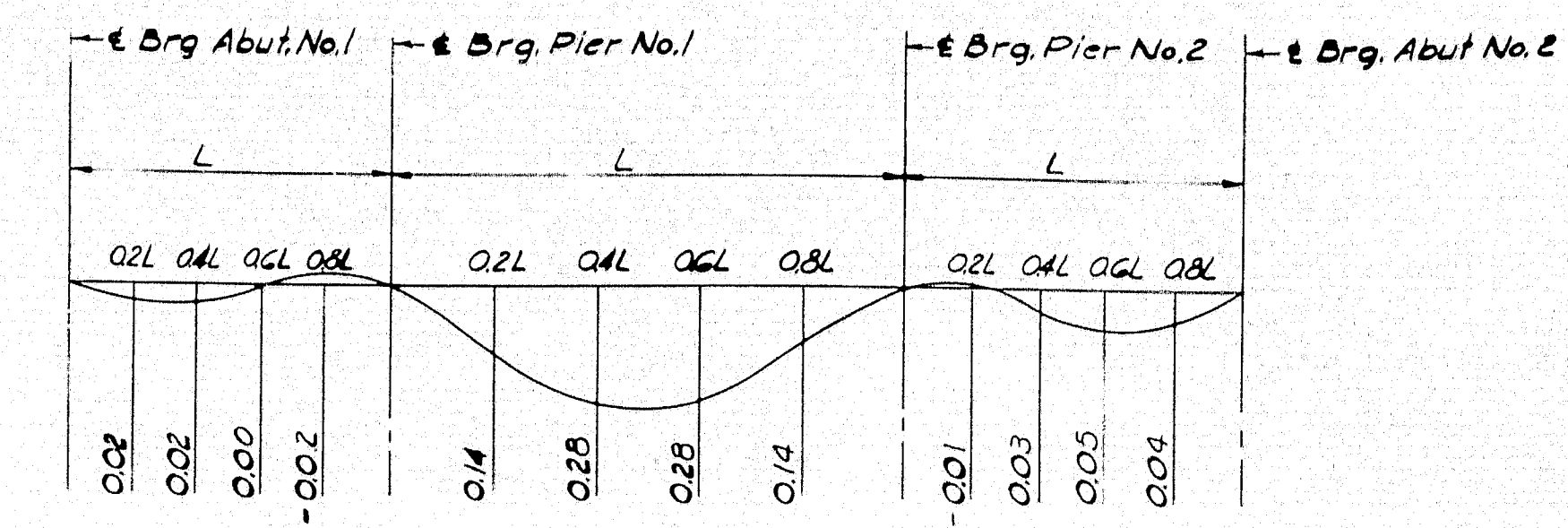
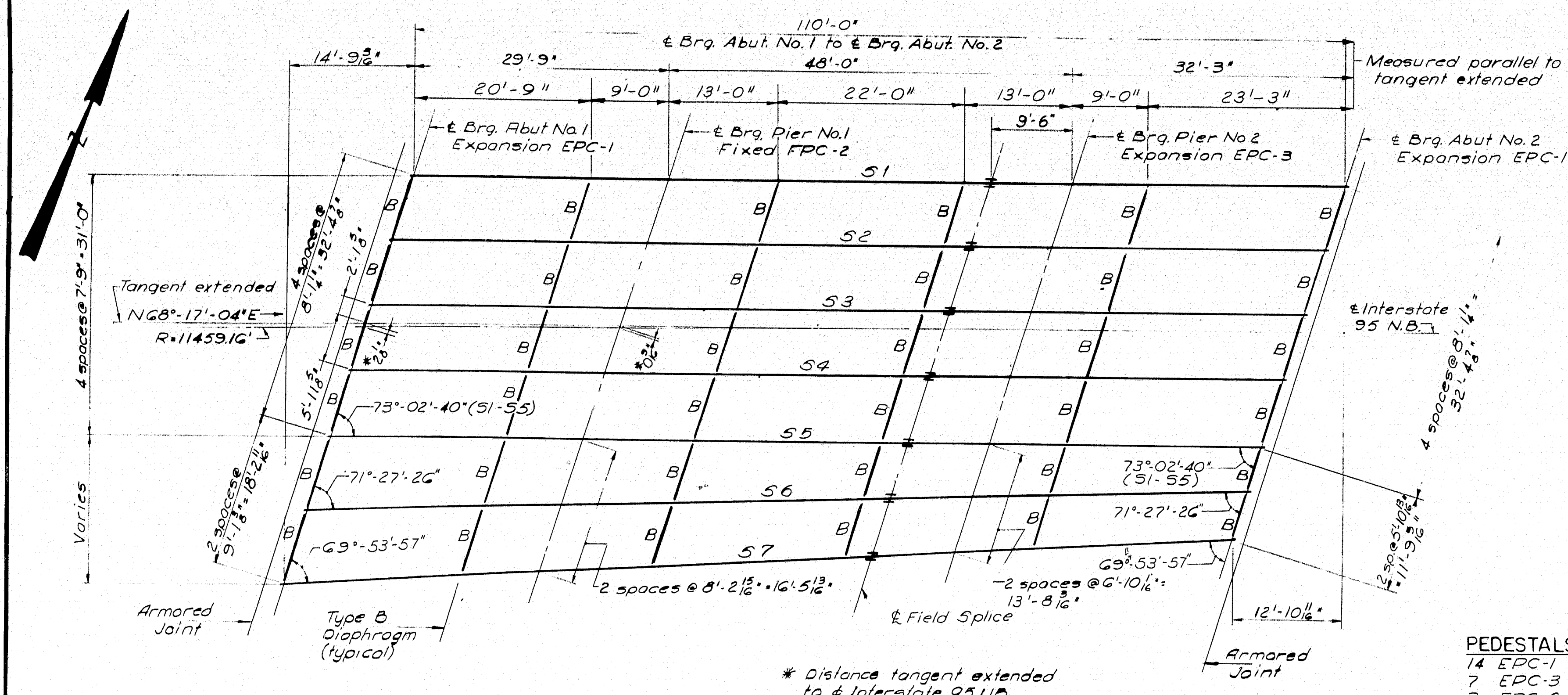
No Scale

DESIGN - G.H. DETAIL - A.A.L.  
TRACE - S.M.  
CHECK - S.M.  
BRIDGE NO. SURVEY - 109  
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
INTERSTATE 95 SB  
OVER  
RELOCATED EAST BRANCH  
MATTAWAMKEAG RIVER  
IN THE TOWN OF  
OAKFIELD  
ARROOSTOOK COUNTY  
STRUCTURAL STEEL & BLOCKING  
SHEET 10 OF 16 AUGUSTA, MAINE FEBRUARY 1965  
DYER BROOK OAKFIELD (12)

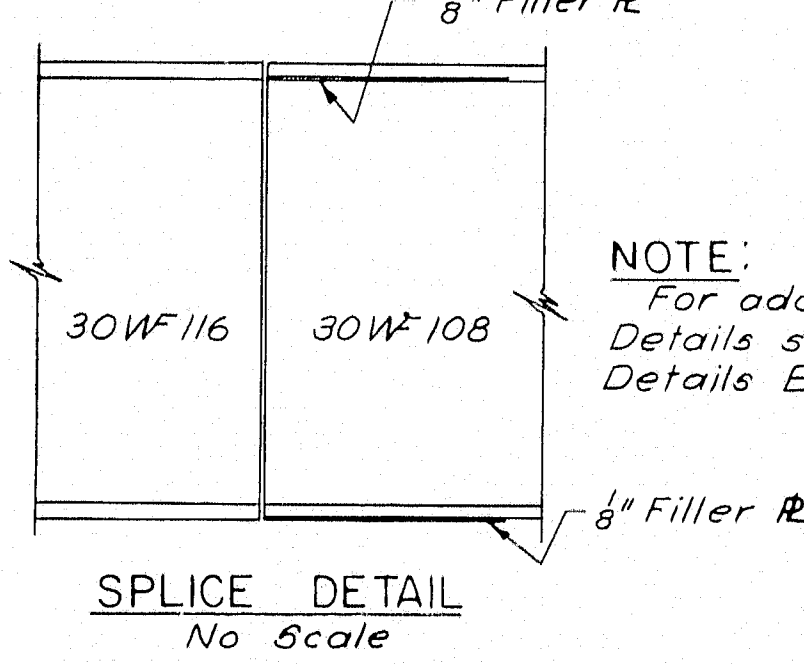
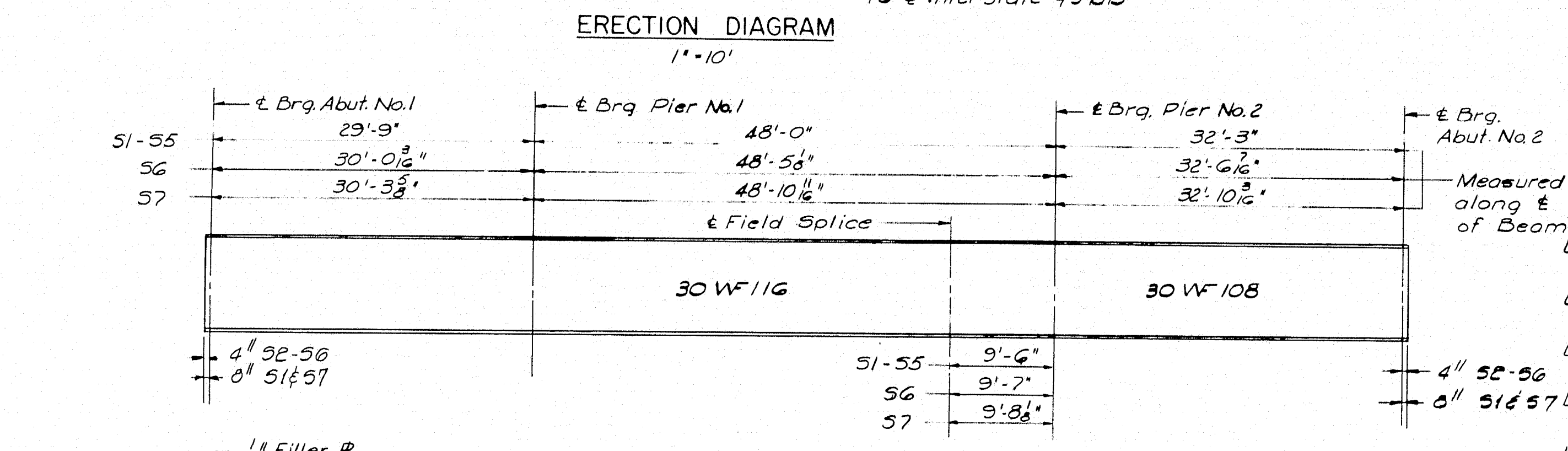
M-2264



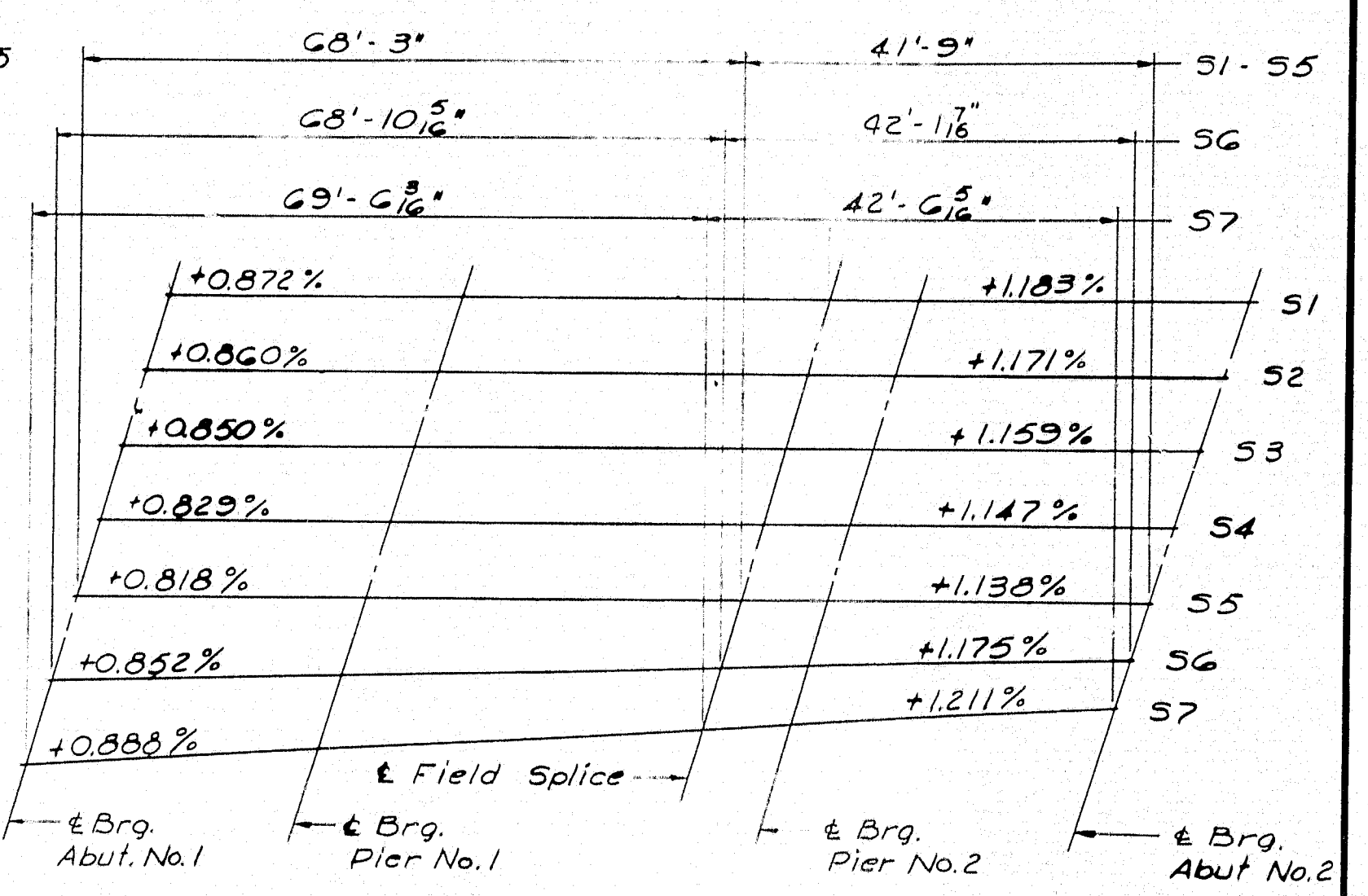
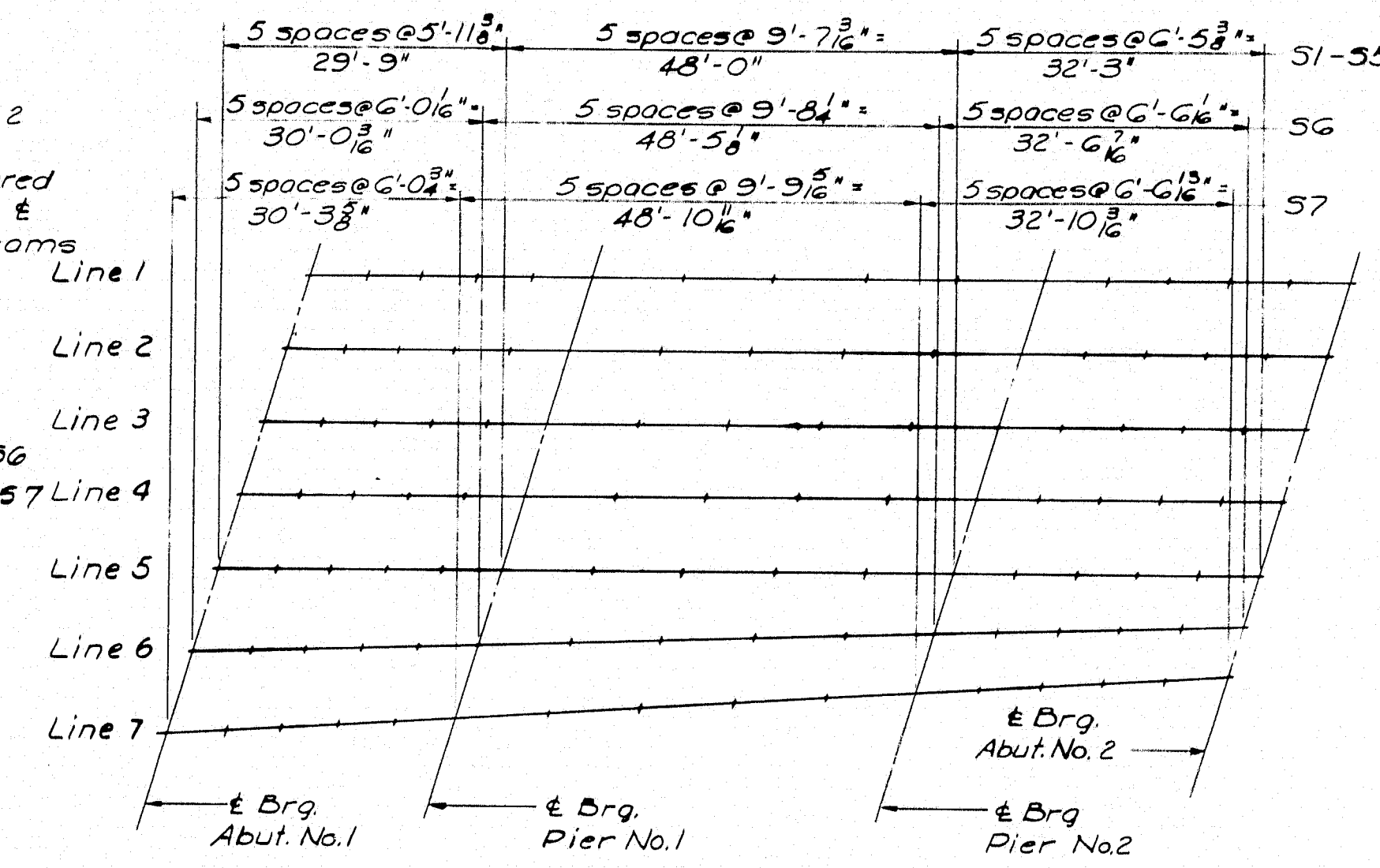




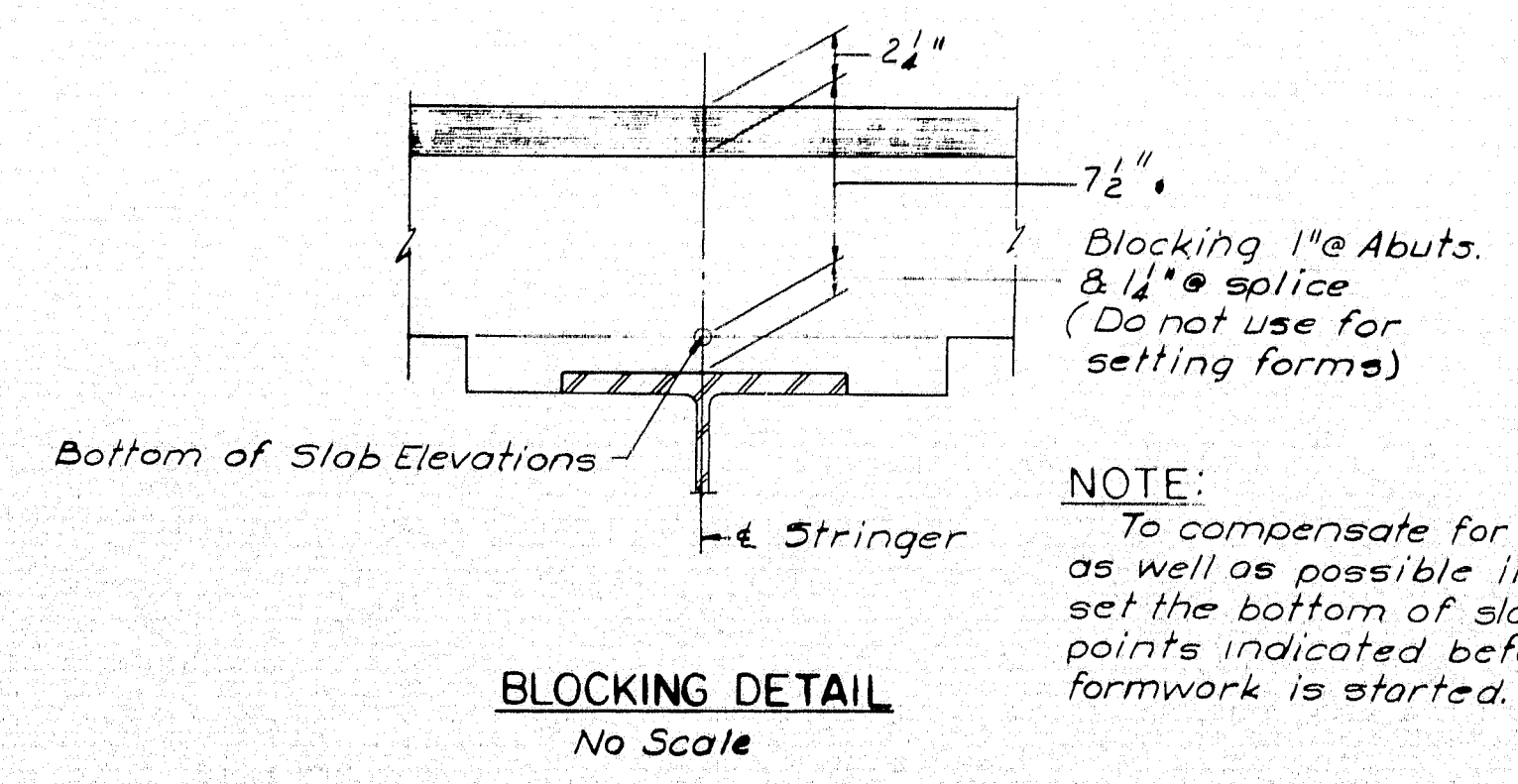
**REFERENCE**  
 Splice - See Standard Details BD-103-G4  
 Diaphragms - See Standard Details BD-104-G4  
 Pedestals - See Standard Details BD-101-G4  
 Armored Joint - See Standard Details BD-104-G4  
 Bridge Drains - See Standard Details BD-104-G4  
**SPECIFICATIONS**  
 Fabrication and Erection: State of Maine Standard Specifications, Highway and Bridges, Revision of Jan. 1956 and Supplemental Specifications of Feb. 1960.  
 Design and Detail: A.A.S.H.O. Standard Specifications for Highway Bridges of 1961 and Interim Specifications of 1961, 1962, 1963 & 1964.  
 Materials: Except as otherwise noted on the Standard Details, all materials shall conform to A.S.T.M. Designation A-36



**PEDESTALS**  
 14 EPC-1 Required  
 7 EPC-3 Required  
 7 FPC-2 Required



	Abut. No. 1	SPAN NO. 1					Abut. No. 2	SPAN NO. 2					Abut. No. 3	SPAN NO. 3					Abut. No. 4
	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55	51-55
Line 1	561.20	561.24	561.29	561.33	561.38	561.43	561.53	561.63	561.73	561.81	561.91	561.98	562.05	562.13	562.21	562.29	562.39	562.49	562.59
Line 2	561.32	561.36	561.41	561.45	561.50	561.55	561.64	561.74	561.84	561.92	562.01	562.09	562.16	562.24	562.32	562.39	562.49	562.59	562.69
Line 3	561.43	561.48	561.52	561.57	561.61	561.66	561.76	561.86	561.95	562.03	562.12	562.19	562.27	562.35	562.42	562.50	562.59	562.69	562.79
Line 4	561.36	561.40	561.44	561.49	561.53	561.58	561.67	561.77	561.86	561.95	562.03	562.10	562.18	562.25	562.33	562.40	562.49	562.59	562.69
Line 5	561.21	561.25	561.29	561.33	561.38	561.43	561.52	561.62	561.70	561.79	561.87	561.94	562.02	562.09	562.17	562.24	562.33	562.43	562.53
Line 6	561.04	561.08	561.13	561.17	561.22	561.27	561.37	561.47	561.56	561.65	561.74	561.81	561.89	561.97	562.04	562.12	562.21	562.31	562.41
Line 7	560.87	560.92	560.96	561.01	561.06	561.11	561.21	561.32	561.42	561.51	561.61	561.68	561.76	561.84	561.92	562.00	562.09	562.19	562.29
56	6'-0.16"	12'-0.16"	18'-0.16"	24'-0.16"	30'-0.16"	36'-0.16"	42'-0.16"	48'-0.16"	54'-0.16"	60'-0.16"	66'-0.16"	72'-0.16"	78'-0.16"	84'-0.16"	90'-0.16"	96'-0.16"	102'-0.16"	108'-0.16"	114'-0.16"
57	6'-0.16"	12'-0.16"	18'-0.16"	24'-0.16"	30'-0.16"	36'-0.16"	42'-0.16"	48'-0.16"	54'-0.16"	60'-0.16"	66'-0.16"	72'-0.16"	78'-0.16"	84'-0.16"	90'-0.16"	96'-0.16"	102'-0.16"	108'-0.16"	114'-0.16"



DESIGN - G.H. DETAIL A.A.L.  
 TRACE - S.M.  
 CHECK - S.M.

BRIDGE NO. 110  
 SURVEY - PLOT

STATE HIGHWAY COMMISSION  
 BRIDGE DIVISION

INTERSTATE 95 NB.  
 OVER  
 RELOCATED EAST BRANCH  
 MATTAWAKEAG RIVER

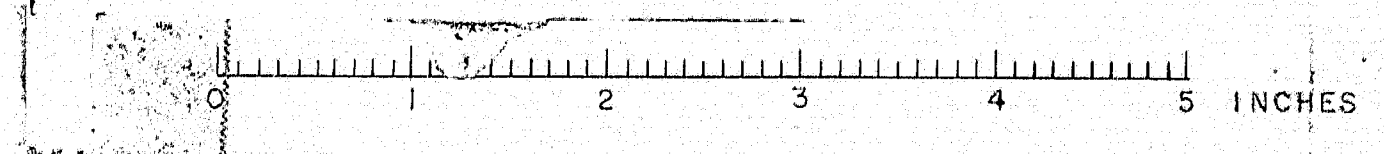
IN THE TOWN OF  
 OAKFIELD  
 AROOSTOOK COUNTY

STRUCTURAL STEEL & BLOCKING

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
 CONSULTING ENGINEERS  
 NEW YORK BOSTON KANSAS CITY

SHEET 11 OF 16 AUGUSTA, MAINE FEBRUARY 1965

M-2265 DYER BROOK OAKFIELD (12)





NOTE: Because of the Monolithic Placement of the Concrete Slab, the Construction Joints were Eliminated and the Reinforcing Either Spliced or Placed Continuously. The Concrete Haunch Over the B Diaphragm was Also Eliminated.

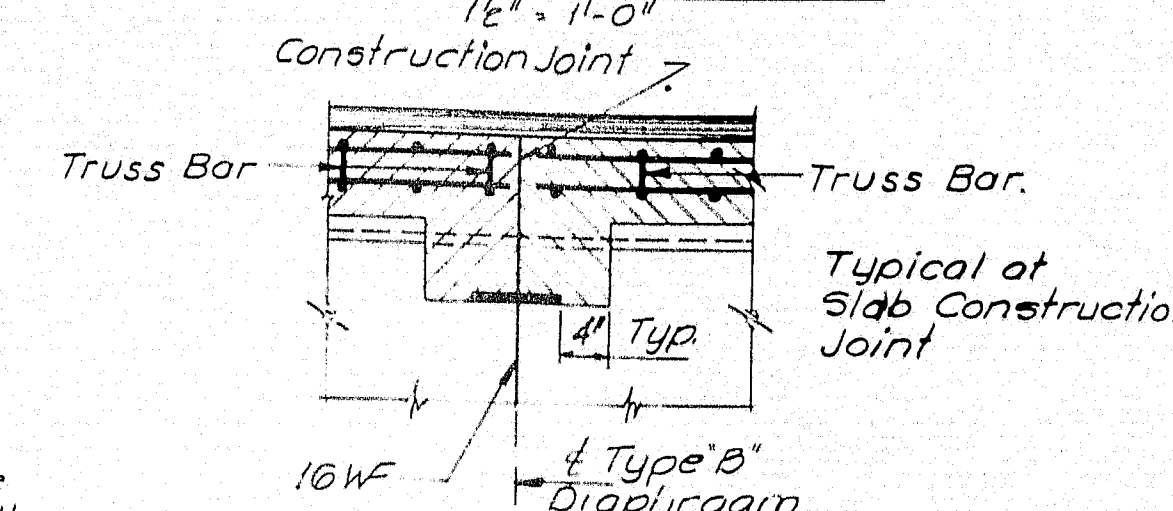
NOTE: Each panel starts with S505 & ends with S502 except at A<sub>3</sub>.

TYPICAL REINFORCING PATTERN

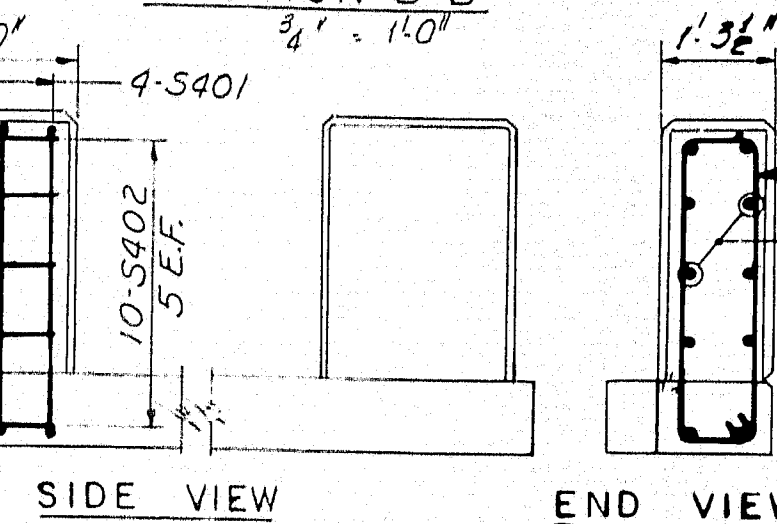
Sequence of Anchor Rod indicates Grouted into Granite.

Membrane Waterproofing (Shown with 2" Bituminous Concrete & 1" Allowed For Membrane Waterproofing) (By Others)

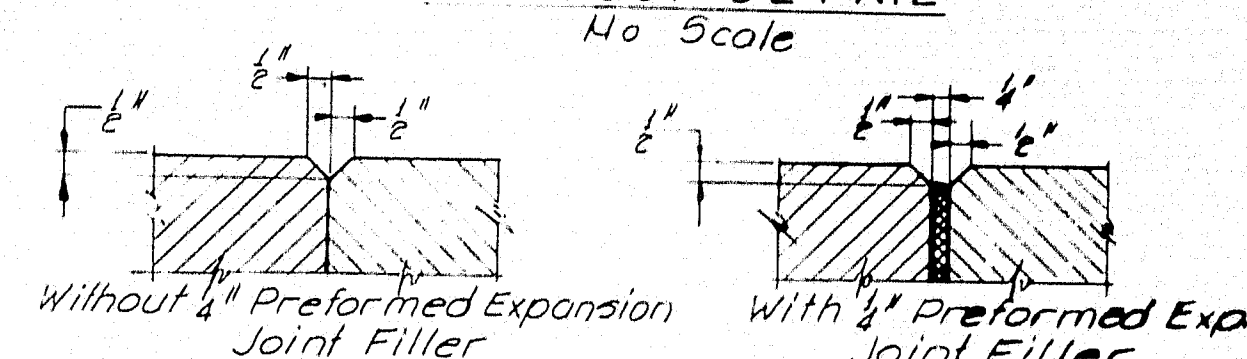
TYPICAL CURB SECTION



SECTION B-B



END POST DETAIL



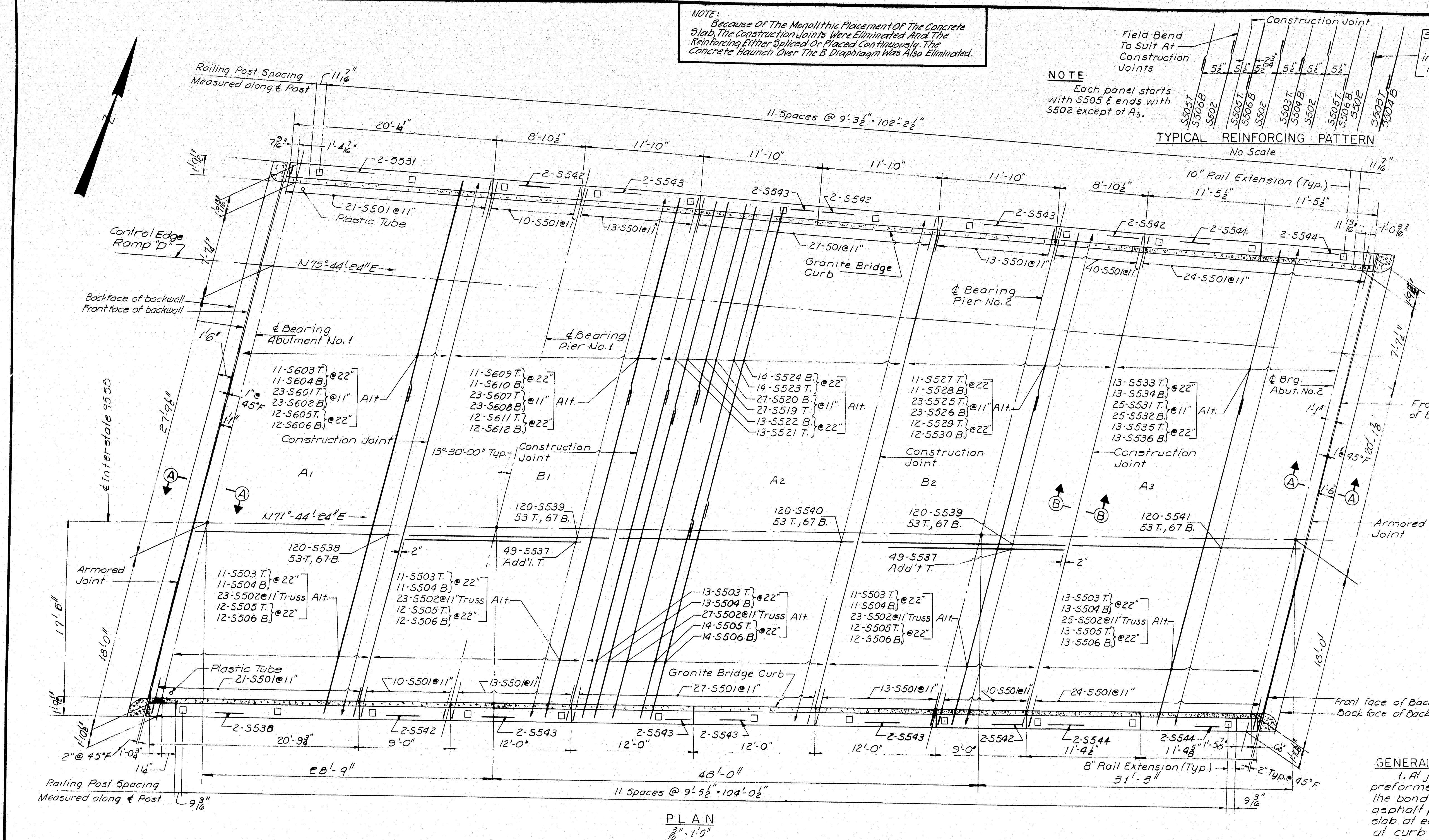
1" V-GROOVES

GENERAL SUPERSTRUCTURE NOTES:

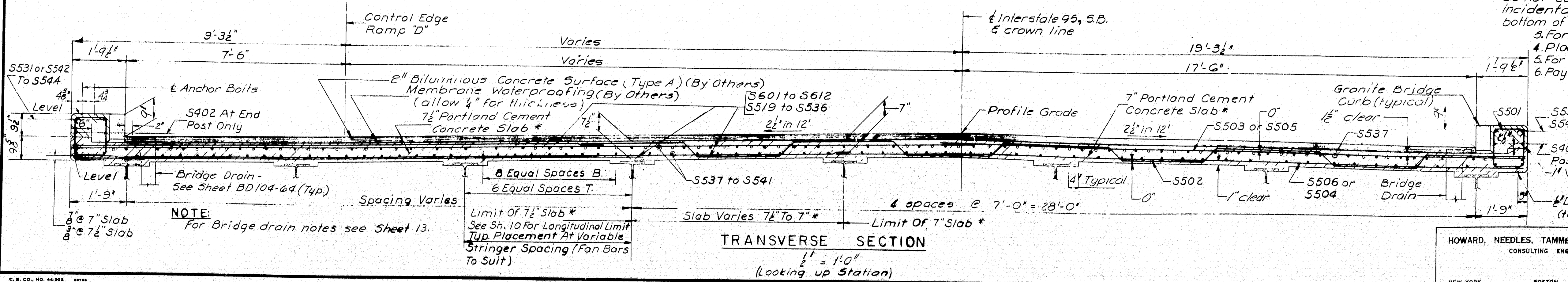
- At joints in curbs and granite bridge curbs over piers, use 1" preformed expansion joint filler. At all other curb joints, break the bond between concrete surfaces with a suitable grade of asphalt paint. Form 1/4" V-groove on outside face of curb and slab at each vertical joint. Provide joints in granite bridge curb at curb construction joints.
- At low points in slabs, place a plastic tube 1" through the slab for drainage. Exact location to be determined in the field. Do not cover the tube with waterproofing. This work will be incidental to contract items. Tubes shall extend 2" below bottom of slab. Place tubes to drip clear of bridge seat.
- For bridge rail, see Standard Detail 13, BD107-64, BD108-64.
- Place concrete in A panels before placing concrete in B panels.
- For Section A-A & Armored Joint Layout, see Sheet 13.
- Payment for concrete end posts shall be made under Item 701-40.

DESIGN - G.H. DETAIL - D.A.T.  
TRACE - V.A.V.  
BRIDGE NO. 95 S.B.  
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
INTERSTATE 95 S.B.  
OVER  
RELOCATED EAST BRANCH  
MATTAWAKEAG RIVER  
IN THE TOWN OF  
OAKFIELD  
AROSTOOK COUNTY  
SUPERSTRUCTURE  
SHEET 12 OF 16 AUGUSTA, MAINE FEBRUARY 1965  
DYER BROOK OAKFIELD (12)

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
NEW YORK BOSTON KANSAS CITY

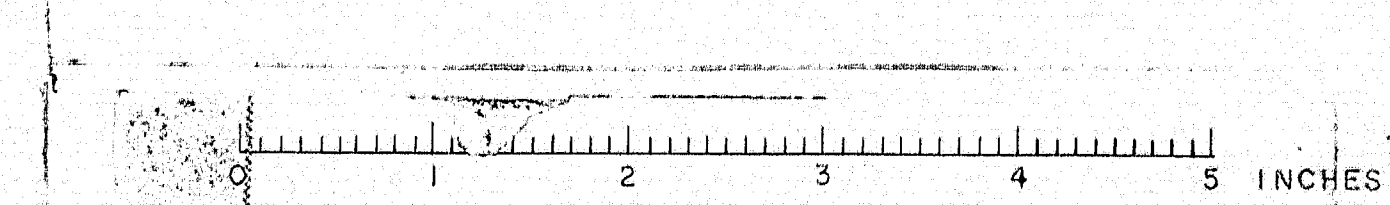


PLAN

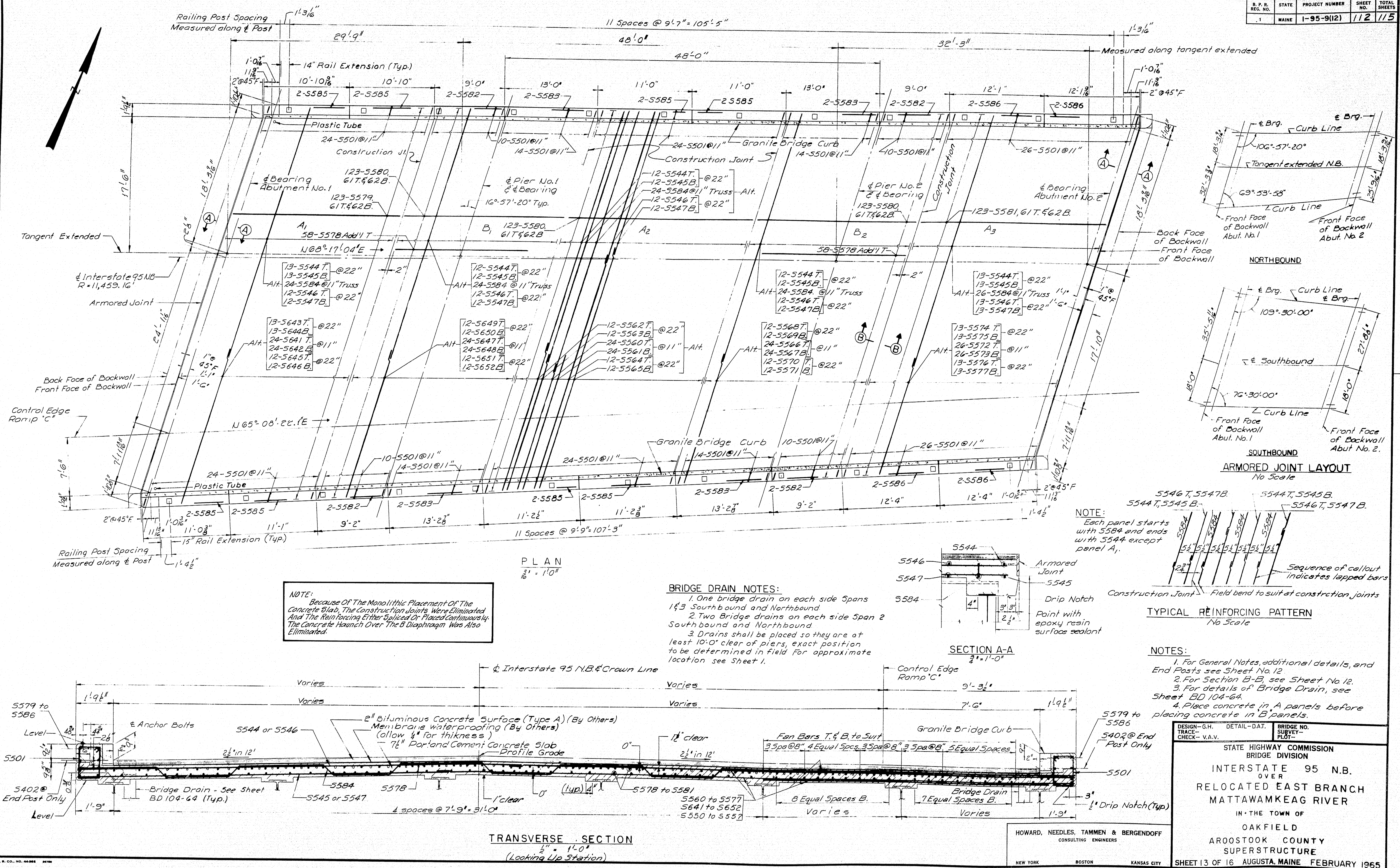


TRANSVERSE SECTION

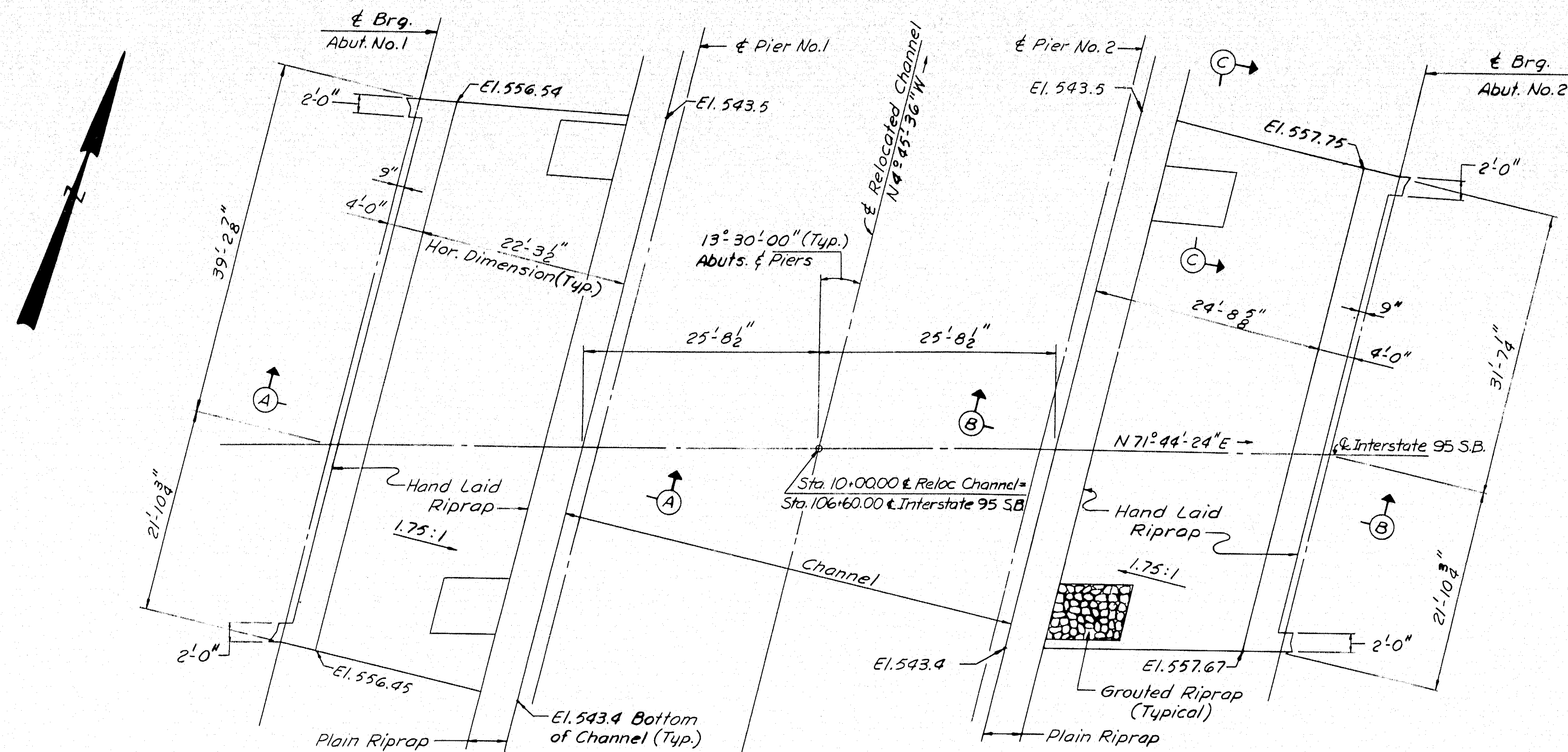
NOTE: For Bridge drain notes see Sheet 13.



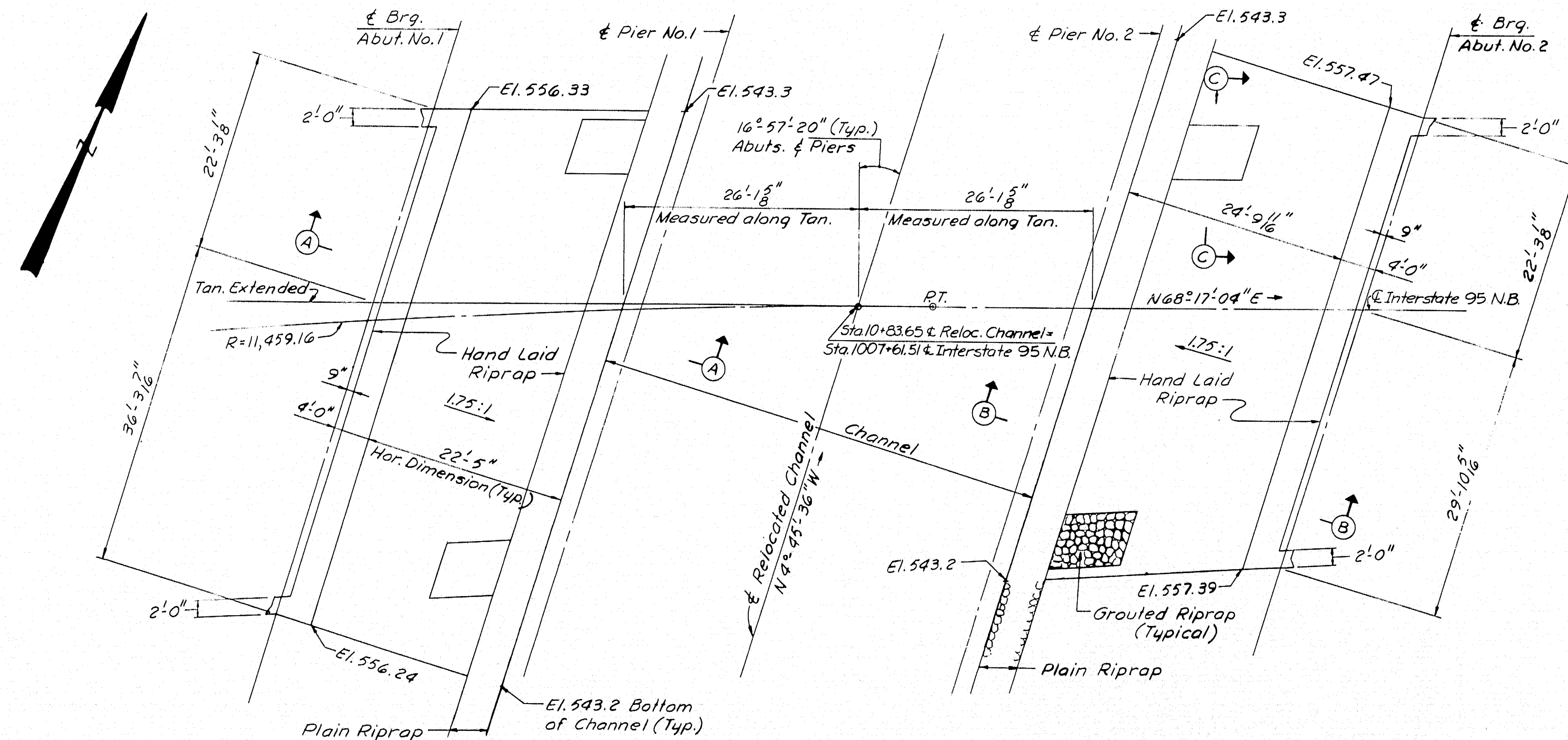






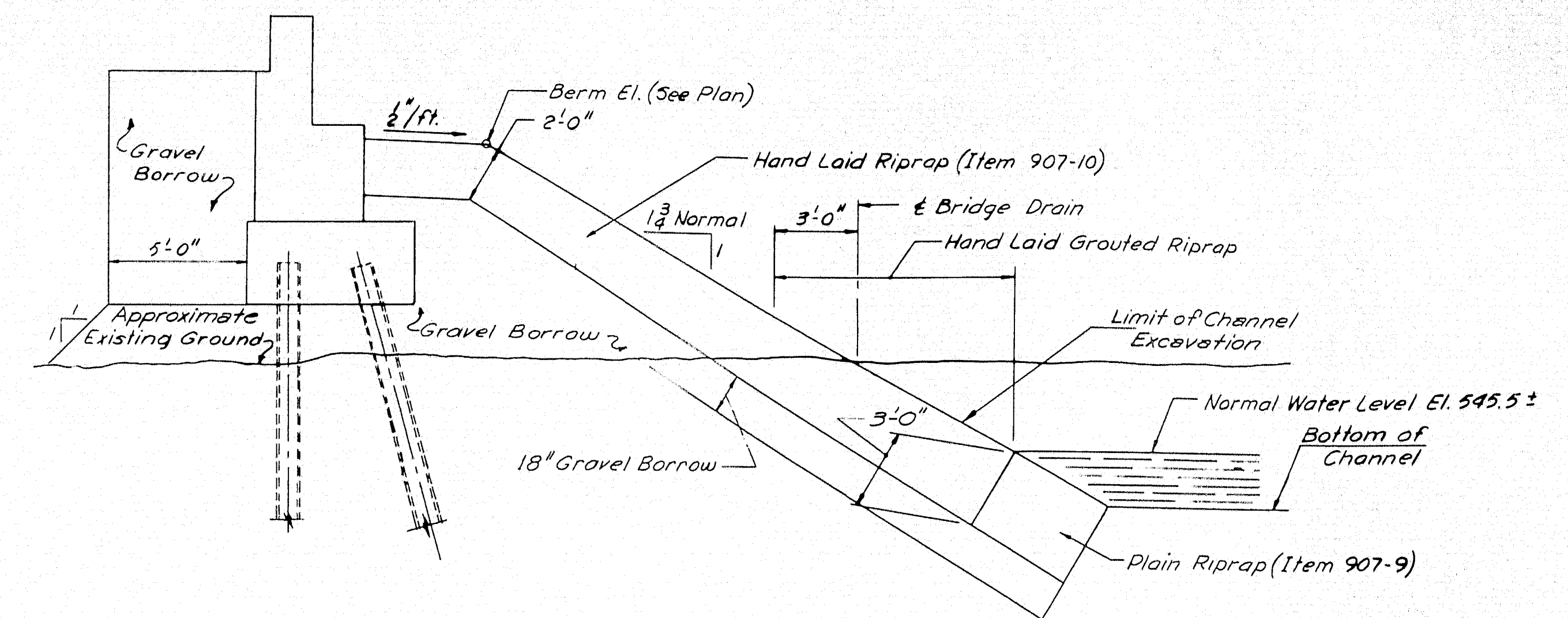


PLAN - SOUTHBOUND  
1" = 10'



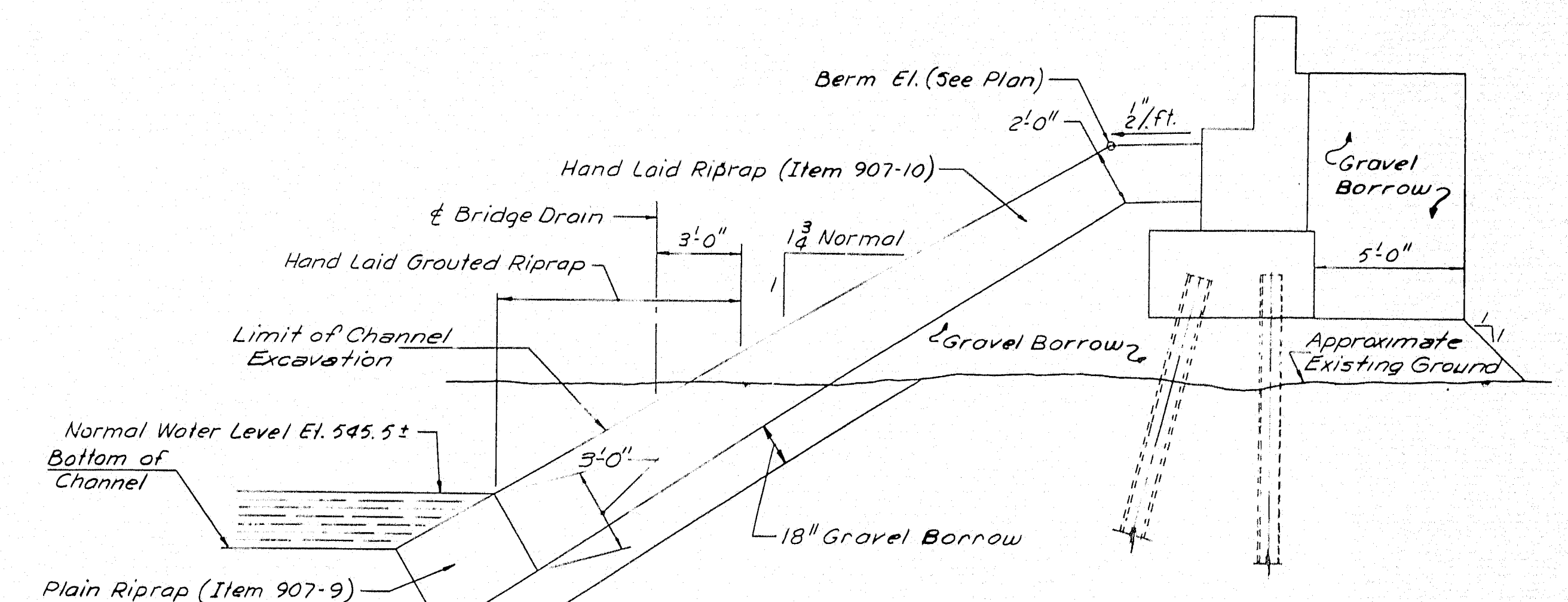
PLAN - NORTHBOUND  
1" = 10'

NOTE:  
For limits of Riprap see  
General Plan, Sheet 1.



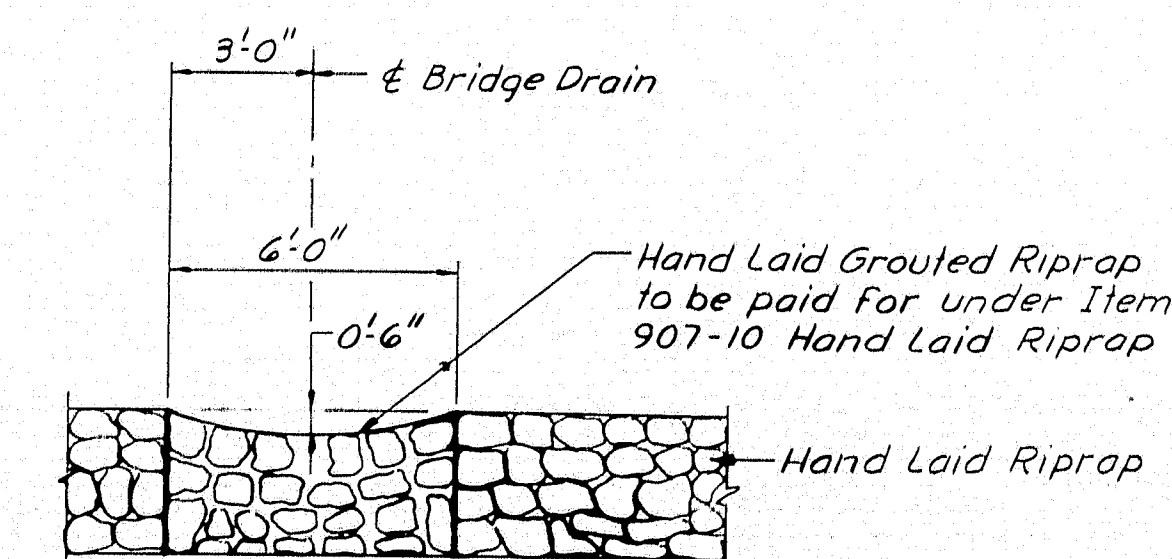
SECTION A-A  
1/4" = 1'0"

NOTE:  
Payment for any excavation required  
for slope protection will be made under  
Item for Structural Earth Excavation  
Piers Item 204-14.



SECTION B-B  
1/4" = 1'0"

NOTE:  
The 18" of Gravel Borrow under  
the Riprap may be reduced or omitted  
if existing material is suitable.

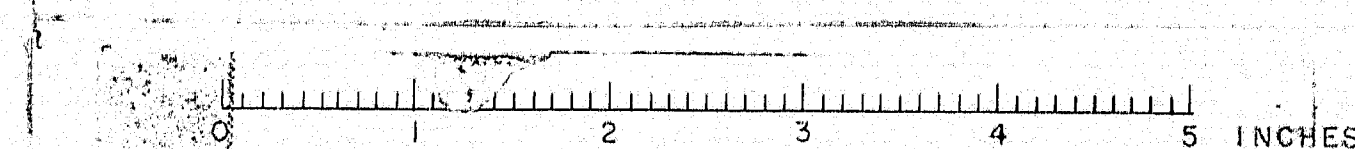


SECTION C-C  
1/4" = 1'0"

DESIGN - TRACE - CHECK - S.M.	DETAIL - J.W.M.	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
INTERSTATE 95 OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER IN THE TOWN OF OAKFIELD AROSTOOK COUNTY SLOPE PROTECTION		
SHEET 14 OF 16 AUGUSTA, MAINE FEBRUARY 1965		

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
NEW YORK BOSTON KANSAS CITY

M-2268 Dyer Brook Oakfield (12)

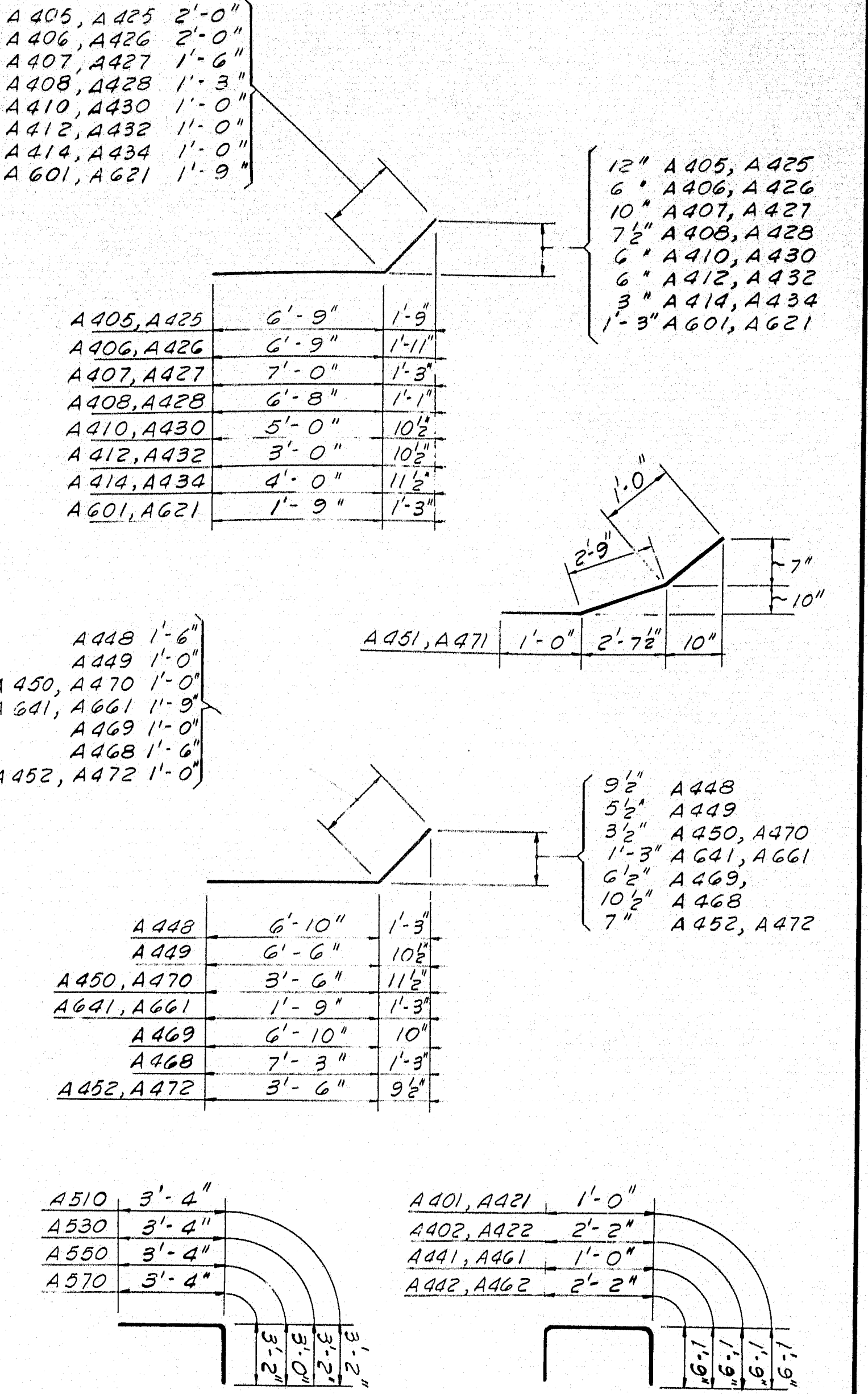




ABUTMENT 1 (Southbound)					LOCATION
MARK	SIZE	NUMBER	LENGTH	INCR.	
STRAIGHT BARS					
A403	4	12	30'-0"		Backwall
A404	4	8	8'-6"		Wingwall
A409	4	1	6'-0"		"
A411	4	1	4'-0"		"
A413	4	1	5'-0"		Wingwall
BENT BARS					
A501	5	16	28'-9"		Stem
A502	5	78	4'-0"		Backwall
A503	5	39	3'-0"		Stem & Backwall
A504	5	39	6'-0"		Footings & Stem
A505	5	71	2'-6"		Footings & Stem
A506	5	2	8'-1"		Wingwall
A507	5	2	8'-4"		"
A508	5	14	4'-4"		"
A509	5	14	5'-2"	7 1/2"	" 2 Groups of 7
			8'-4"	6 3/8"	Wingwall 2 Groups of 7
A602	6	110	5'-6"		Footings
A603	6	24	29'-0"		"
A604	6	10	10'-0"		"
A605	6	10	10'-6"		"
A606	6	16	3'-6"		"
			5'-6"	3 1/2"	" 2 Groups of 8
A607	6	18	3'-6"		"
A608	6	4	1'-0"	2 1/4"	Footings 2 Groups of 9
					Granite Curbs
BENT BARS					
A401	4	16	4'-6"		Pads
A402	4	16	5'-8"		Pads
A405	4	4	8'-9"		Wingwall
A406	4	4	8'-9"		"
A407	4	2	8'-6"		"
A408	4	2	7'-11"		"
A410	4	1	6'-0"		"
A412	4	1	4'-0"		"
A414	4	1	5'-0"		Wingwall
A510	5	39	6'-6"		Stem
A601	6	39	3'-6"		Approach Slab Seat
ABUTMENT 2 (Southbound)					
STRAIGHT BARS					
A423	4	12	26'-3"		Backwall
A424	4	8	8'-6"		Wingwall
A429	4	1	6'-0"		"
A431	4	1	4'-0"		"
A433	4	1	5'-0"		Wingwall
A521	5	16	25'-0"		Stem
A522	5	68	4'-0"		Backwall
A523	5	34	3'-0"		Stem & Backwall
A524	5	34	6'-0"		Footings & Stem
A525	5	66	2'-6"		Footings & Stem
A526	5	2	7'-10"		Wingwall
A527	5	2	7'-9"		"
A528	5	14	4'-4"		"
A529	5	14	7'-10"	7"	" 2 Groups of 7
			7'-9"	6"	Wingwall 2 Groups of 7
A622	6	96	5'-6"		Footings
A623	6	24	25'-3"		"
A624	6	10	10'-0"		"
A625	6	10	10'-6"		"
A626	6	16	3'-6"		"
			5'-6"	3 1/2"	" 2 Groups of 8
A627	6	18	3'-6"		"
A628	6	4	1'-0"	2 1/4"	Footings 2 Groups of 9
					Granite Curb

ABUTMENT 2 (Southbound) Cont.					LOCATION
MARK	SIZE	NUMBER	LENGTH	INCR.	
BENT BARS					
A421	4	16	4'-6"		Pads
A422	4	16	3'-8"		Pads
A425	4	4	8'-9"		Wingwall
A426	4	4	8'-9"		"
A427	4	2	8'-6"		"
A428	4	2	7'-11"		"
A430	4	1	6'-0"		"
A432	4	1	4'-0"		"
A434	4	1	5'-0"		Wingwall
A530	5	34	6'-6"		Stem
A621	6	34	3'-6"		Approach Slab Seat
ABUTMENT 1 (Northbound)					
STRAIGHT BARS					
A443	4	12	26'-0"		Backwall
A444	4	16	6'-9"		Wingwall
A445	4	2	5'-0"		"
A446	4	2	3'-0"		"
A447	4	2	4'-0"		Wingwall
A541	5	16	27'-6"		Stem
A542	5	74	4'-0"		Backwall
A543	5	37	3'-0"		Stem & Backwall
A544	5	37	6'-0"		Footings & Stem
A545	5	69	2'-6"		Footings & Stem
A546	5	2	8'-3"		Wingwall
A547	5	2	7'-11"		"
A548	5	14	4'-6"		"
			8'-3"	7 1/2"	" 2 Groups of 7
A549	5	14	4'-9"		"
			7'-11"	6 3/8"	Wingwall 2 Groups of 7
A642	6	104	5'-6"		Footings
A643	6	24	27'-9"		"
A644	6	10	11'-0"		"
A645	6	10	10'-0"		"
A646	6	18	3'-6"		"
			6'-0"	6 3/8"	" 2 Groups of 9
A647	6	16	3'-6"		"
			5'-0"	2 1/2"	Footings 2 Groups of 8
A648	6	4	1'-0"		"
BENT BARS					
A441	4	14	4'-6"		Pads
A442	4	14	5'-8"		Pads
A448	4	2	8'-4"		Wingwall
A449	4	2	7'-6"		Wingwall
A450	4	12	4'-6"		Backwall & Wingwall
A451	4	6	4'-9"		"
A452	4	6	4'-6"		Backwall & Wingwall
A550	5	37	6'-4"		Stem
A641	6	36	3'-6"		Approach Slab Seat
ABUTMENT 2 (Northbound)					
STRAIGHT BARS					
A463	4	12	22'-10"		Backwall
A464	4	16	6'-9"		Wingwall
A465	4	2	5'-0"		"
A466	4	2	3'-0"		"
A467	4	2	4'-0"		Wingwall
A561	5	16	24'-4"		Stem
A562	5	66	4'-0"		Backwall
A563	5	33	3'-0"		Stem & Backwall
A564	5	33	6'-2"		Footings & Stem
A565	5	65	2'-6"		Footings & Stem
A566	5	2	8'-1"		Wingwall
A567	5	2	8'-4"		Wingwall

ABUTMENT 2 (Northbound) Cont.					LOCATION
MARK	SIZE	NUMBER	LENGTH	INCR.	
STRAIGHT BARS					
A568	5	14	4'-4"		Pads
			8'-1"	7 1/2"	Wingwall 2 Groups of 7
A569	5	14	5'-3"		"
			8'-4"	6 1/2"	Wingwall 2 Groups of 7
A662	6	92	5'-6"		Footings
A663	6	24	24'-7"		"
A664	6	10	11'-0"		"
A665	6	10	10'-0"		"
A666	6	18	3'-6"		"
			6'-0"	3 1/2"	" 2 Groups of 9
A667	6	16	3'-6"		"
			5'-0"	2 1/2"	Footings 2 Groups of 8
A668	6	4	1'-0"		Granite Curb
BENT BARS					
A461	4	14	4'-6"		Pads
A462	4	14	5'-8"		Pads
A468	4	2	8'-9"		Wingwall
A469	4	2	7'-10"		Wingwall
A470	4	12	4'-6"		Backwall & Wingwall
A471	4	6	4'-9"		"
A472	4	6	4'-6"		Backwall & Wingwall
A570	5	33	6'-6"		Stem
A661	6	32	3'-6"		Approach Slab Seat
Approach Slabs					
A5401	4	44	27'-7"		Abutment 1 (Southbound)
A5601	6	212	14'-6"		Abutment 1 (Southbound)
A5421	4	44	23'-1"		Abutment 2 (Southbound)
A5621	6	178	14'-6"		Abutment 2 (Southbound)
A5441	4	44	25'-7"		Abutment 1 (Northbound)
A5641	6	204	14'-6"		Abutment 1 (Northbound)
A5461	4	44	22'-3"		Abutment 2 (Northbound)
A5661	6	176	14'-6"		Abutment 2 (Northbound)
PIER 1 (Southbound)					
STRAIGHT BARS					
P503	5	32	28'-9"		Stem Horizontal
P504	5	32	25'-6"		Stem Horizontal
P601	6	8	30'-0"		Cap
P602	6	8	25'-5"		Cap
P604	6	120	17'-8"		Stem Vertical
P605	6	120	4'-9"		Footings Dowels
P606	6	10	30'-0"		Footings Longitudinal
P607	6	10	31'-3"		Footings Longitudinal
P701	7	61	8'-6"		Footings Transverse
BENT BARS					
P401	4	28	9'-2"		Cap
P501	5	32	2'-8"		Nosing
P502	5	64	4'-4"		Nosing
			5'-3"	3 1/4"	Nosing

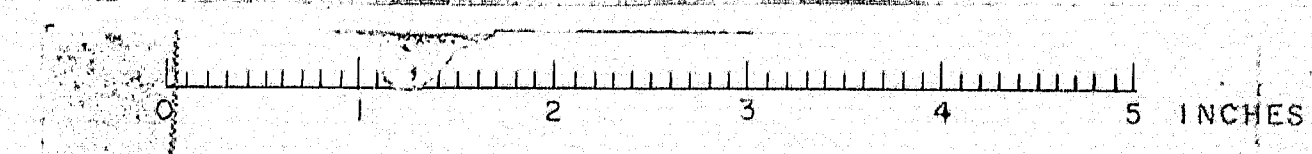


NOTES:  
1. All dimensions are to center of bars.  
2. All reinforcing bars shall be intermediate grade steel.  
3. Reinforcing steel to have 2" minimum cover unless otherwise shown.

DESIGN - G.H. DETAIL - J.R.A. BRIDGE NO. 114  
TRACE - CHECK - P.R.N. SURVEY - PLOT  
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
INTERSTATE 95  
OVER  
RELOCATED EAST BRANCH  
MATTAWAMKEAG RIVER  
IN THE TOWN OF  
OAKFIELD  
ARROOSTOOK COUNTY  
REINFORCING STEEL  
SHEET 15 OF 16 AUGUSTA, MAINE FEBRUARY 1965

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
NEW YORK BOSTON KANSAS CITY

M-2269 DYE BROOK OAKFIELD(12)

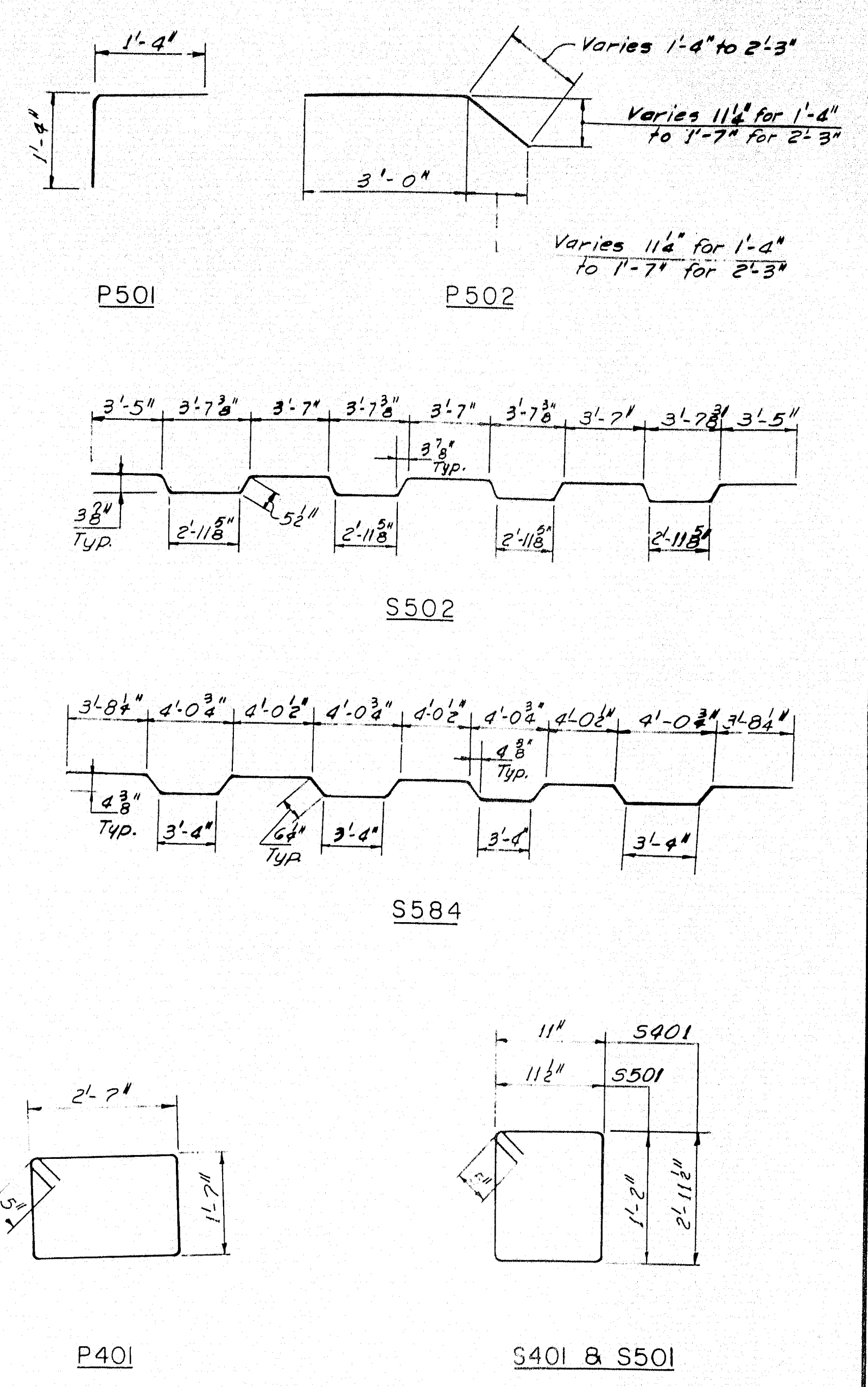




PIER 2 (Southbound)				
MARK	SIZE	NUMBER	LENGTH	INCR. LOCATION
STRAIGHT BARS				
P503	5	32	26'-9"	Stem Horizontal
P505	5	32	22'-2"	Stem Horizontal
P601	6	8	30'-0"	Cap
P603	6	8	21'-11"	Cap
P604	6	112	17'-8"	Stem Vertical
P605	6	112	4'-9"	Footing Dowels
P606	6	10	30'-0"	Footing Longitudinal
P608	6	10	27'-9"	Footing Longitudinal
P701	7	58	8'-6"	Footing Transverse
BENT BARS				
P401	4	26	9'-2"	Cap
P501	5	32	2'-8"	Nosing
P502	5	64	4'-4" to 5'-3"	Nosing
PIER 1 (Northbound)				
STRAIGHT BARS				
P506	5	32	25'-6"	Stem Horizontal
P507	5	32	21'-3"	Stem Horizontal
P601	6	8	30'-0"	Cap
P605	6	108	4'-9"	Footing Dowels
P606	6	10	30'-0"	Footing Longitudinal
P609	6	108	17'-3"	Stem Vertical
P610	6	10	28'-9"	Footing Longitudinal
P611	6	8	22'-11"	Cap
P701	7	59	8'-6"	Footing Transverse
BENT BARS				
P401	4	27	9'-2"	Cap
P501	5	32	2'-8"	Nosing
P502	5	64	4'-4" to 5'-3"	Nosing
PIER 2 (Northbound)				
STRAIGHT BARS				
P506	5	32	25'-6"	Stem Horizontal
P508	5	32	18'-5"	Stem Horizontal
P601	6	8	30'-0"	Cap
P605	6	102	4'-9"	Footing Dowels
P606	6	10	30'-0"	Footing Longitudinal
P608	6	10	27'-9"	Footing Longitudinal
P609	6	102	17'-3"	Stem Vertical
P610	6	8	19'-11"	Cap
P701	7	58	8'-6"	Footing Transverse
BENT BARS				
P401	4	25	9'-2"	Cap
P501	5	32	2'-8"	Nosing
P502	5	64	4'-4" to 5'-3"	Nosing

SUPERSTRUCTURE (Southbound)				
MARK	SIZE	NUMBER	LENGTH	INCR. LOCATION
STRAIGHT BARS				
S402	4	40	1'-8"	End Post
S503	5	59	35'-2"	Deck Transverse
S504	5	59	31'-0"	"
S505	5	63	20'-2"	"
S506	5	63	23'-9"	Deck Transverse
Bars S507 to S518 Not used				
S519	5	27	23'-0"	Deck Transverse
S520	5	27	23'-7"	"
S521	5	13	20'-5"	"
S522	5	13	24'-1"	"
S523	5	14	34'-11"	"
S524	5	14	31'-4"	"
S525	5	23	21'-3"	"
S526	5	23	21'-10"	"
S527	5	11	19'-11"	"
S528	5	11	22'-4"	"
S529	5	12	33'-2"	"
S530	5	12	29'-7"	"
S531	5	27	19'-9"	"
S532	5	25	20'-4"	"
S533	5	13	17'-10"	"
S534	5	13	20'-10"	"
S535	5	13	31'-8"	"
S536	5	13	28'-1"	Deck Transverse
S537	5	98	17'-8"	Deck Longitudinal
S538	5	122	20'-4"	"
S539	5	240	20'-8"	"
S540	5	120	23'-8"	"
S541	5	120	22'-10"	"
S542	5	8	8'-8"	Deck Longitudinal
S543	5	24	11'-8"	Safety Walk
S544	5	8	11'-0"	Safety Walk
S601	6	23	25'-10"	Deck Transverse
S602	6	23	26'-5"	"
S603	6	11	22'-10"	"
S604	6	11	26'-11"	"
S605	6	12	37'-9"	"
S606	6	12	34'-2"	"
S607	6	23	24'-5"	"
S608	6	23	25'-0"	"
S609	6	11	21'-8"	"
S610	6	11	25'-6"	"
S611	6	12	36'-4"	"
S612	6	12	32'-9"	Deck Transverse
BENT BARS				
S401	4	16	8'-7"	End Post
S501	5	236	5'-1"	Safety Walk
S502	5	121	33'-1 1/2"	Deck Transverse
SUPERSTRUCTURE (Northbound)				
STRAIGHT BARS				
S402	4	40	1'-8"	End Post
S544	5	62	30'-7"	Deck Transverse
S545	5	62	34'-8"	"
S546	5	61	28'-6"	"
S547	5	61	18'-5"	Deck Transverse
Bars S548 to S559 Not used				
S560	5	24	16'-11"	Deck Transverse
S561	5	24	17'-6"	"
S562	5	12	22'-1"	"
S563	5	12	18'-0"	"
S564	5	12	30'-2"	"
S565	5	12	34'-2"	"
S566	5	24	15'-8"	"
S567	5	24	16'-3"	"
S568	5	12	20'-10"	Deck Transverse

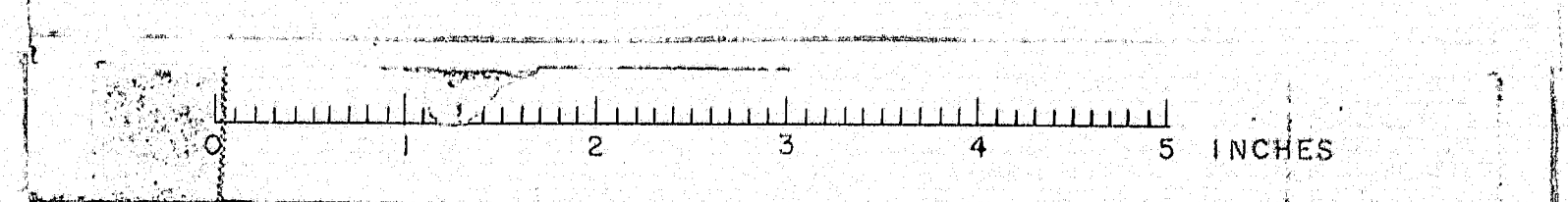
SUPERSTRUCTURE (Northbound) Continued				
MARK	SIZE	NUMBER	LENGTH	INCR. LOCATION
STRAIGHT BARS				
S569	5	12	16'-9"	Deck Transverse
S570	5	12	28'-11"	"
S571	5	12	32'-11"	"
S572	5	26	14'-5"	"
S573	5	26	15'-0"	"
S574	5	13	19'-7"	"
S575	5	13	15'-6"	"
S576	5	13	27'-8"	"
S577	5	13	30'-8"	Deck Transverse
S578	5	116	17'-6"	Deck Longitudinal
S579	5	123	21'-4"	"
S580	5	369	21'-8"	"
S581	5	123	23'-10"	Deck Longitudinal
S582	5	8	8'-8"	Safety Walk
S583	5	8	12'-8"	"
S585	5	16	10'-6"	"
S586	5	8	11'-9"	Safety Walk
S641	6	24	19'-5"	Deck Transverse
S642	6	24	20'-0"	"
S643	6	13	24'-7"	"
S644	6	13	20'-6"	"
S645	6	12	32'-8"	"
S646	6	12	36'-8"	"
S647	6	24	18'-2"	"
S648	6	24	18'-9"	"
S649	6	12	23'-4"	"
S650	6	12	19'-3"	"
S651	6	12	31'-5"	"
S652	6	12	35'-5"	Deck Transverse
BENT BARS				
S401	4	16	8'-7"	End Post
S501	5	244	5'-1"	Safety Walk
S584	5	122	37'-0"	Deck Transverse



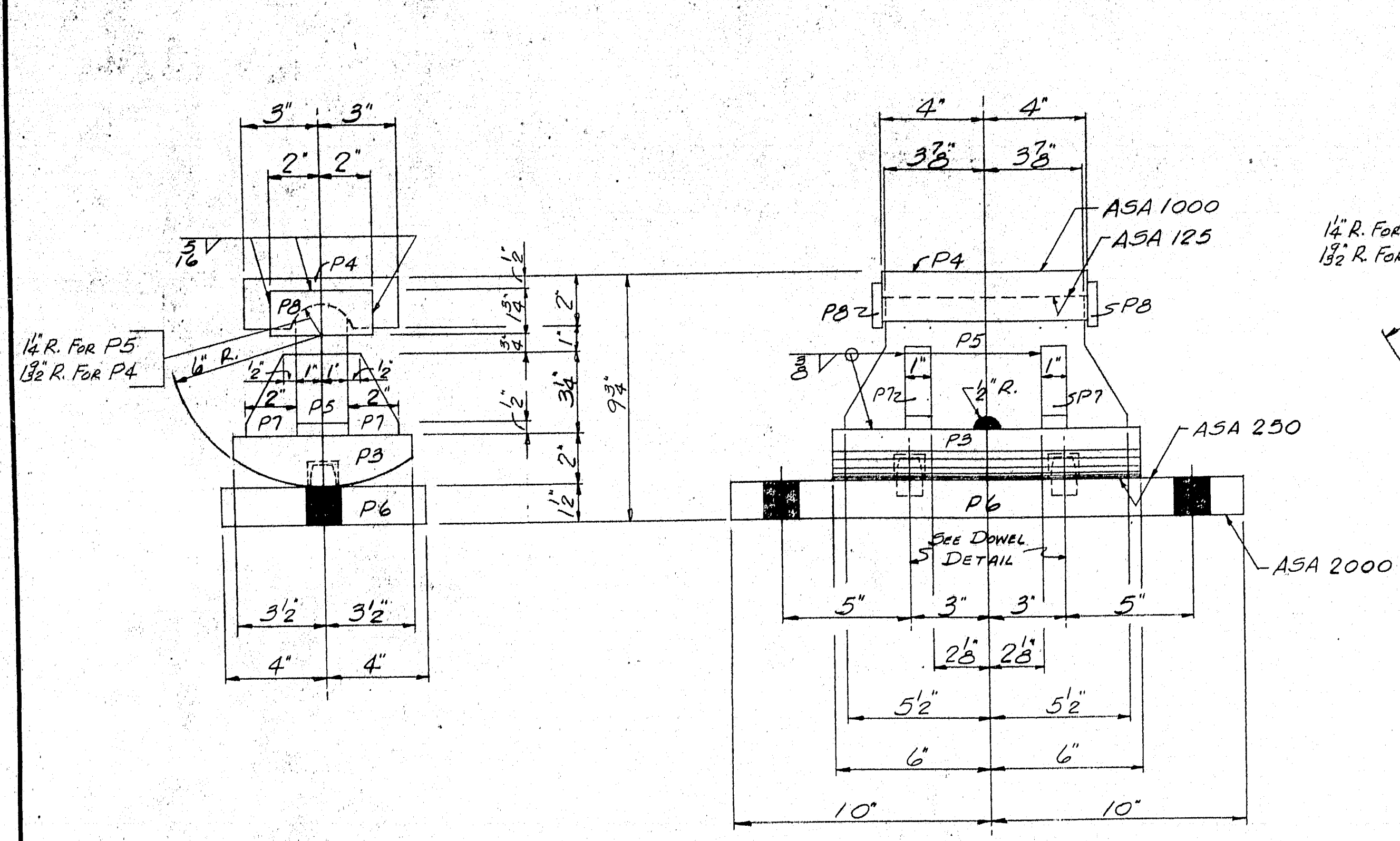
- NOTES:
1. All dimensions are to c. of bars.
  2. All reinforcing bars shall be intermediate grade steel.
  3. Reinforcing steel to have 2" minimum cover unless otherwise shown.

DESIGN - G.H. DETAIL - J.R.A. CHECK - P.R.N.	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER RELOCATED EAST BRANCH MATTAWAMKEAG RIVER IN THE TOWN OF OAKFIELD ARROSTOOK COUNTY REINFORCING STEEL	
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS NEW YORK BOSTON KANSAS CITY	SHEET 16 OF 16 AUGUSTA, MAINE FEBRUARY 1965

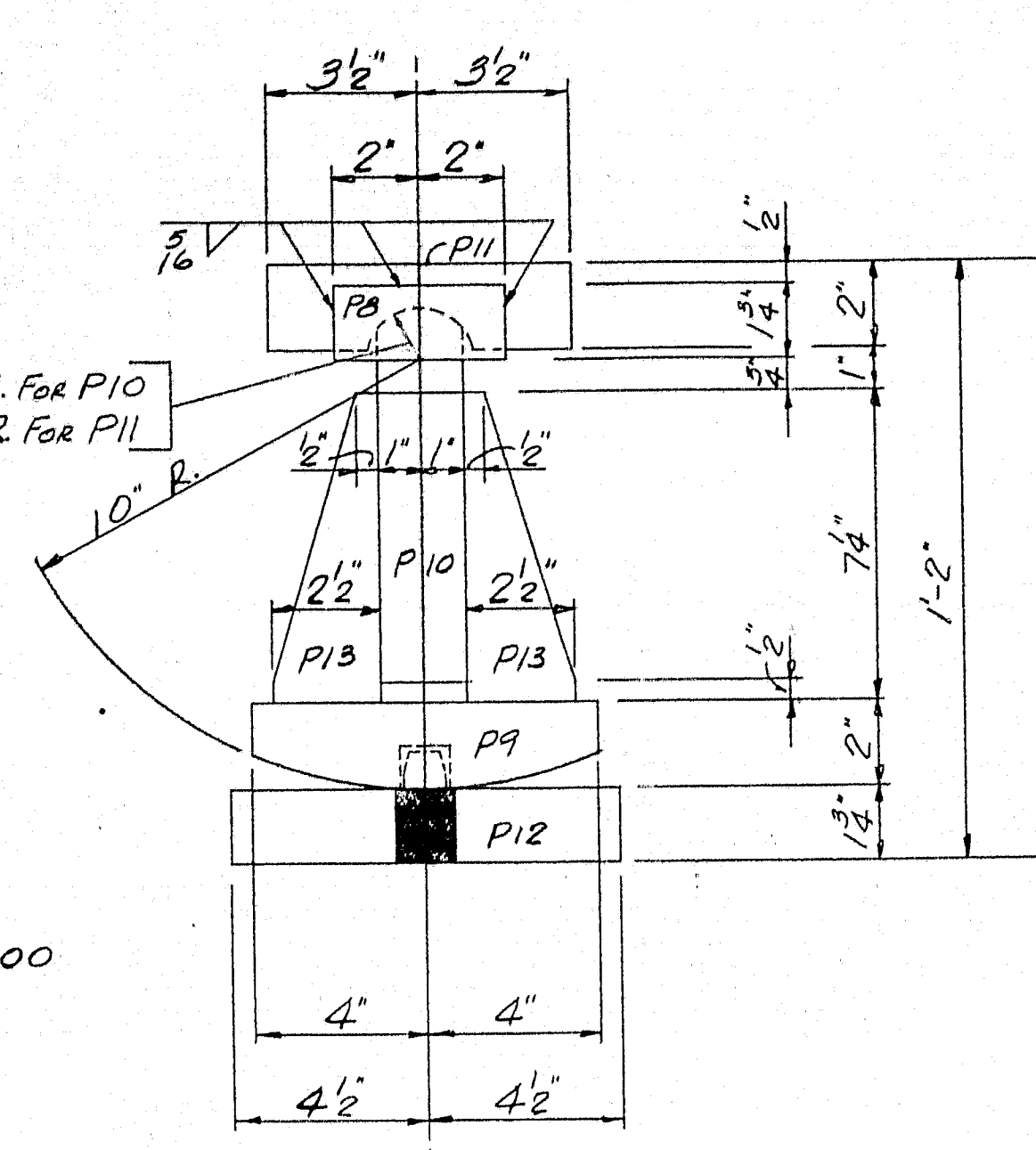
M-2270 DYER BROOK OAKFIELD (12)



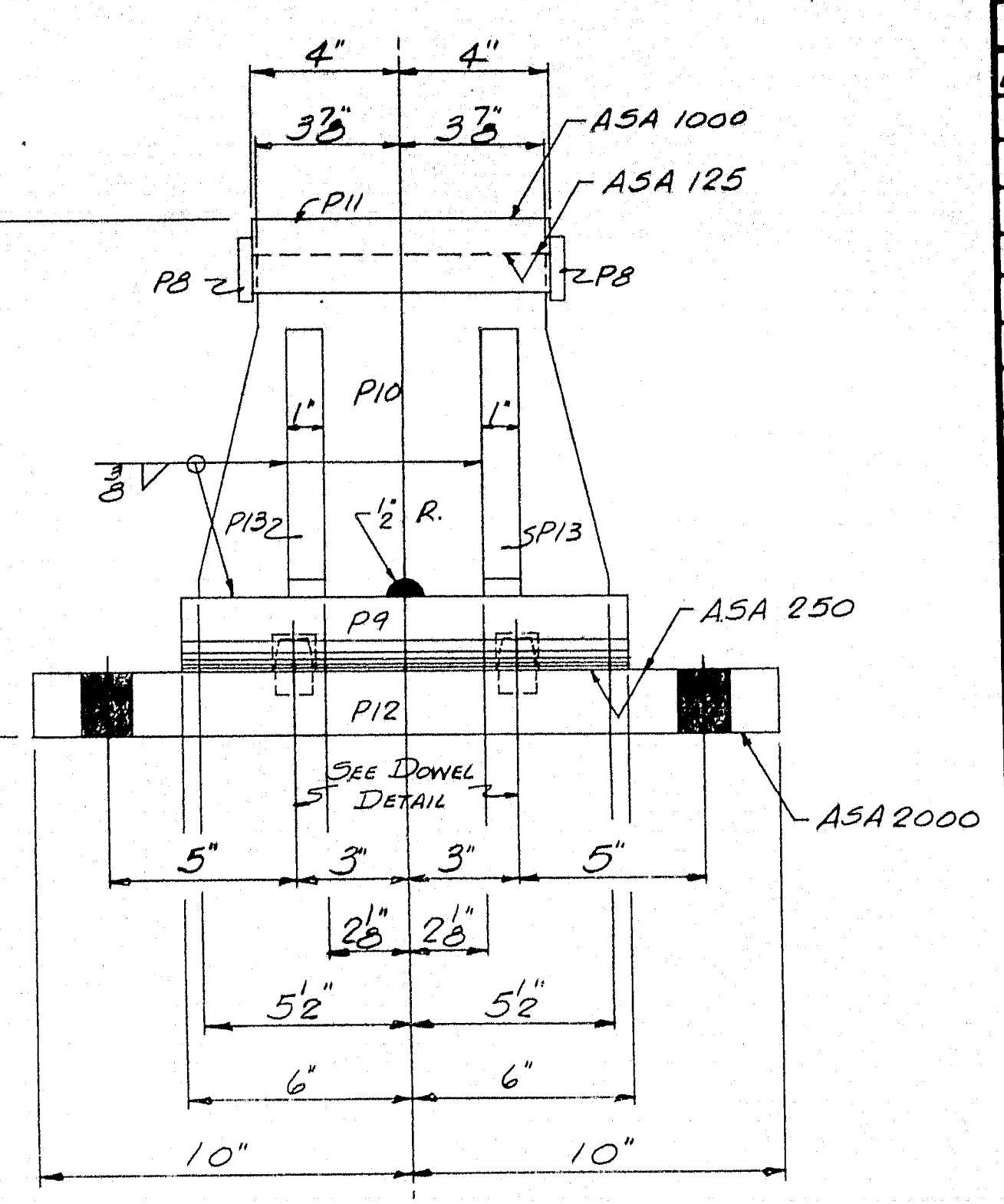




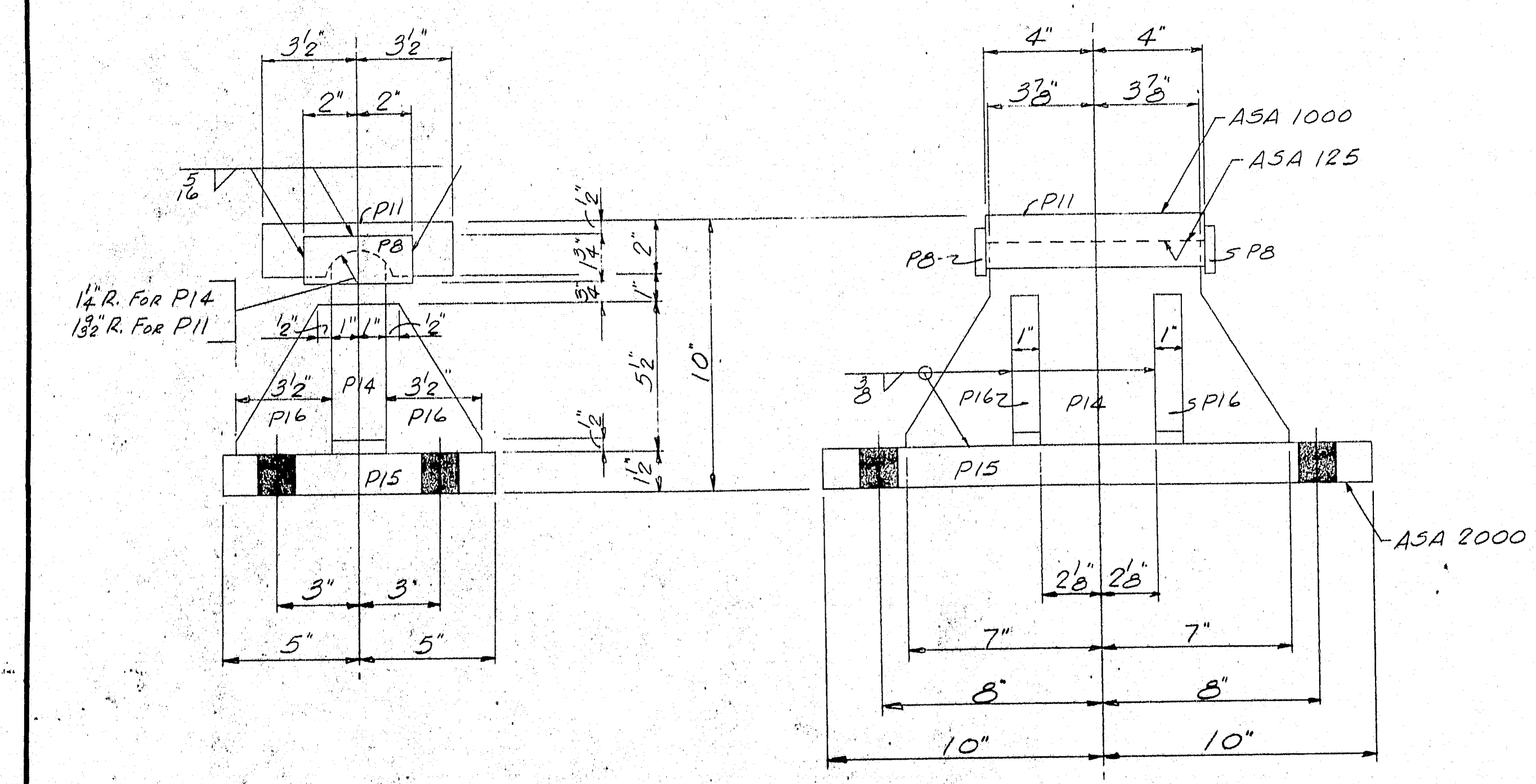
14 - EPC-1 REQ'D.



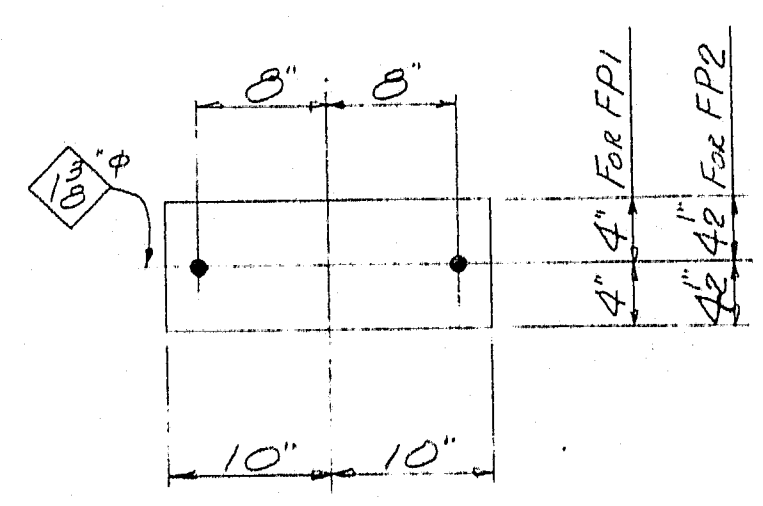
7 - EPC-3 REQ'D.



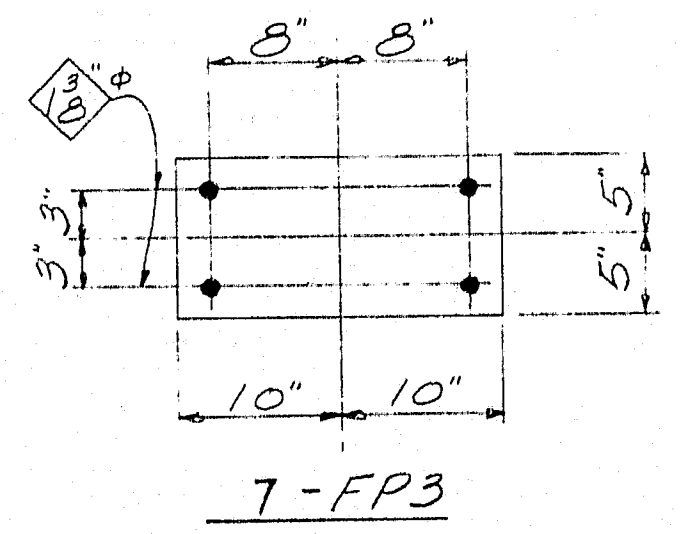
SHIP		BILL OF MATERIAL				DWG. 065-135-52
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
EPC1	14		PEDESTAL			
	14	P3	R-7x2	1 0		A36
	14	P4	R-6x2	0 8		
	14	P5	R-5x1/2	0 11		
	14	P6	R-8x1/2	1 8		
	56	P7	R-2x1	0 34		
	28	P8	R-1 1/4x3	0 4		
EPC3	7		PEDESTAL			
	7	P9	R-8x2	1 0		A36
	7	P10	R-9x2	0 11		
	7	P11	R-7x2	0 8		
	7	P12	R-9x1 1/2	1 8		
	28	P13	R-2 1/2x1	0 74		
	14	P8	R-1 1/4x3	0 4		
EPC2	7		PEDESTAL			
	7	P14	R-7 1/2x2	1 2		A36
	7	P11	R-7x2	0 8		
	7	P15	R-10x1/2	1 8		
	28	P16	R-3 1/2x1	0 52		
	14	P8	R-1 1/4x3	0 4		
	42	R1	ROD-1"φ	0 1/2		A36
FP1	14		8x3	1 8		FABCO PAD SA47
FP2	7		9x3	1 8		Do Do
FP3	7		10x3	1 8		Do Do
AB1	70		ROD-1"φ	1 3		THREADED & SWEDGED A36
	140		1" HEX NUT			REQ. # 3881
	70		1" STD. WASHER			



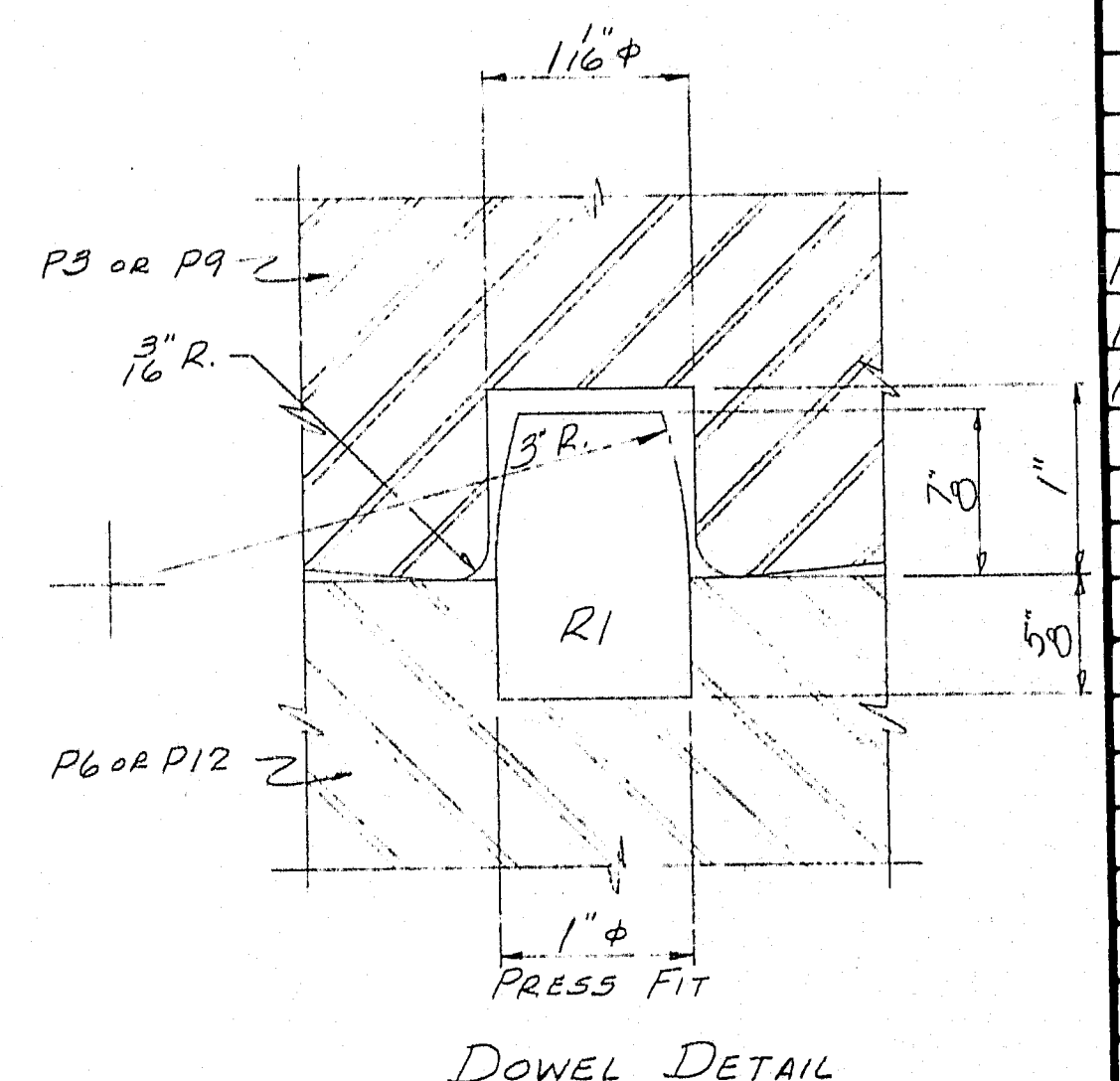
7 - FPC-2 REQ'D.



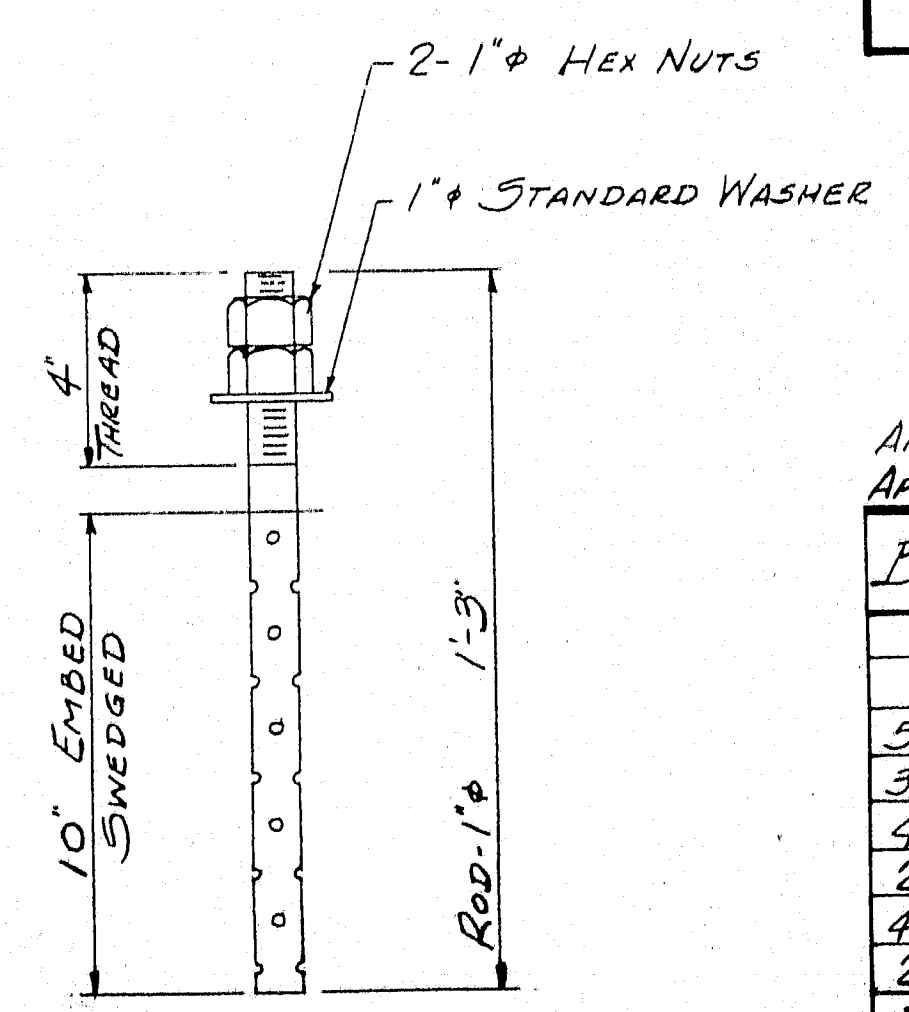
14 - FPC-1  
7 - FPC-2



7 - FPC-3



DOWEL DETAIL



70-AB1

PAINT NOTES  
NO PAINT ON ANCHOR BOLTS - OIL THREADS. NO PAINT ON TOP SURFACE & 1" DOWN FROM TOP ON SIDES OF SOLE PLATES. COAT WITH BOILED LINSEED OIL. EXAMINE ONCHORDS, JOINTS, AND SURFACES FOR CORROSION. IF CORROSION IS FOUND, REMOVE IT AND REPAIR. ONE SHOP COAT PAINT ON SURFACES FINISHED ASA 250. NO PAINT ON SURFACES ASA 125, COAT WITH HOT MIXTURE OF WHITE LEAD & TALLOW.

WELD WITH LH-ET02B OR LH-E602B OR 5AN-18 PREHEAT  
SHOP CONNECTIONS: 1" TO 2" THK. MAT. TO 50' F.  
FIELD CONNECTIONS: BOLT & WELD  
HOLES: 1/8" U.N.  
PAINT: STATE OF MAINE SPECS. & SEE PAINT NOTE THIS SHEET  
PROJ. NO. I-95-9(12)

APPRO. AS NOTED 8-10-65  
APP'D. 7-29-65

BEARING PEDESTALS & ANCHOR BOLTS NORTHBOUND

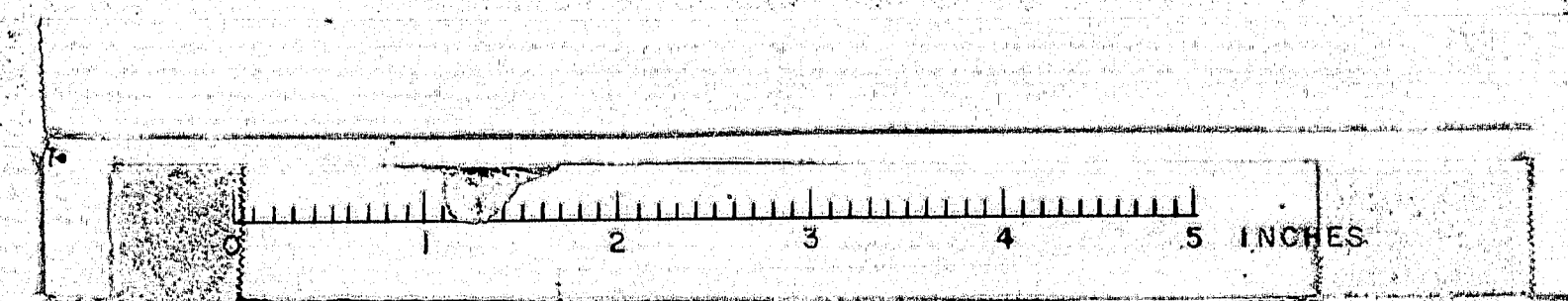
PRINT ISSUE

5 S.H.C. 9-10-65  
3 CUST. 9-19-65  
4 PORT. 9-18-65  
2 F.A. 8-6-65  
4 PORT. 7-28-65  
2 F.A. 7-28-65

DRAWN 7-28-65 R.A.M.  
REVISION 8-4-65 R.A.M.  
REVISION

Bancroft & Martin Inc.  
Brewer, Maine  
I-95 OVER E. BRANCH MATTAUMKEAS RIVER  
OAKFIELD, MAINE  
CUSTOMER CIANCHETTE BROS. INC.  
DESIGNER M.S.H.C. BRIDGE DIVISION  
ORDER VERBAL DWG. 065-135-52

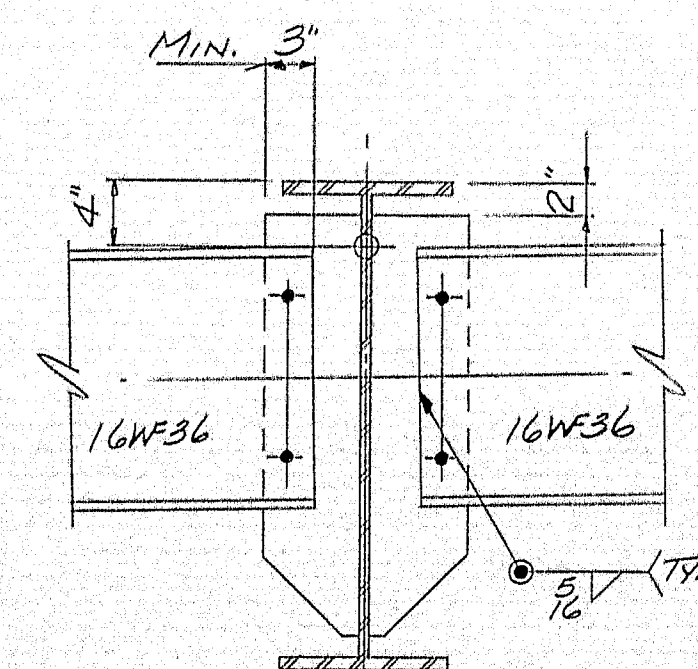
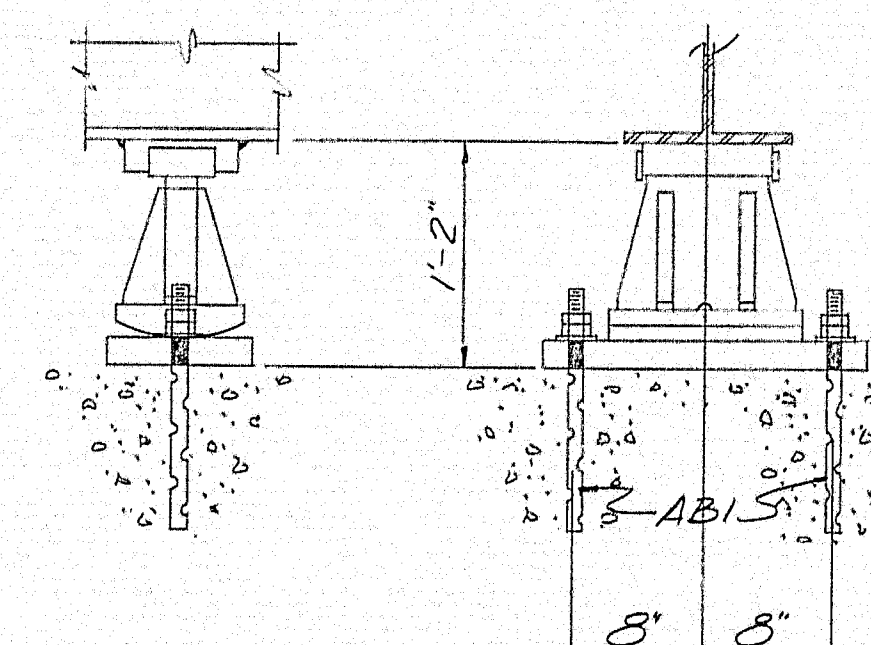
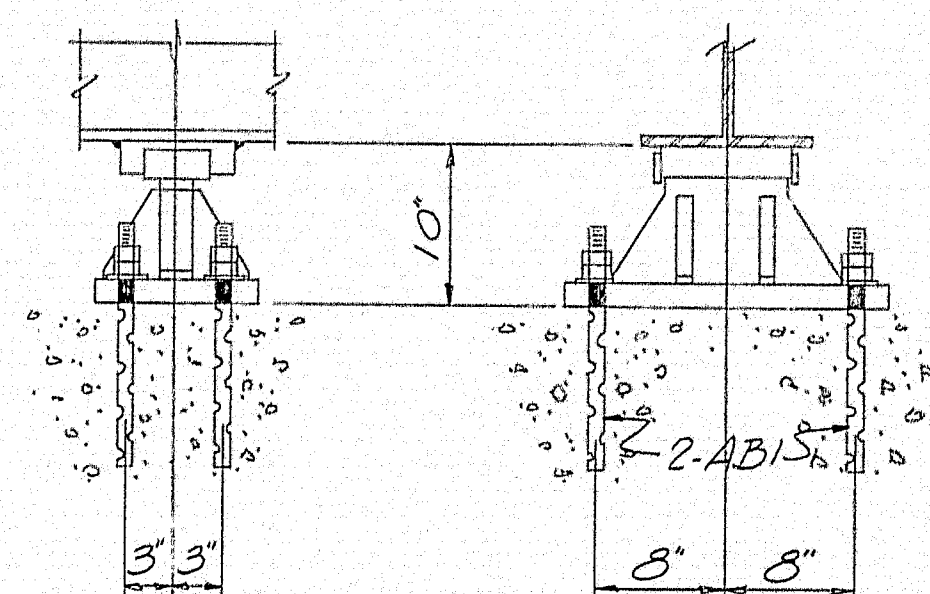
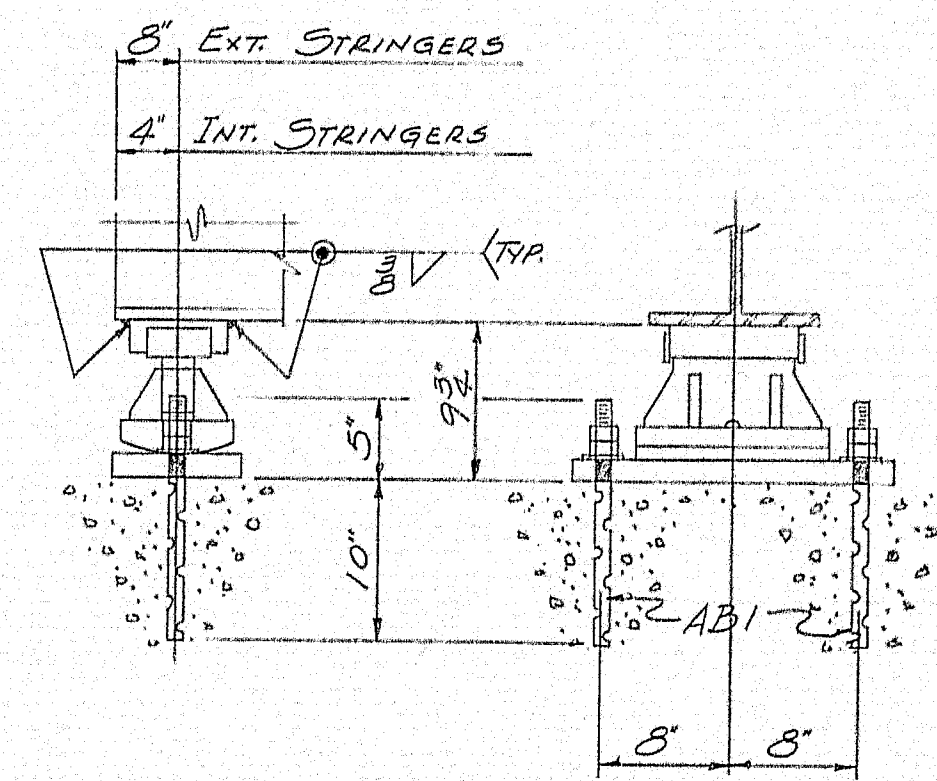
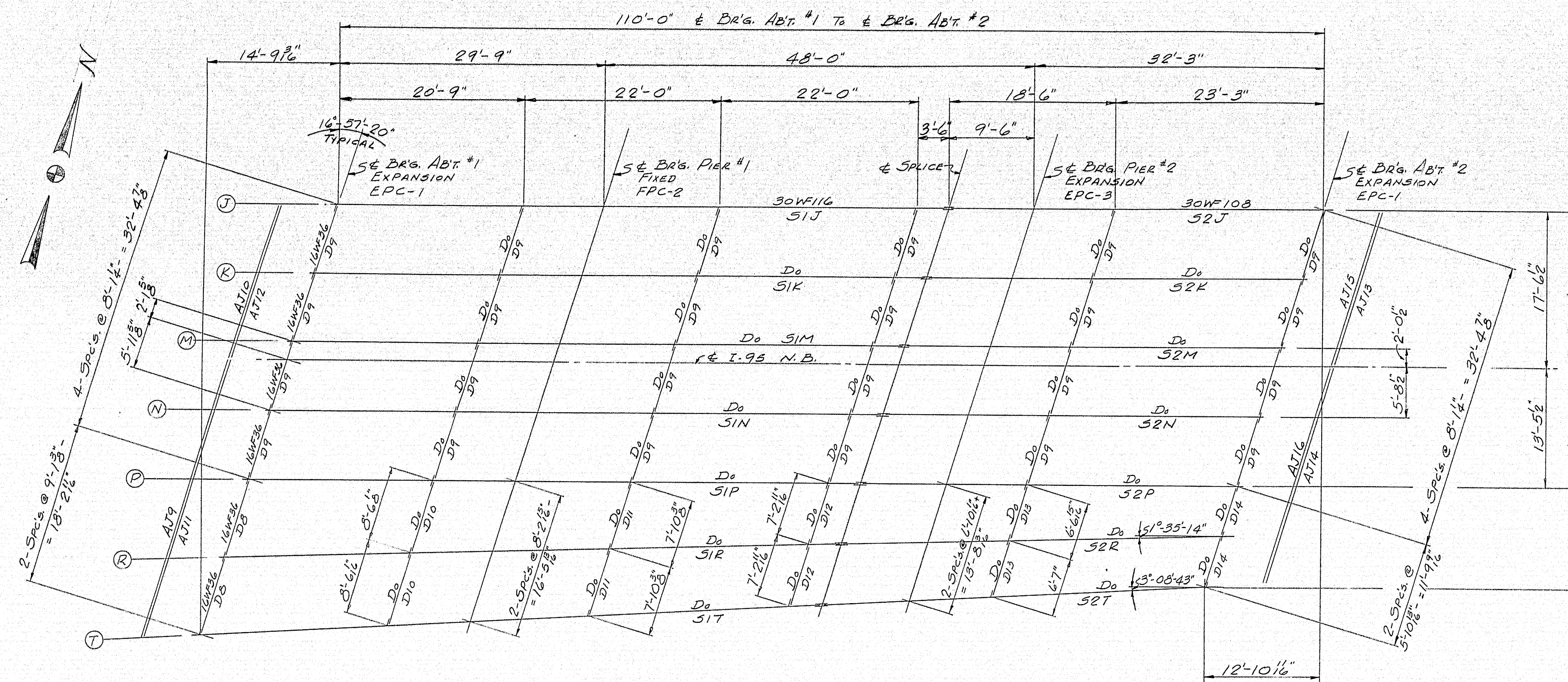












## NOTES

ALL STRUCTURAL STEEL SHALL CONFORM TO THE LATEST  
REVISION OF THE SPECIFICATIONS ASTM A36 UNLESS  
OTHERWISE NOTED.

FIELD BOLTS SHALL BE ASTM A325 7/8" HEAVY HEXAGON  
STRUCTURAL BOLTS WITH ONE HEAVY SEMI-FINISHED NUT  
& ONE HARDENED WASHER.

FABRICATE & ERECT IN ACCORDANCE WITH MSHC STANDARD SPECIFICATIONS.

FIELD TO INSTALL 5/8"Ø ROUND HEAD CARRIAGE BOLTS IN  
FLEMING BRACKET HOLES WITH HEAD ON EXTERIOR SIDE OF  
STRINGER.

SHOP CONNECTIONS: LH-E7018 or LH-E7028

FIELD CONNECTIONS: 7"  $\phi$  H.S. BOLTS & WELD

HOLES: 15"  $\phi$   
16"  $\phi$

PAINT: STATE OF MAINE SPEC'S

APP'D. 8-25-65

PROJ. No. I-95-9(12)

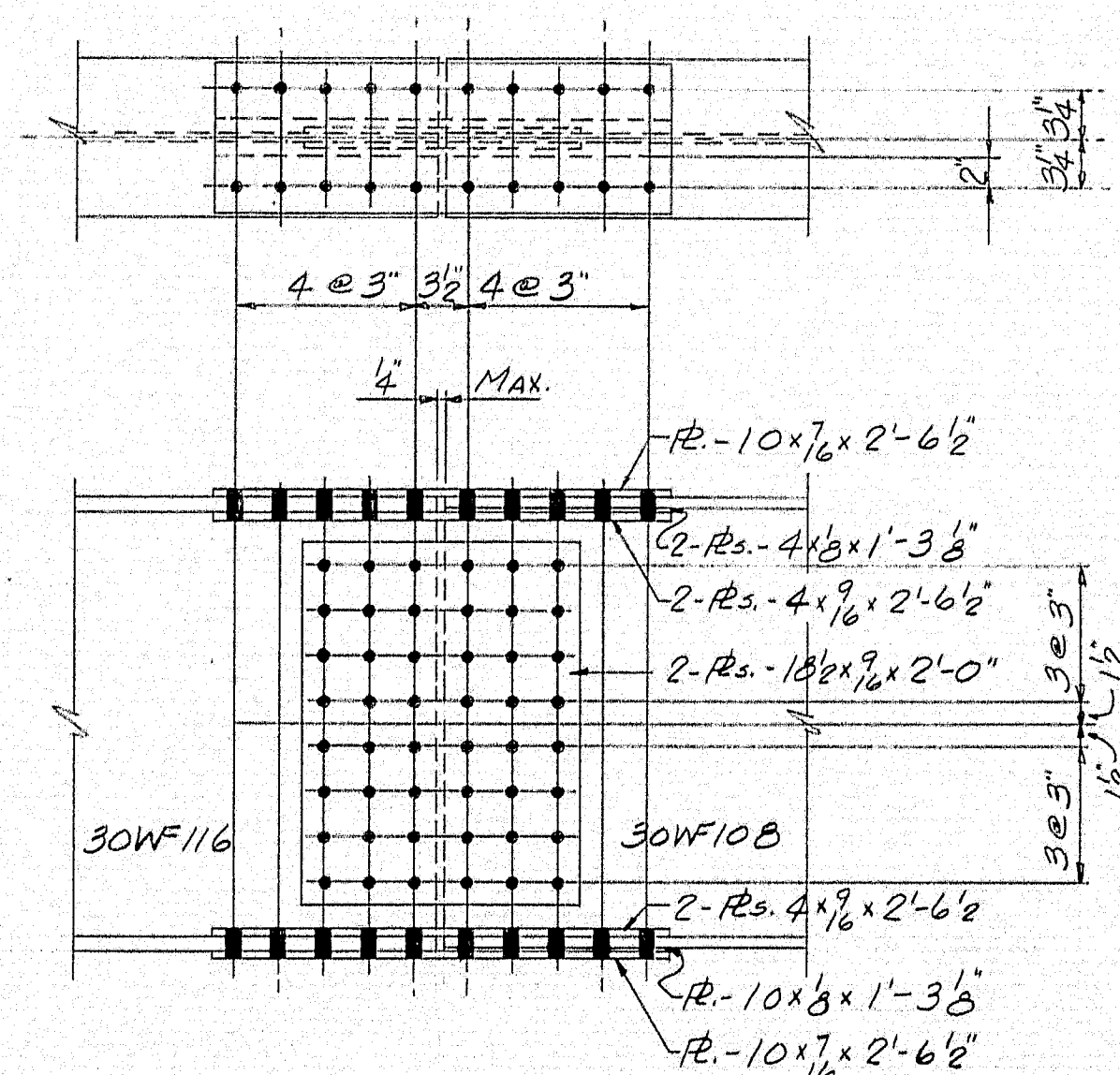
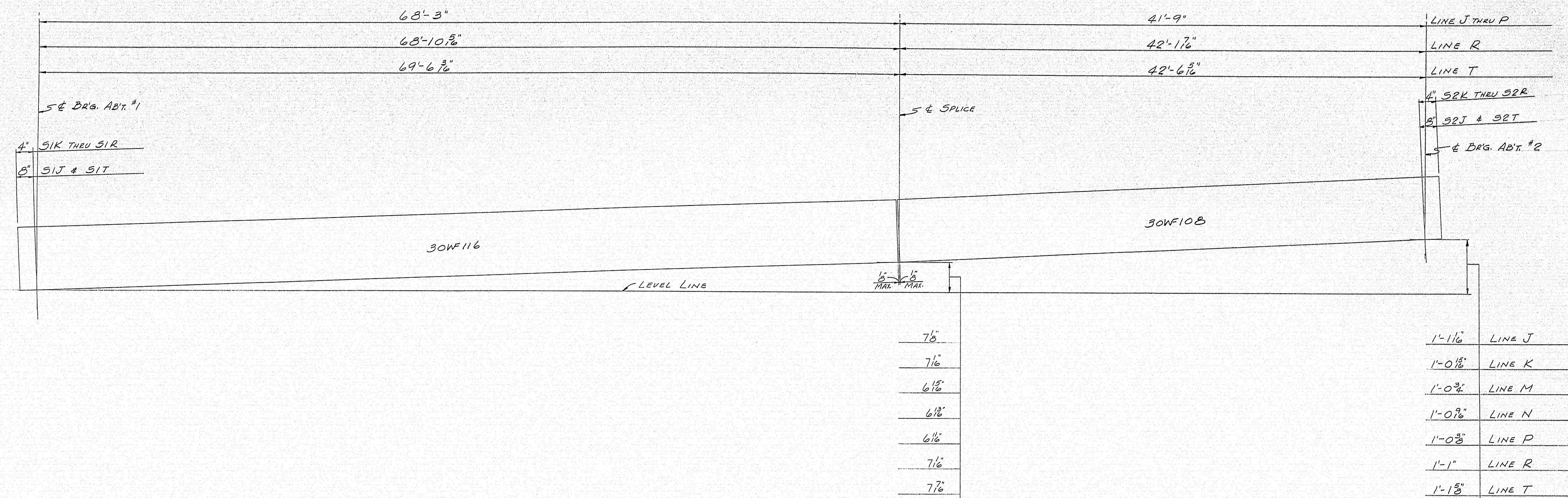
FRAMING PLAN			NORTHBOUND	
PRINT ISSUE			<i>Bancroft &amp; Martin Inc.</i> <i>Brewer, Maine</i> I-95 OVER E. BRANCH MATTAWANKEAG RIVER OAKFIELD, MAINE	
2 SHOP	9-10-65			
5 G.H.C.	9-1-65			
3 CUST.	9-1-65			
2 F.A.	8-13-65		CUSTOMER <u>CIANCHETTE BRO'S, INC.</u> DESIGNER <u>M.S.H.C., BRIDGE DIVISION</u> ORDER <u>VERBAL</u> DWG. <u>B65-135-E2</u>	
2 F.A.	8-6-65			
DRAWN	7-22-65 R.A.M.			
REVISION	8-13-65 R.A.M.			
REVISION				
REVISION				

**97-175**









STRINGER SPLICE DETAIL

40-3/8" x 0-3/4" H.S. BOLTS FOR FLG. SPLICE  
48-3/8" x 0-3" H.S. BOLTS FOR WEB SPLICE

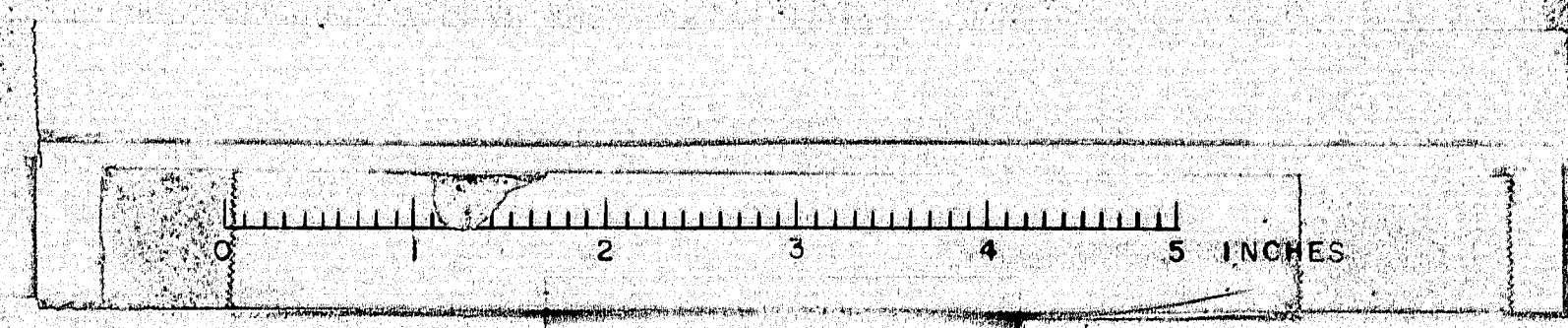
SHOP CONNECTIONS: LH-E7013 or LH-E7028  
FIELD CONNECTIONS: 3/8" H.S. Bolt & WELD  
HOLES: 1/8" &  
PAINT: STATE OF MAINE SPEC'S.

APP'D. AS NOTED 8-10-65 PROJ. No. I-95-9 (12)

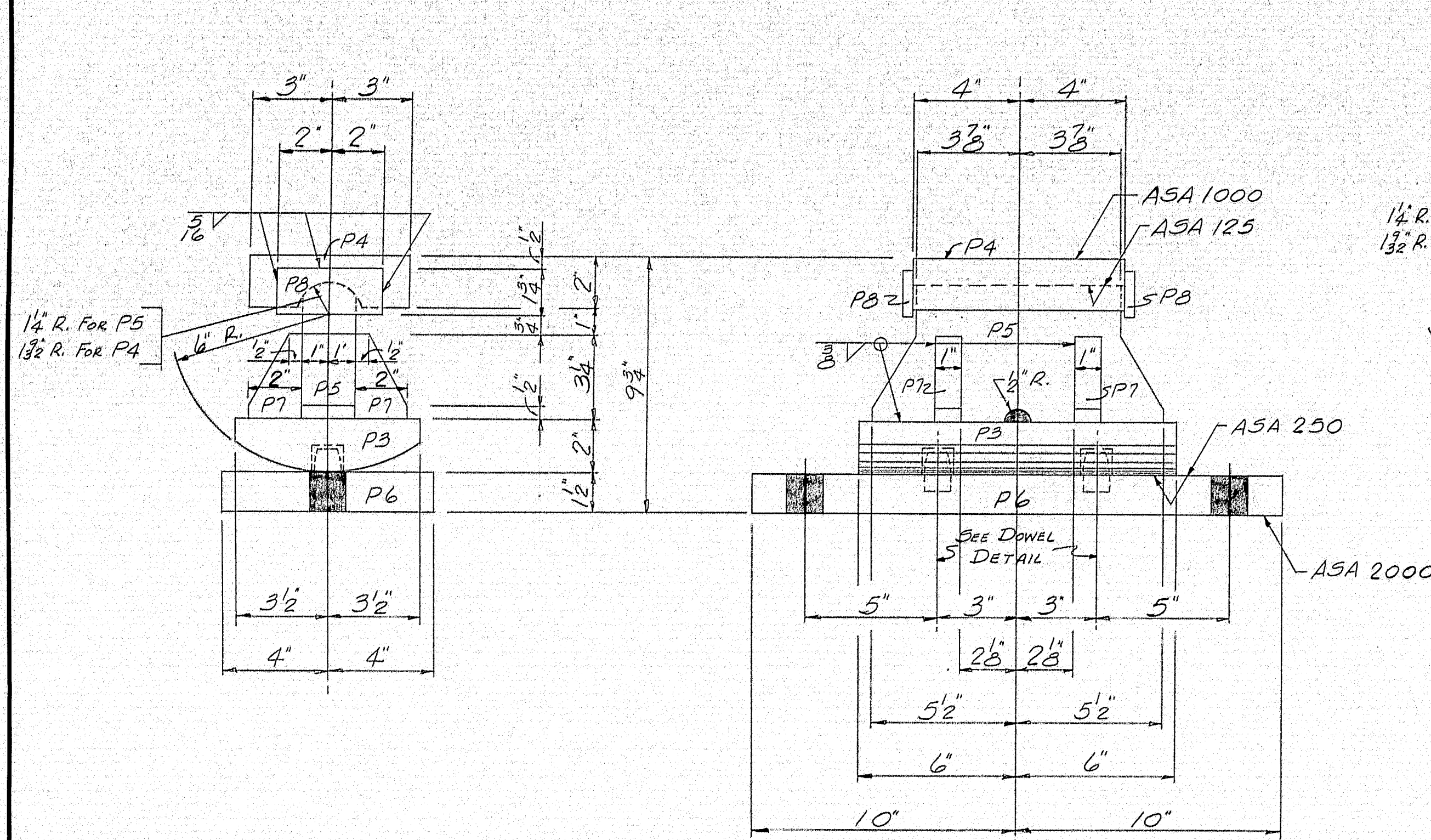
STRINGER ELEVATION DIAGRAM & SPLICE DETAIL - NORTHBOUND

PRINT ISSUE			Bancroft & Martin Inc. Brewer, Maine	
NO.	DATE	BY		
5	S.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER OAKFIELD, MAINE	
3	CUST.	9-10-65		
2	SHOP	9-10-65		
2	F.A.	8-6-65	CUSTOMER CIANCHETTE BRO'S, INC. DESIGNER M.S.H.C., BRIDGE DIVISION	
DRAWN	8-4-65	R.A.M.		
REVISION				
REVISION				
REVISION			ORDER VERBAL	DWG. B65-135-E4

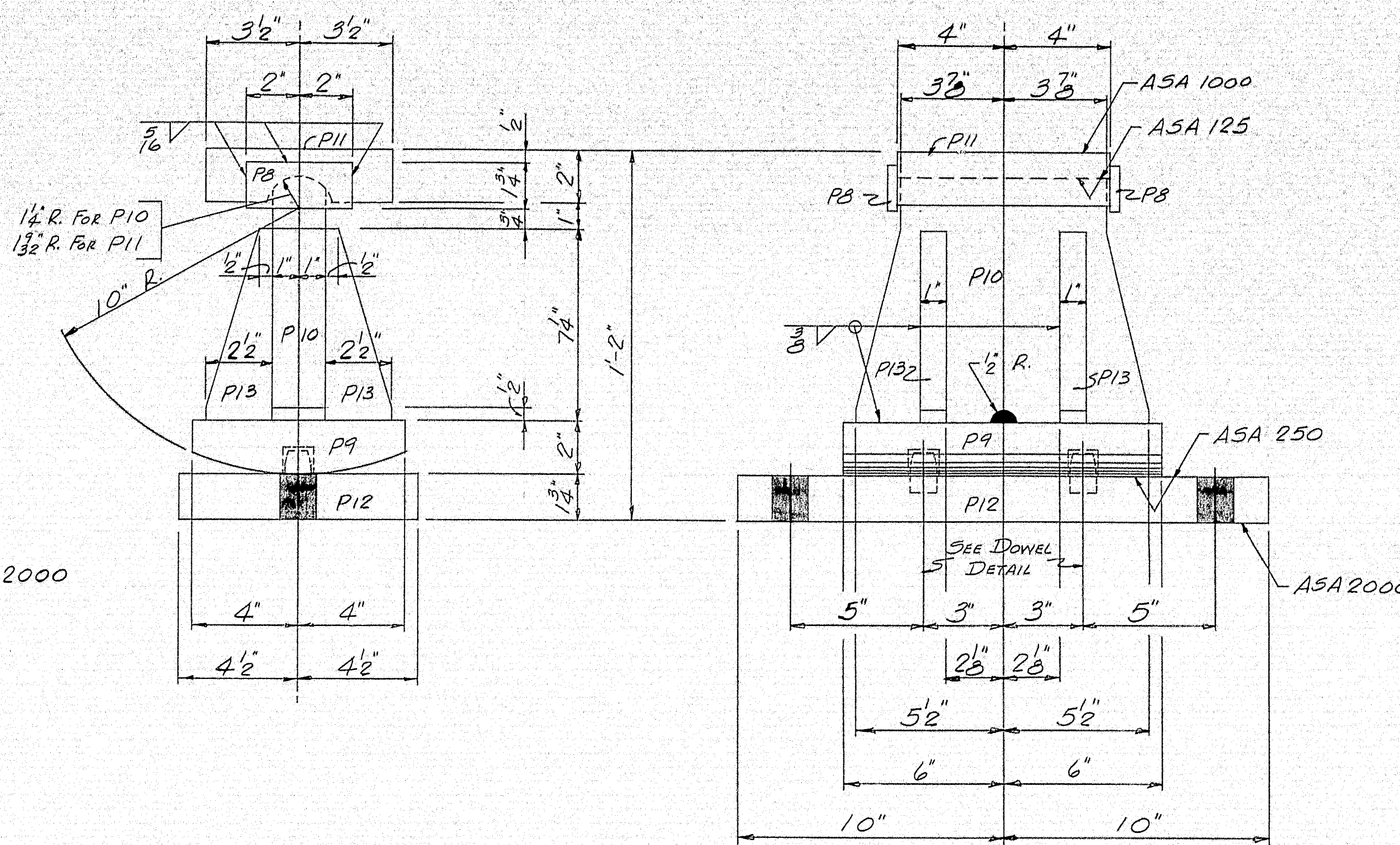
97-177



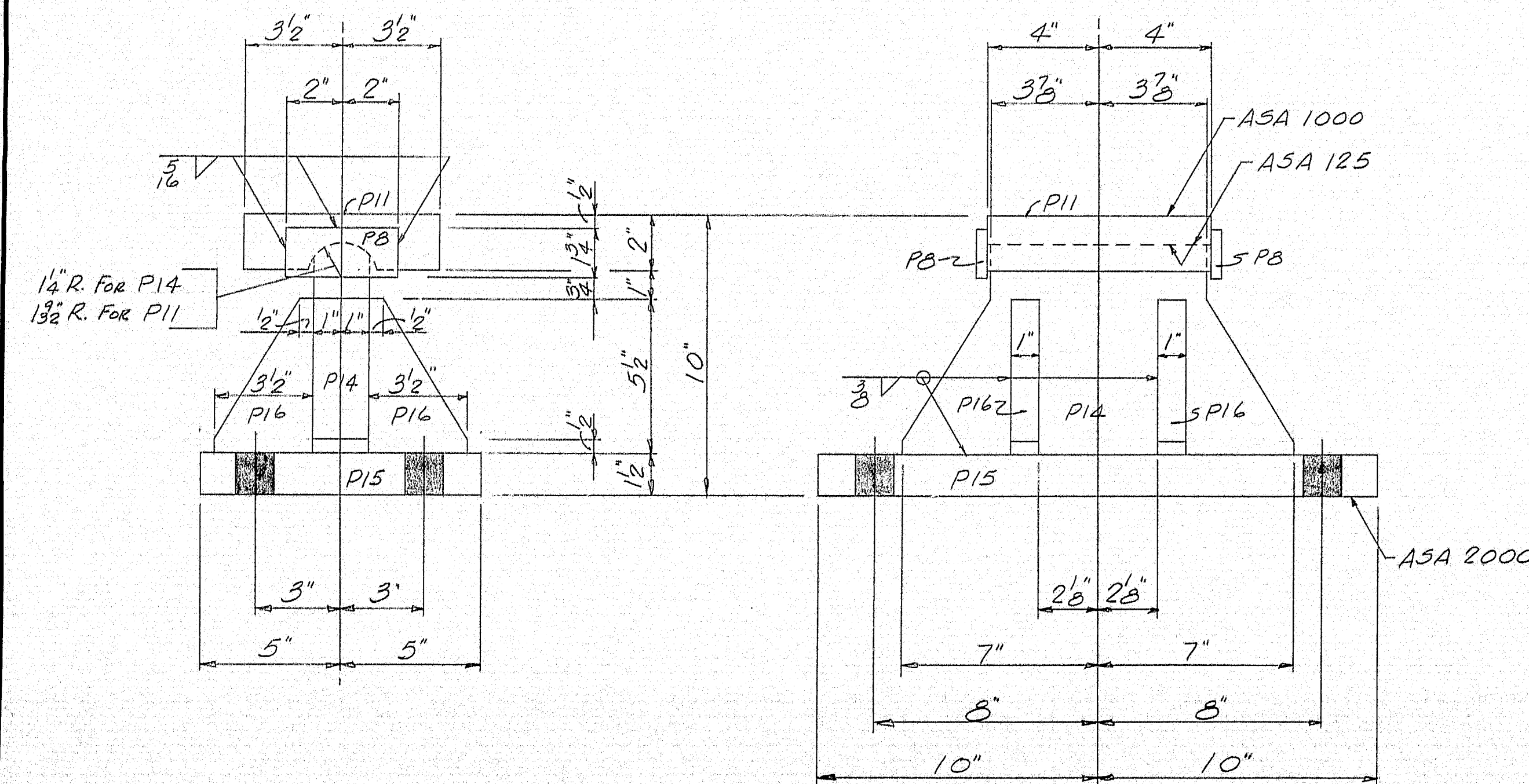




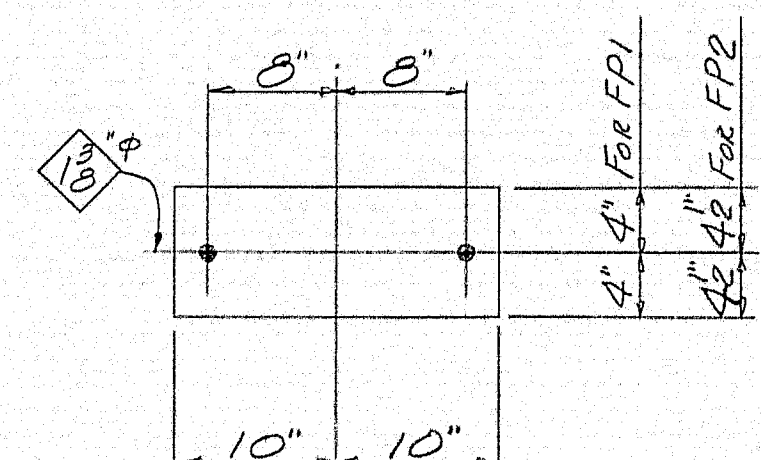
16 - EPC-1 REQ'D.



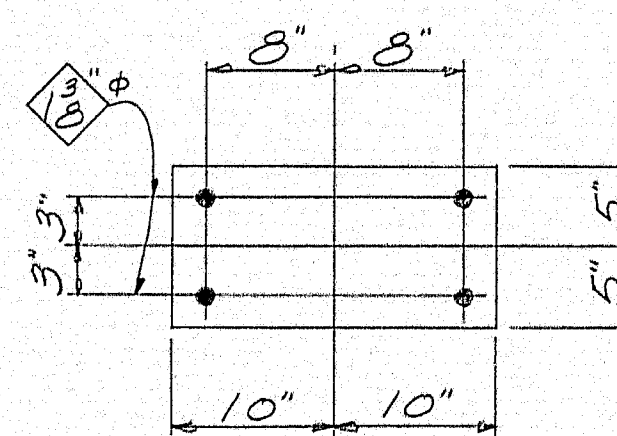
8 - EPC-3 REQ'D.



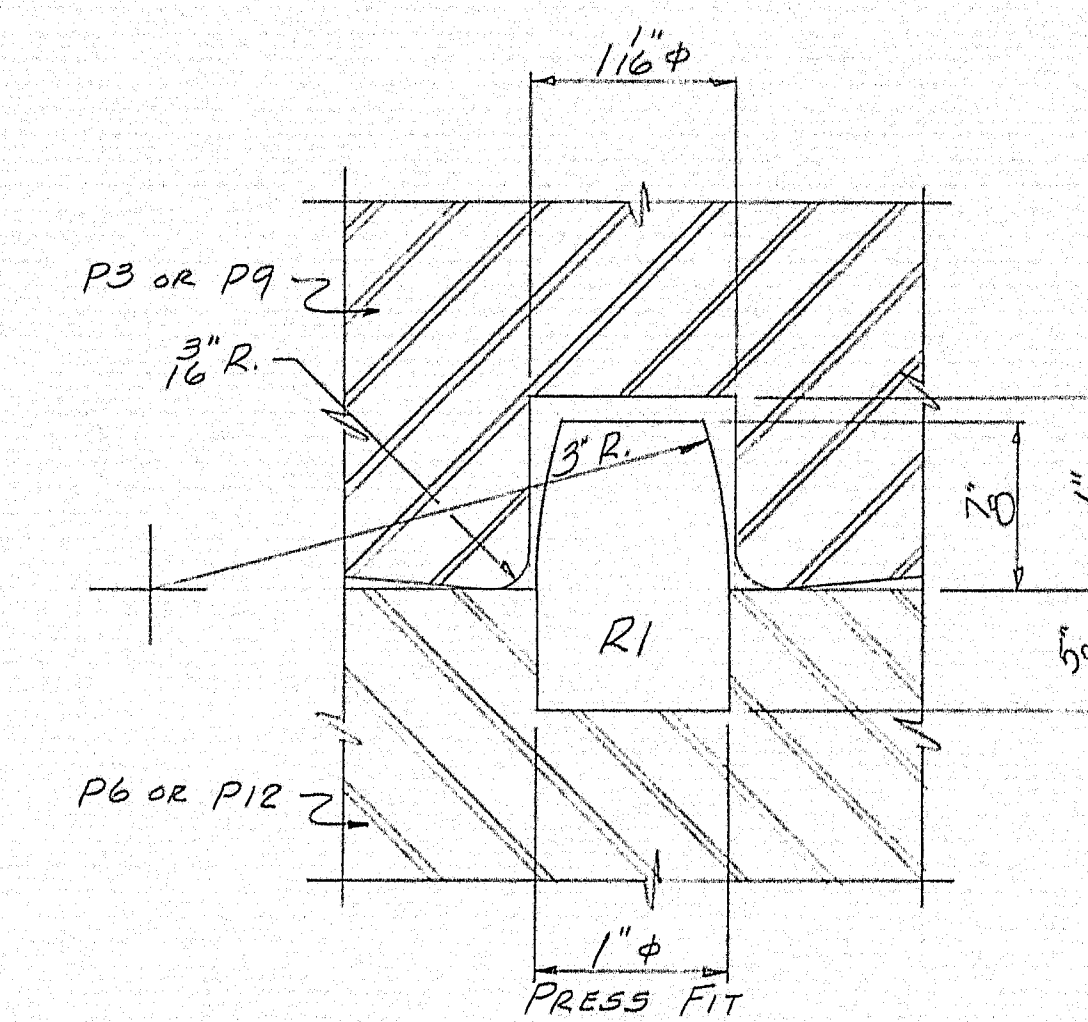
8 - FPC-2 REQ'D.



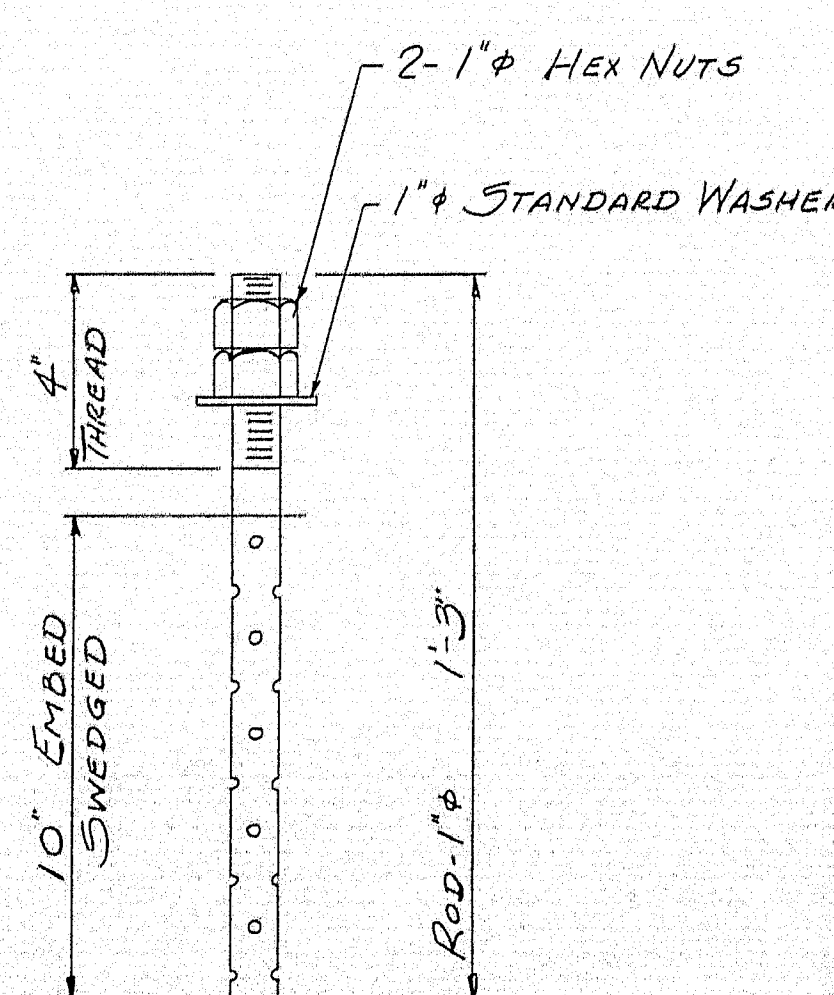
16 - FP1  
8 - FP2



8 - FP3



DOWEL DETAIL



80 - AB1

**PAINT NOTES**  
NO PAINT ON ANCHOR BOLTS - OIL THREADS. NO PAINT ON TOP SURFACE & 1/2" DOWN FROM TOP ON SIDES OF SOLE PLATES. COAT WITH BOILED LINSEED OIL. ~~REPAIR~~ ON SURFACES FINISHED WITH WHITE LEAD & TALLOW. ~~REPAIR~~ TURN OF WHITE LEAD & TALLOW. ~~REPAIR~~ FINISH WITH WHITE LEAD & TALLOW. ONE SHOP COAT PAINT ON SURFACES FINISHED ASA 250. NO PAINT ON SURFACES ASA 125, COAT WITH HOT MIXTURE OF WHITE LEAD & TALLOW.

SHIP		BILL OF MATERIAL				DWG. B65-135-51
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
EPC1	16		PEDESTAL			
	16	P3	R-7x2	1 0		A36
	16	P4	R-6x2	0 8		
	16	P5	R-5x2	0 11		
	16	P6	R-3x1 1/2	1 3		
	64	P7	R-2x1	0 34		
	32	P8	R-1 1/4 x 3/8	0 4		
EPC3	8		PEDESTAL			
	8	P9	R-8x2	1 0		A36
	8	P10	R-9x2	0 11		
	8	P11	R-7x2	0 8		
	8	P12	R-9x1 1/4	1 8		
	32	P13	R-2 1/2 x 1	0 74		
	16	P8	R-1 1/4 x 3/8	0 4		
FPC2	8		PEDESTAL			
	8	P14	R-7 1/2 x 2	1 2		A36
	8	P11	R-7x2	0 8		
	8	P15	R-10x1 1/2	1 8		
	32	P16	R-3 1/2 x 1	0 52		
	16	P8	R-1 1/4 x 3/8	0 4		
	48	R1	ROD-1" φ	0 1/2		A36
FP1	16		3 x 1/8	1 3		FABCO PAD 5A47
FP2	8		9 x 3/8	1 8		Do Do
FP3	8		10 x 3/8	1 3		Do Do
AB1	80		ROD-1" φ	1 3		THREAD & SWAGED
	160		1" HEX NUT			REQ. #3880
	80		1" STD. WASHER			

WELD WITH LH-ET02B OR LH-EG02B OR SAW-I & PREHEAT  
SHOP CONNECTIONS: 1" TO 2" THK. MAT. TO 50" F.  
FIELD CONNECTIONS: BOLT & WELD  
HOLES: 1/8" U.N.  
PAINT: STATE OF MAINE SPEC'S. & SEE PAINT NOTE THIS SHEET  
PROJ. NO. I-95-9 (12)

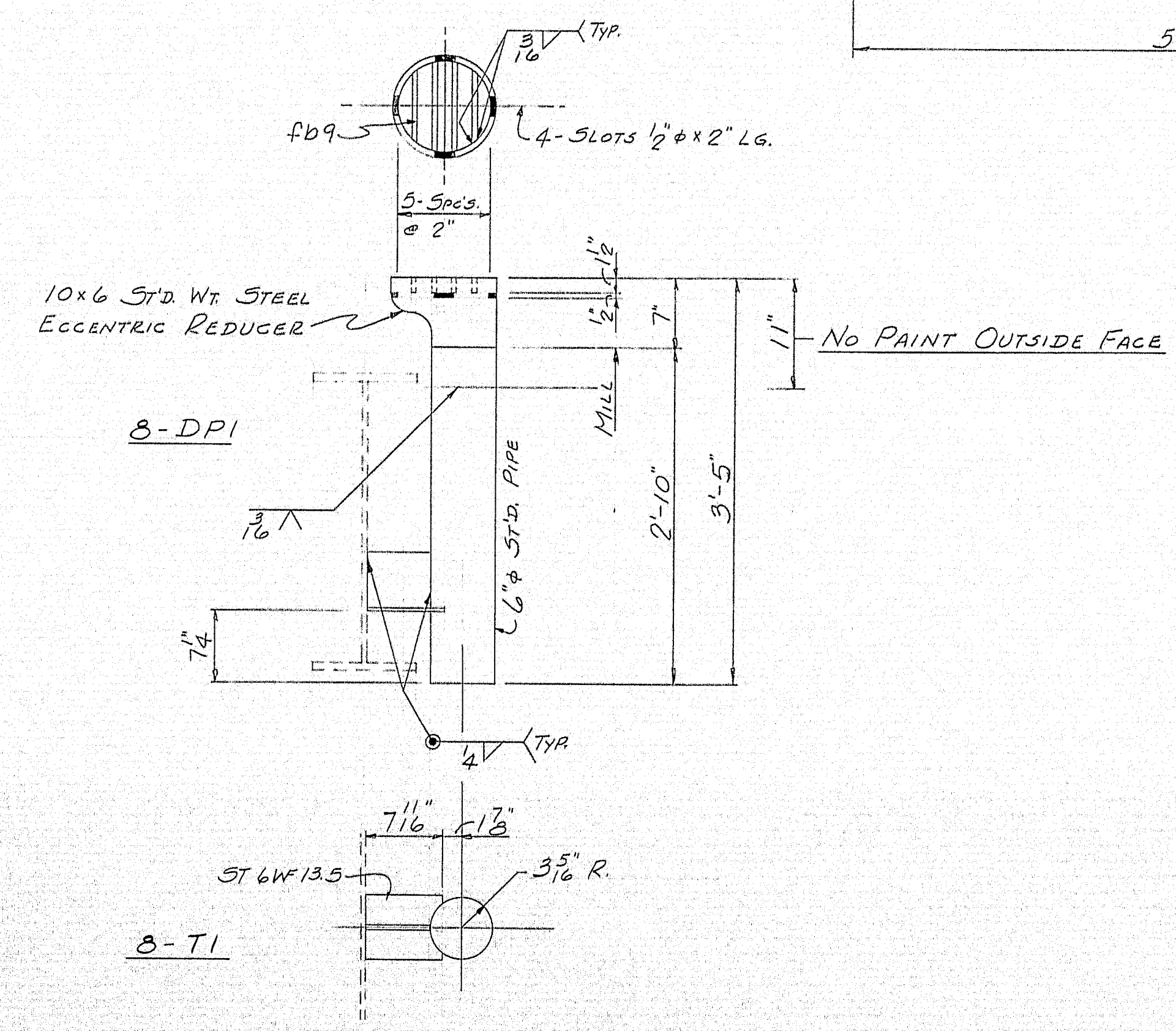
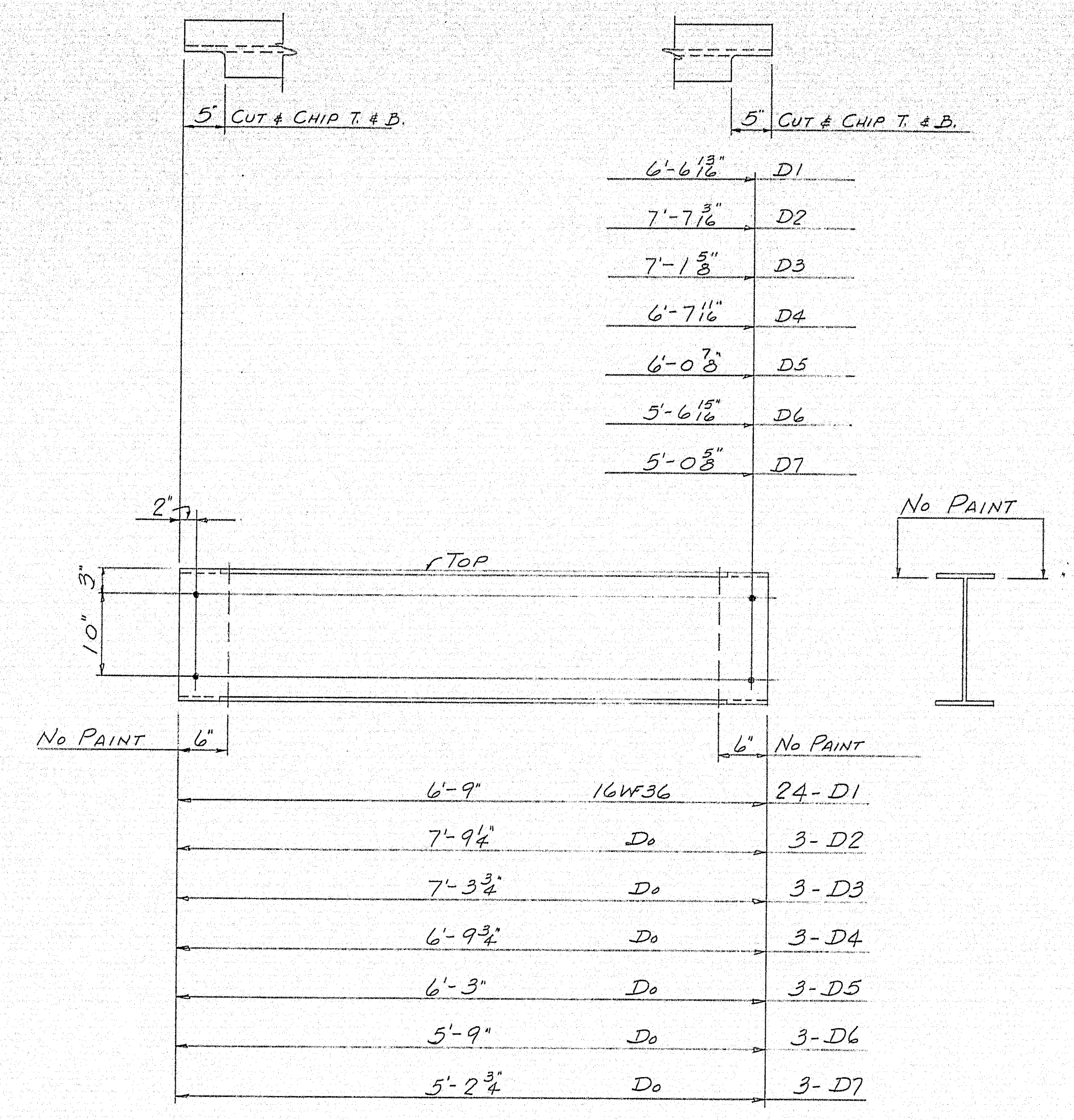
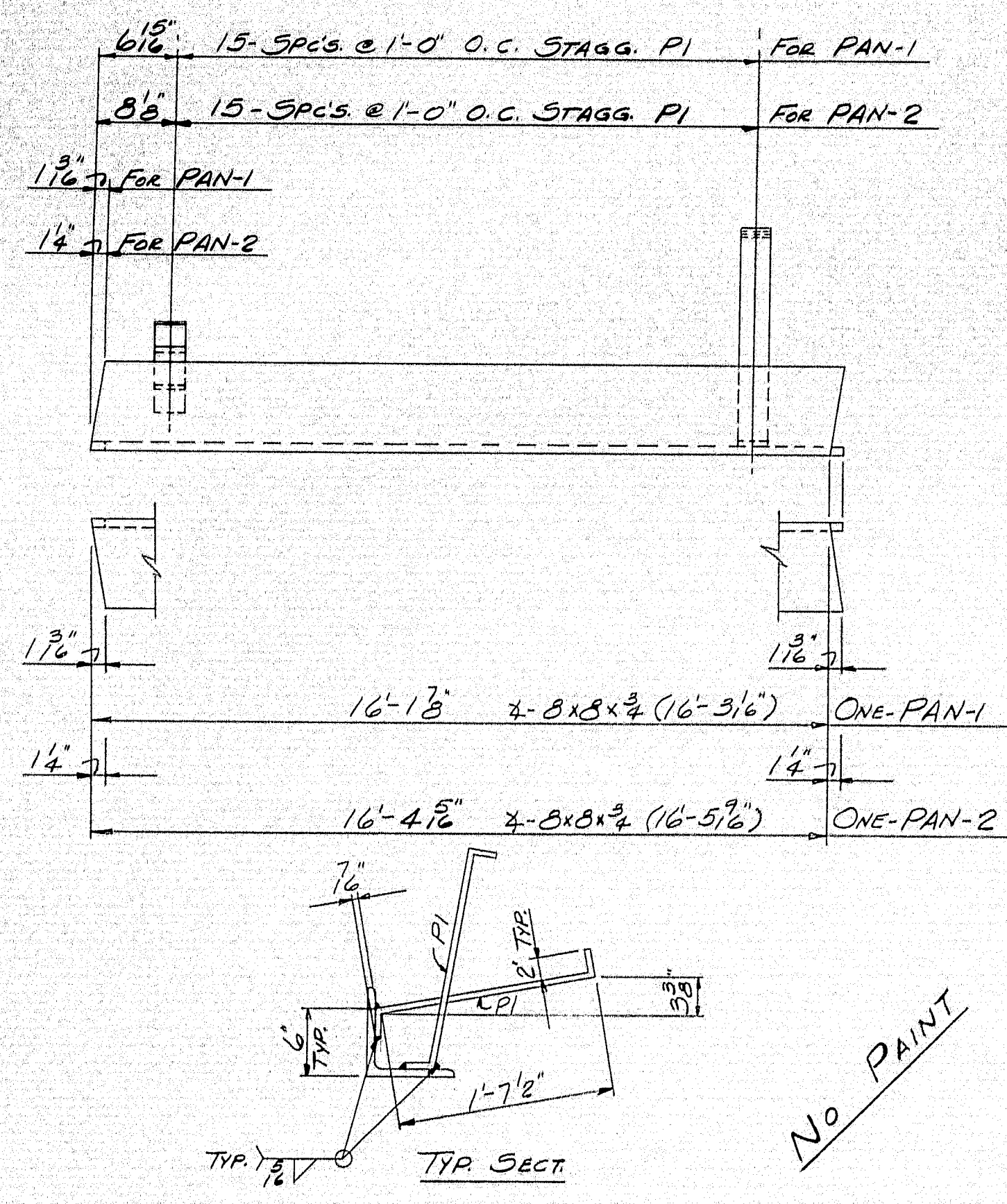
APP'D. AS NOTED 8-10-65  
APP'D. 7-29-65

BEARING PEDESTALS & ANCHOR BOLTS		SOUTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc.	
5 J.H.C. 9-10-65		Brewer, Maine	
3 CUST. 9-10-65		I-95 OVER E. BRANCH MATTAWAMKEAG RIVER	
4 PORT. 9-8-65		OAKFIELD MAINE	
2 F.A. 8-6-65		CUSTOMER CIANCHETTE BRO'S, INC.	
4 PORT. 7-28-65		DESIGNER M.S.H.C. BRIDGE DIVISION	
2 F.A. 7-28-65		ORDER VERBAL DWG. B65-135-51	
DRAWN 7-28-65 R.A.M.			
REVISION 8-4-65 R.A.M.			
REVISION			
REVISION			









SHIP		BILL OF MATERIAL				DWG. B65-135-53	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS	
PAN-1	1		4-8x8x3/4	16	316	FOR PIER #1	MARYBET-R
PAN-2	1		D0	16	516	FOR PIER #2	D0
	32	P1	2-2 3/4 x 1/2	2	0	BENT	A36
D1	24		16WF36	6	9		
D2	3		D0	7	94		
D3	3		D0	7	33		
D4	3		D0	6	93		
D5	3		D0	6	3		
D6	3		D0	5	9		
D7	3		D0	5	2 3/4		
DP1	8		6" STD. PIPE	2	10		
	8		10x6 STD. WT. STEEL ECC. REDUCER	0	7		
	32	FB9	BAR-1/2x1/2	0	10	FIT	A36
T1	8		ST6WF13.5	0	7 1/2		A36

SHOP CONNECTIONS: LH-E7013 OR LH-E7028  
 FIELD CONNECTIONS: BOLT & WELD  
 HOLES: 1/8" & U.N.  
 PAINT: STATE OF MAINE SPECS.

APP'D. 8-2-65 PROJ. No. I-95-9(12)

PIER ANGLE NOSINGS - DIAPHRAGMS - DRAINS SOUTHBOUND

PRINT ISSUE	
5	S.H.C. 9-10-65
3	CUST. 9-10-65
5	SHOP 9-10-65
2	F.A. 8-13-65
5	SHOP 8-2-65
2	F.A. 7-30-65

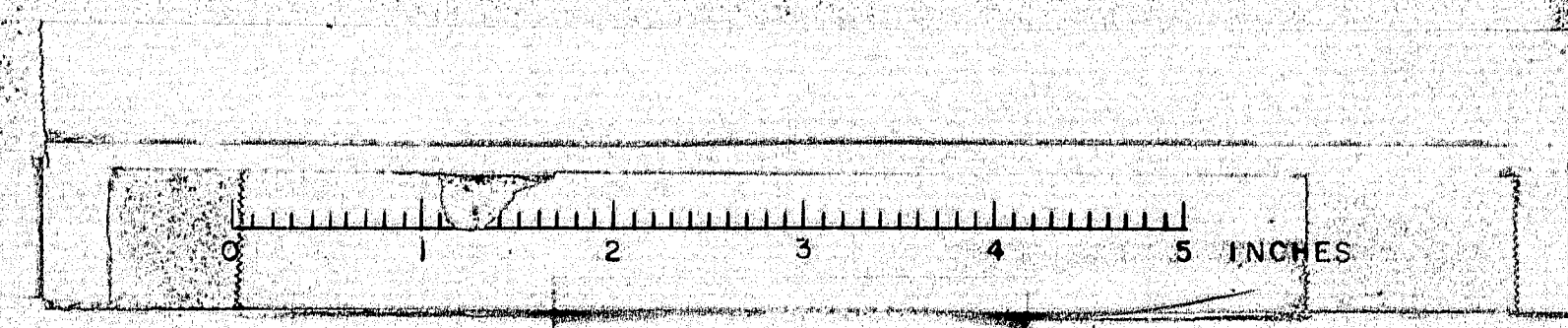
**Bancroft & Martin Inc.**  
 Brewer, Maine

I-95 OVER E. BRANCH MATTAWAMKEAG RIVER  
 OAKFIELD, MAINE

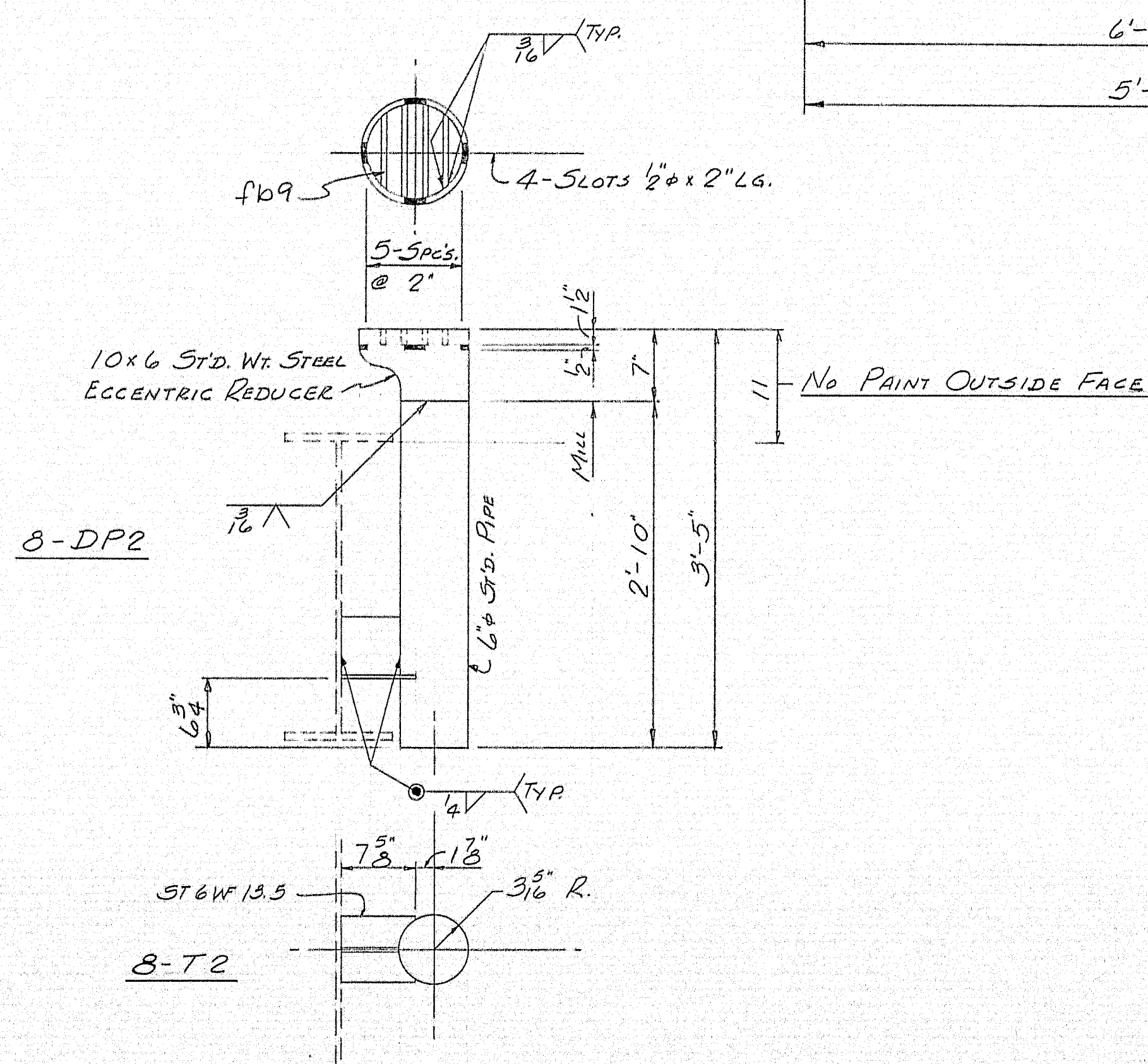
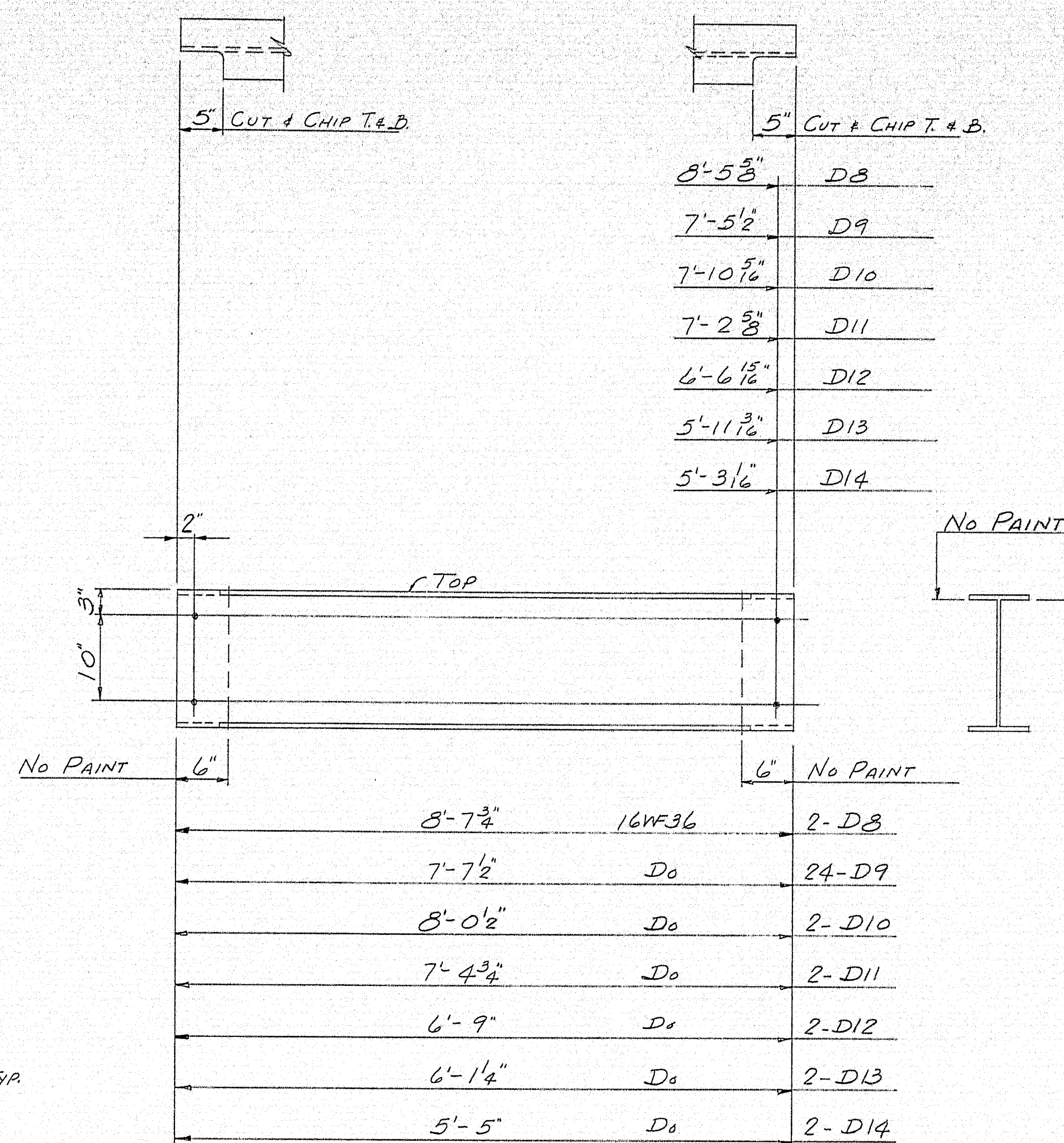
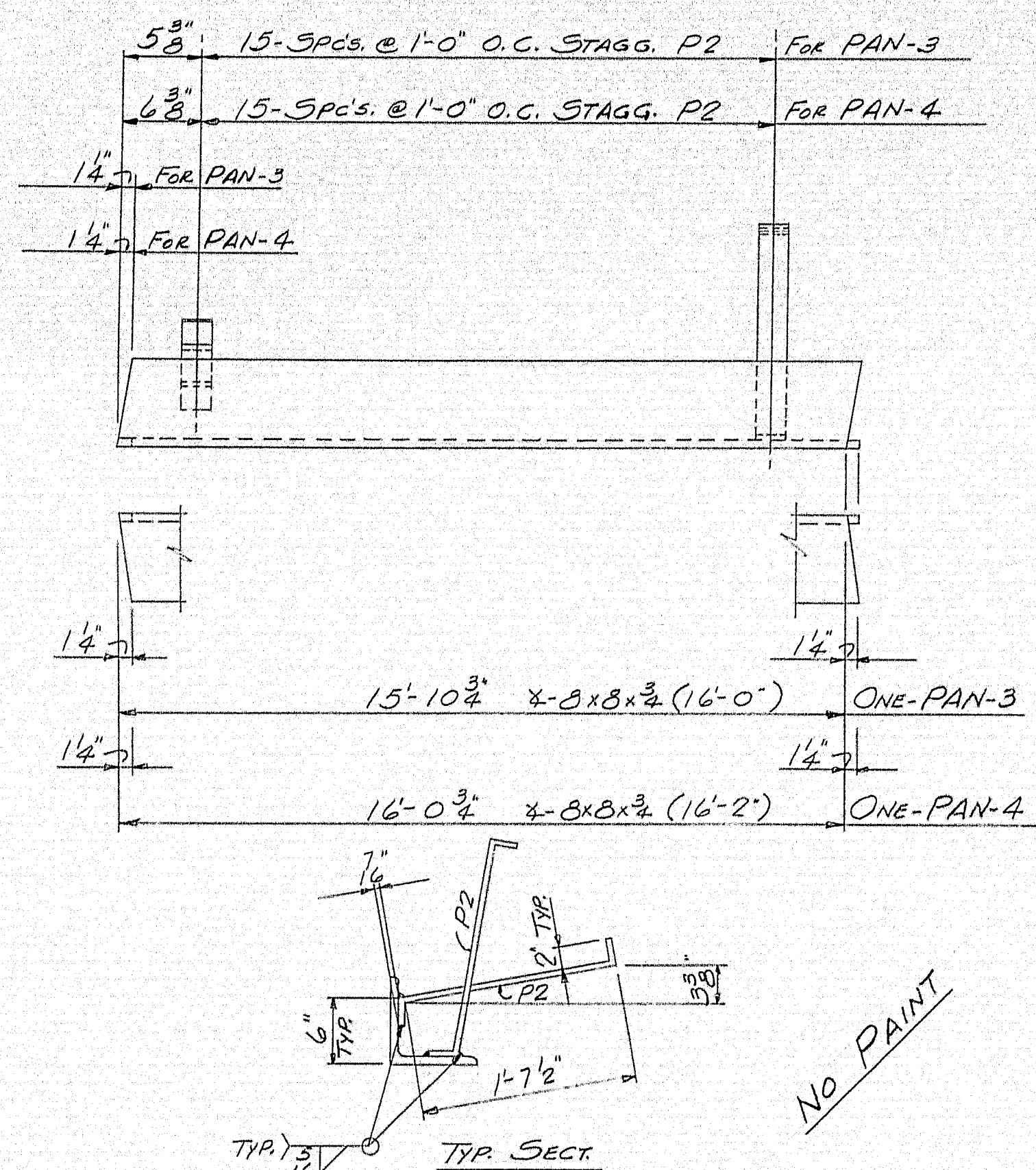
CUSTOMER: CLANCHETTE BROS., INC.  
 DESIGNER: M.S.H.C., BRIDGE DIVISION

ORDER: VERBAL DWG. B65-135-53

97-180





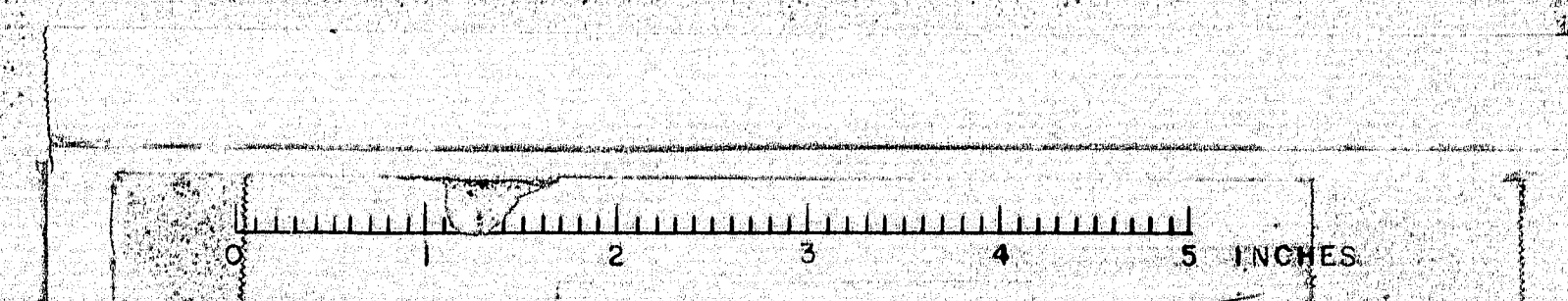


SHIP		BILL OF MATERIAL			DWG. B65-135-54	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
PAN-3	1		4-8x8x3/4	16	0	FOR PIER #1
PAN-4	1		D8	16	2	FOR PIER #2
	32	P2	4-2 3/4 x 2	2	0	BENT
D8	2		16WF36	8	7 3/4	
D9	24		D8	7	7 1/2	
D10	2		D8	8	0 1/2	
D11	2		D8	7	4 3/4	
D12	2		D8	6	9	
D13	2		D8	6	14	
D14	2		D8	5	5	
DP2	8		6x5 STD. PIPE	2	10	
	8		10x6 STD. WT. STEEL ECC. REDUCER	0	7	
	32	FB9	FB9-12x2	0	10	FIT
T2	8		5T 6WF13.5	0	7 3/8	

SHOP CONNECTIONS: LH-E701B or LH-E702B  
 FIELD CONNECTIONS: BOLT & WELD  
 HOLES: 1/2" & U.N.  
 PAINT: STATE OF MAINE SPECS.

APP'D. 8-2-65			PROJ. No. I-95-9 (12)		
PIER ANGLE NOSINGS - DIAPHRAGMS - DRAINS NORTHBOUND					
PRINT ISSUE			Bancroft & Martin Inc. Brewer, Maine I-95 OVER E. BRANCH MATTAWAMKEAG RIVER OAKFIELD MAINE		
5	S.H.C.	9-10-65			
3	CUST.	9-10-65			
5	SHOP	9-10-65			
2	F.A.	8-13-65			
5	SHOP	8-4-65			
2	F.A.	7-30-65	CUSTOMER CIANCHETTE BROS., INC. DESIGNER M.S.H.C., BRIDGE DIVISION		
DRAWN	7-30-65	R.A.M.			
REVISION	8-13-65	R.A.M.			
REVISION					
REVISION			ORDER VERBAL		
			DWG. B65-135-54		

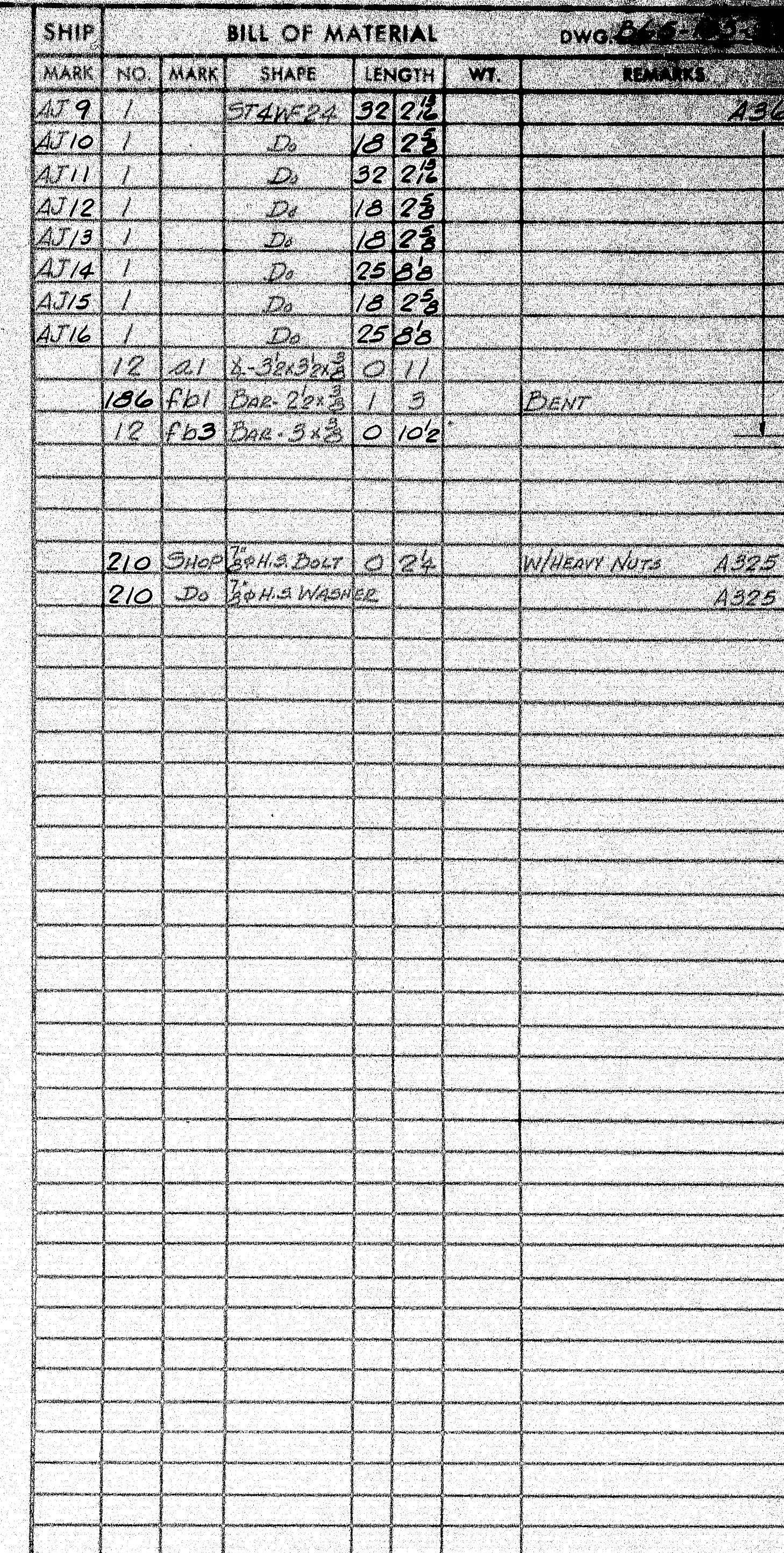
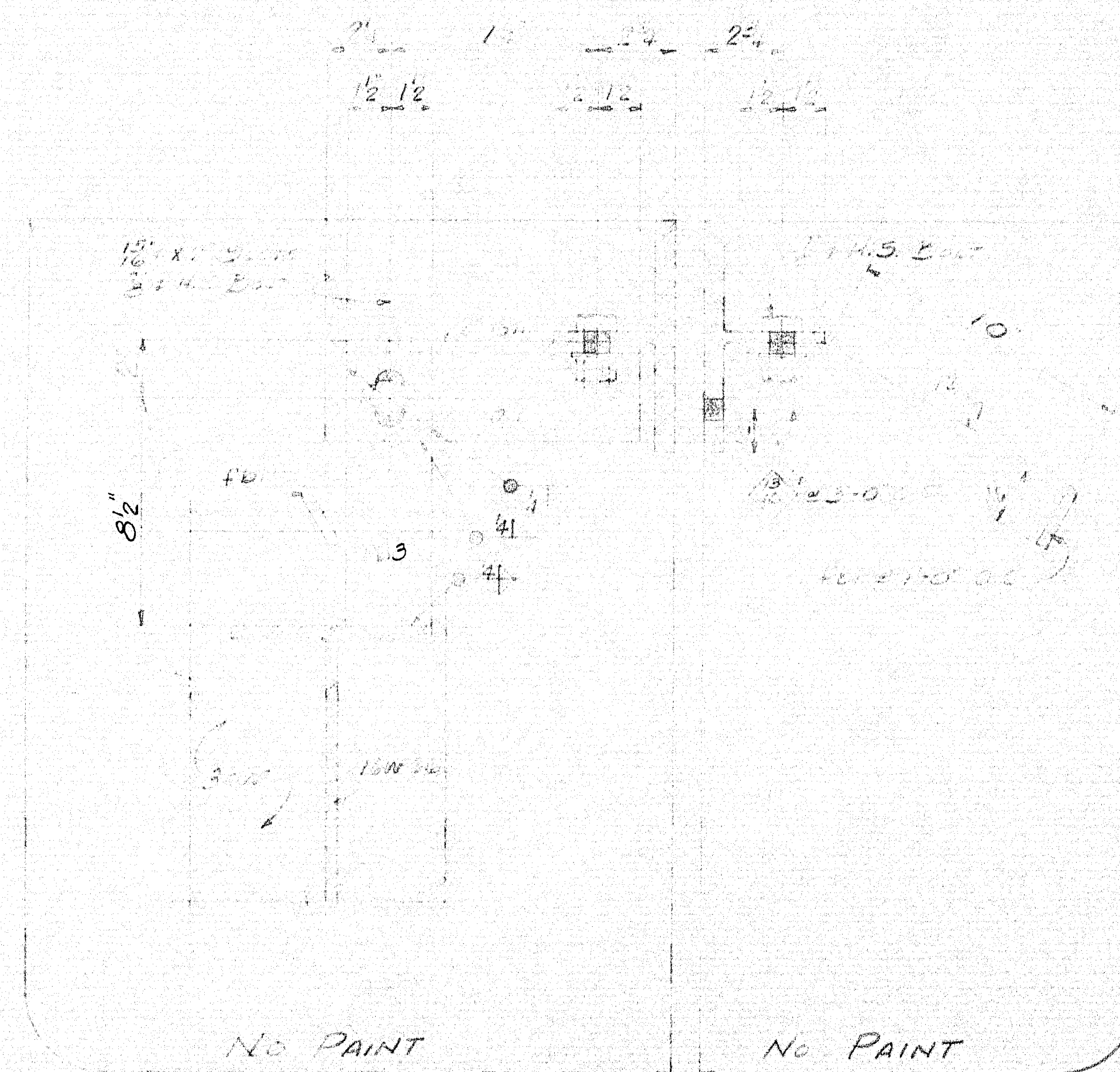
97-181











SHOP CONNECTIONS: 3" H.S. BOLTS  
FIELD CONNECTIONS: WELD  
HOLES: 1 1/2" U.N.  
PAINT: STATE OF MAINE SPEC.

APP'D. AS NOTED 8-10-65

PROJ. No. I-95-9(12)

## ARMORED JOINTS

NORTHBOUND

PRINT ISSUE

*Rancroft & Martin Inc.*  
*Brewer, Maine*

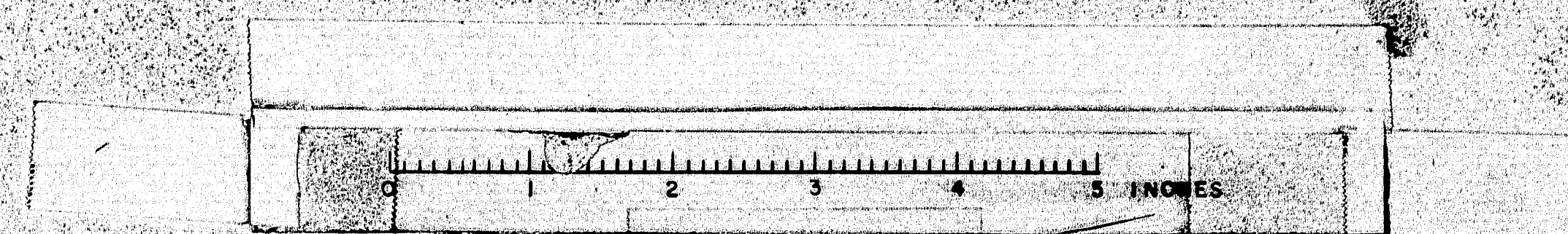
5	S.H.C.	9-1-65	I-95 OVER E. BRANCH MATTAWAMBEAG RIVER
3	CUST.	9-1-65	OAKFIELD,
5	SNRP	9-1-65	MAINE

2	F.A.	B-6-65	CUSTOMER	CIANCHETTE BROS., INC.
DRAWN	B-6-65	P.A.M.	DESIGNER	M.S.H.C., BRIDGE DIVISION
DESIGNER				

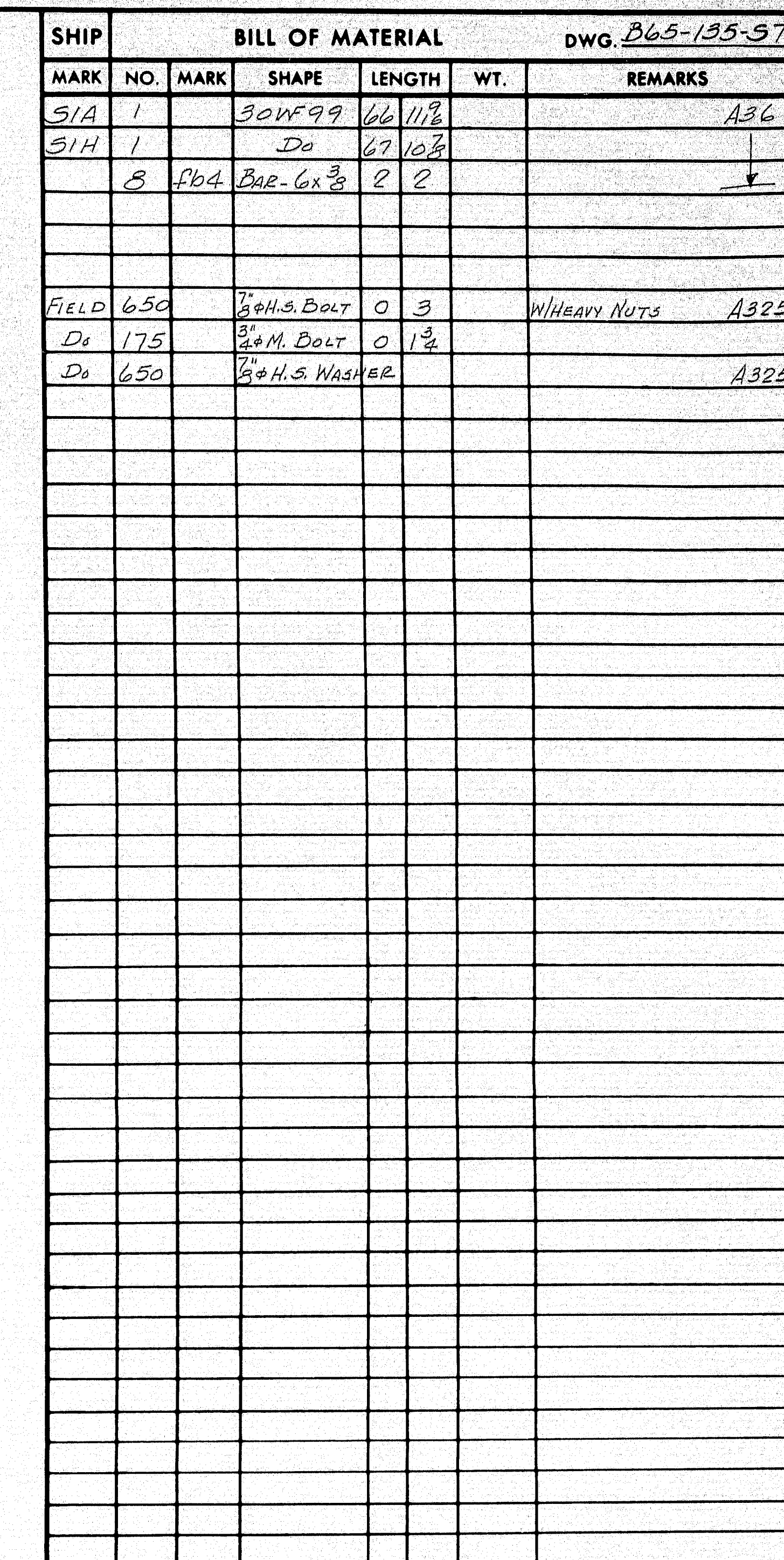
ORDER <u>VERBAL</u>	DWG. <u>B65-135-S6</u>
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97-197

**97-183**







SHOP CONNECTIONS: LH-E7018 or LH-E7028  
FIELD CONNECTIONS: 3"  $\phi$  H. S. BOLT  
HOLES: 1 1/2"  $\phi$   
PAINT: STATE OF MAINE SPEC'S.

APP'D. 8-25-65

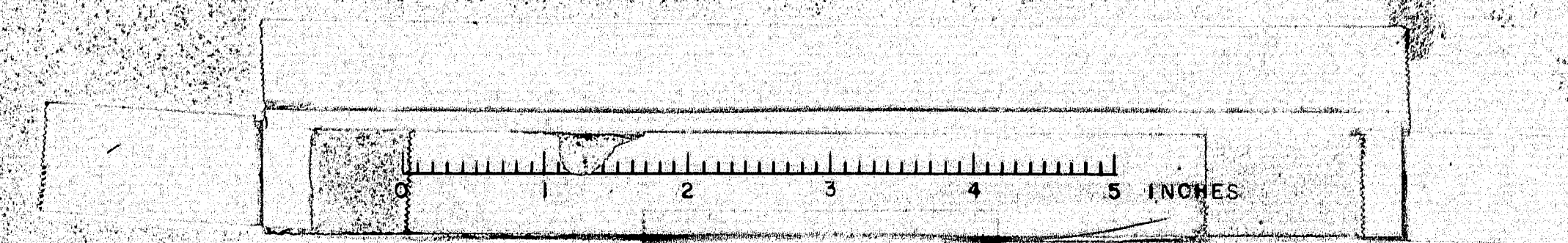
Proj. No. I-95-9(12)

STRINGERS

SOUTHBOUND

PRINT ISSUE			<i>Bancroft &amp; Martin Inc.</i> <i>Brewer, Maine</i> I-95 OVER E. BRANCH MATTAHAMKEAS RIVER OAKFIELD, MAINE CUSTOMER <u>CIANCHETTE BROS., INC.</u> DESIGNER <u>M.S.H.C., BRIDGE DIVISION</u> ORDER <u>VERBAL</u> DWG. <u>B65-135-57</u>
5	S.H.C.	9-10-65	
3	CUST	9-10-65	
3	SHOP	9-10-65	
2	F.A.	8-13-65	
DRAWN <u>B-9-65 R.A.H.</u>			
REVISION			
REVISION			
REVISION			

**97-184**



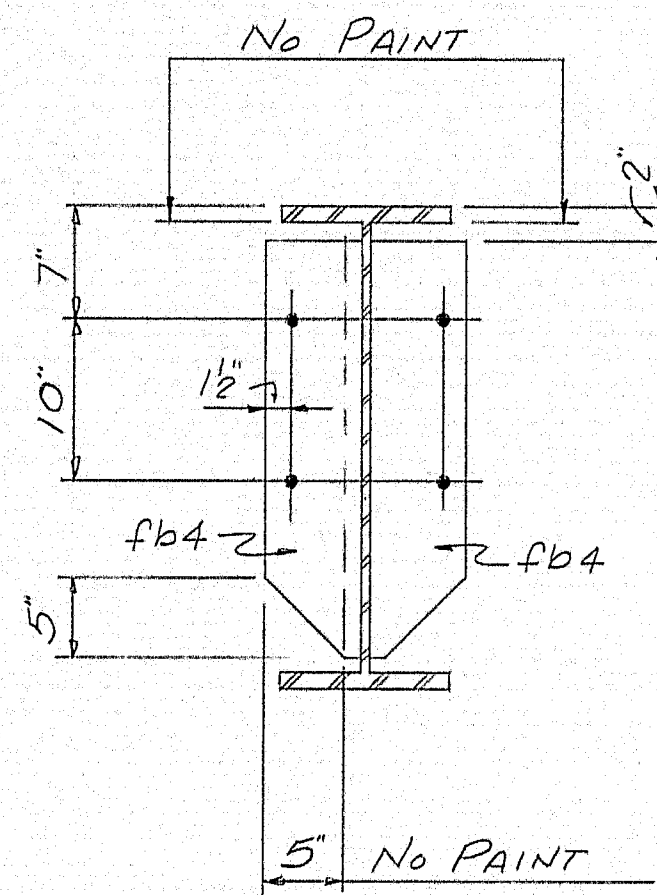












ONE-52H

SHOP CONNECTIONS: LH-E701B OR LH-E702B  
FIELD CONNECTIONS: 3" H.S. BOLT  
HOLES: 1 1/2"  $\phi$   
PAINT: STATE OF MAINE SPEC'S.  
PROJ. No. I-95-9 (12)

PROJ. No. I-95-9 (12)

**97-187**





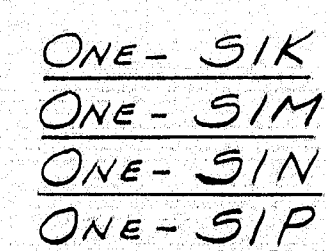








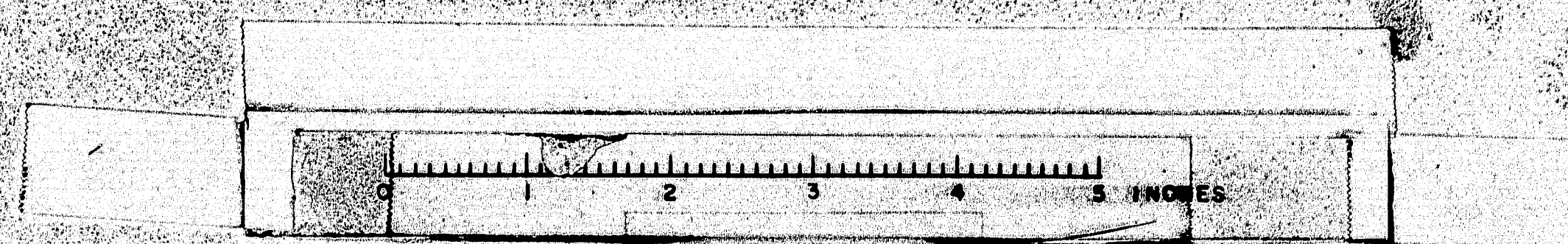




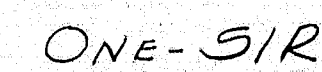
SHOP CONNECTIONS: LH-E7028 " LH-E7028  
FIELD CONNECTIONS: 3/4" H S BOLT  
HOLES: 1.5"  $\phi$   
PAINT: STATE OF MAINE SPEC.

PROJ. No. I-95-9 (2)

**97-191**



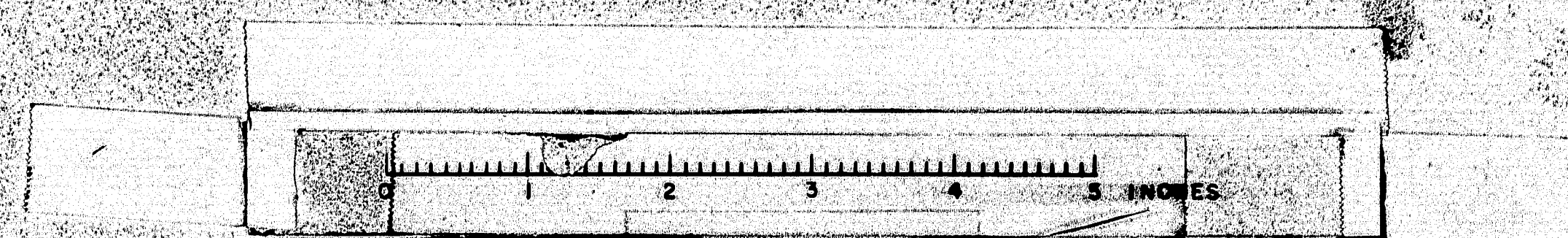




SHOP CONNECTIONS: LH-E701B or LH-E702B  
FIELD CONNECTIONS: 3" x H.S. BOLT  
HOLES: 1 1/2" x  
PAINT: STATE OF MAINE SPEC'S.

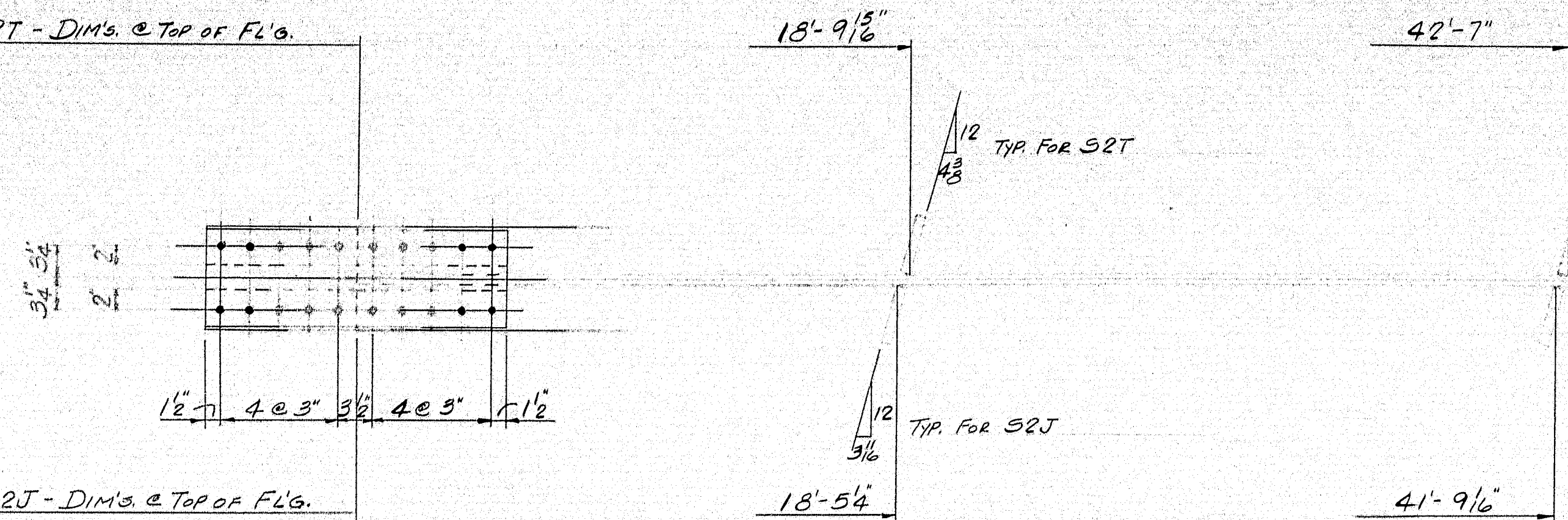
Proj. No. I-95-9(12)

97-192

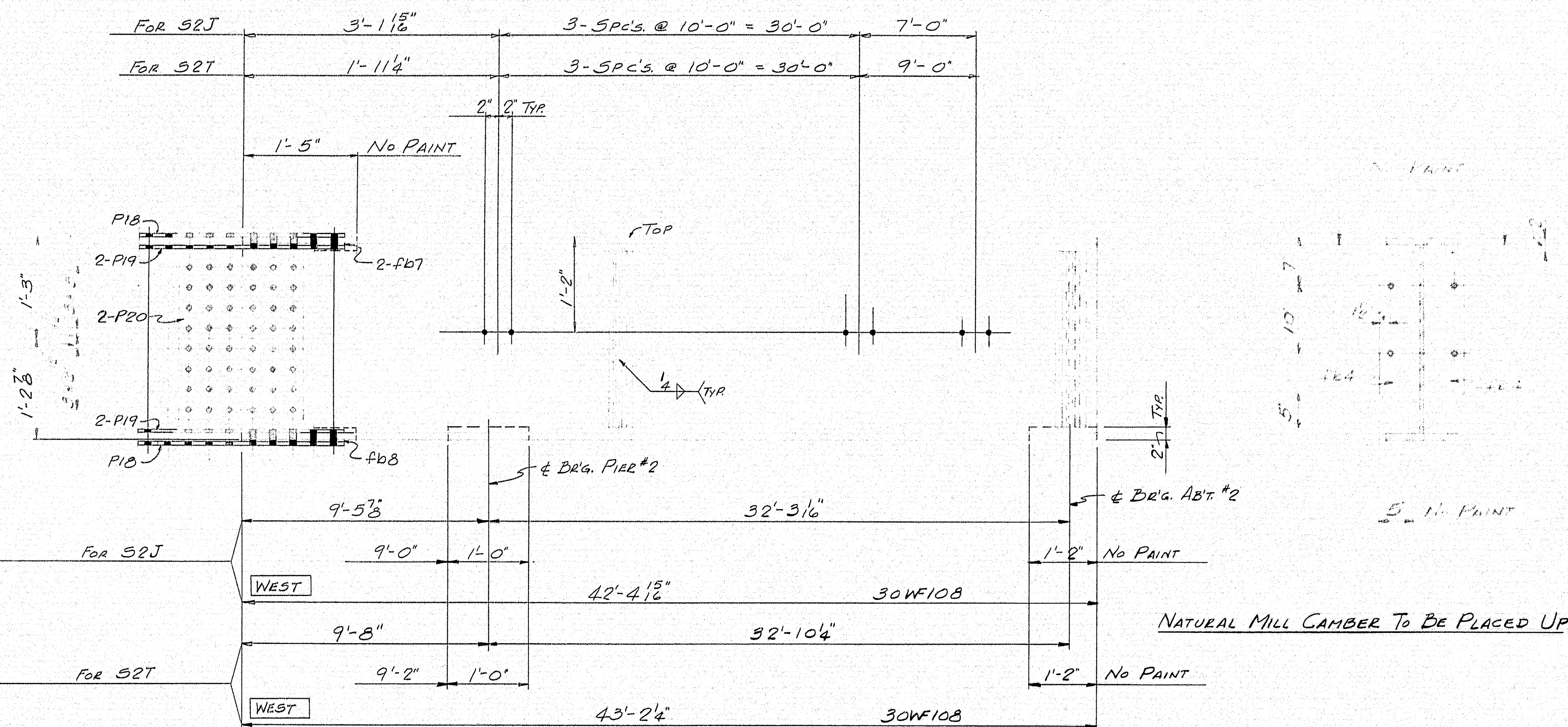




For S2T - DIMS. @ TOP OF FLG.



For S2J - DIMS. @ TOP OF FLG.



ONE - S2J

ONE - S2T

SHIP		BILL OF MATERIAL				DWG. 865-135-S16
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S2J	1		30W108	42' 4 1/2"		A36
S2T	1		D	43' 2 1/4"		
	4	fb4	BAR-6x3	2	2	
	4	fb7	BAR-4x3	1	3 1/2	
	2	fb8	BAR-10x3	1	3 1/2	
	4	P18	R-10x1/6	2	6 1/2	
	8	P19	R-4x1/6	2	6 1/2	
	4	P20	R-18x1/6	2	0	

SHOP CONNECTIONS: LH-E7018 or LH-E7028  
 FIELD CONNECTIONS: 3" H.S. BOLT  
 HOLES: 1/8" #  
 PAINT: STATE OF MAINE SPEC'S

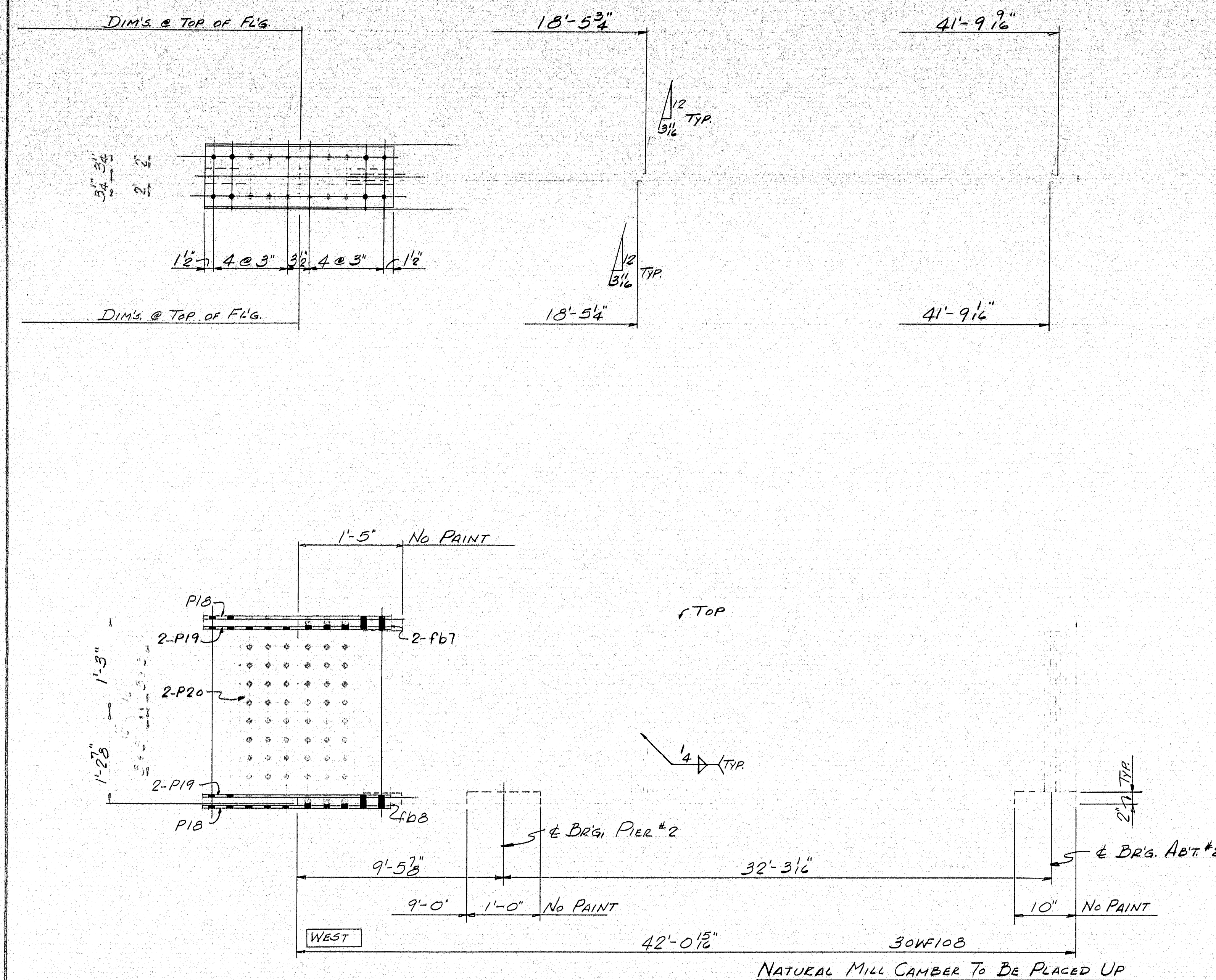
APPD. 8-25-65

PROJ. No. I-95-9 (12)

STRINGERS		NORTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5 S.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER	
3 CUST.	9-10-65	OAKFIELD, MAINE	
5 SHOP	9-10-65		
2 F.A.	8-13-65	CUSTOMER CIANCHETTE BROS., INC.	
DRAWN	8-12-65 R.A.M.	DESIGNER M.S.H.C. BRIDGE DIVISION	
REVISION		ORDER VERBAL	
REVISION		DWG. 865-135-S16	

97-193





ONE - S2K  
ONE - S2M  
ONE - S2N  
ONE - S2P

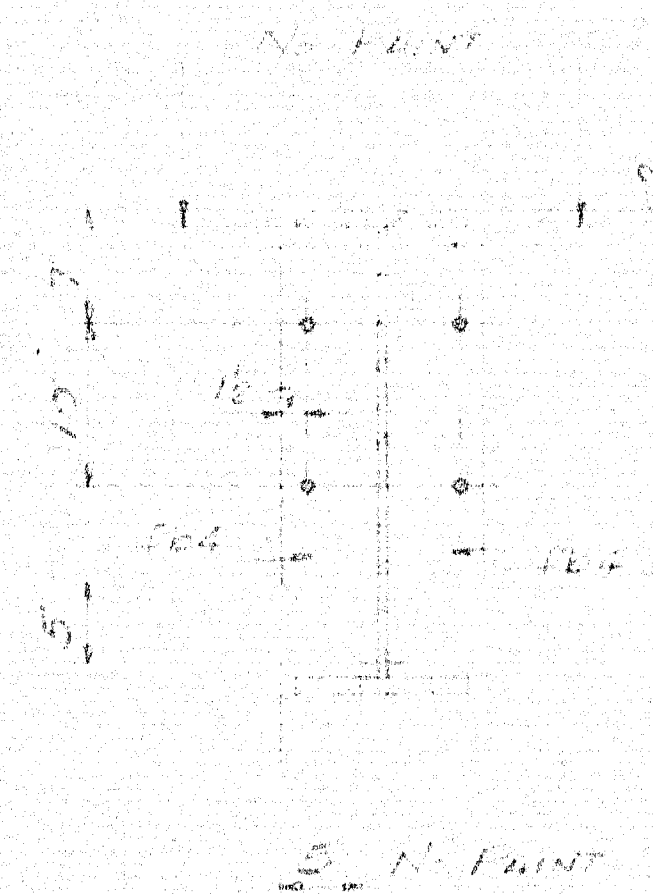
SHIP		BILL OF MATERIAL				DWG. B65-135-517
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S2K	1		30W108	42' 0 1/2"		A36
S2M	1		D <sub>0</sub>	42' 0 1/2"		
S2N	1		D <sub>0</sub>	42' 0 1/2"		
S2P	1		D <sub>0</sub>	42' 0 1/2"		
	16	FD4	DAR-6x3	2	2	
	3	FD7	DAR-4x3	1	3 1/2	
	4	FD8	DAR-10x3	1	3 1/2	
	3	P13	R-10x16	2	6 1/2	
	16	P19	R-4x16	2	6 1/2	
	3	P20	R-13x16	2	0	

SHOP CONNECTIONS: LH-E7018 or LH-E7028  
FIELD CONNECTIONS: 3/4" H.S. BOLT  
HOLES: 1/8" Ø  
PAINT: STATE OF MAINE SPEC'S.

APP'D. 8-25-65		PROJ. No. I-95-9 (12)	
STRINGERS		NORTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5	S.H.C.	9-10-65	I-95 OVER E. BRANCH MATTAWAMKEAG RIVER
3	CUST.	9-10-65	OAKFIELD, MAINE
5	SHOP	9-10-65	
2	FA	8-13-65	
DRAWN	8-12-65	RAM.	CUSTOMER: CIANCHETTE BROS., INC.
REVISION			DESIGNER: M.S.H.C. BRIDGE DIVISION
REVISION			ORDER: VERBAL
REVISION			DWG. B65-135-517

97-194

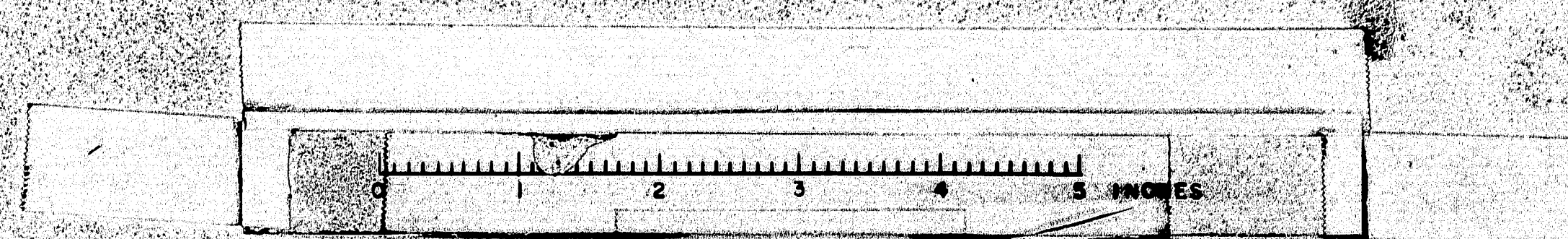




SHOP CONNECTIONS: LH-E701B or LH-E702B  
FIELD CONNECTIONS: 3" H.S. BOLT  
HOLES: 1 5/16"  $\phi$   
PAINT: STATE OF MAINE SPEC'S.

Proj. No. I-95-9 (12)

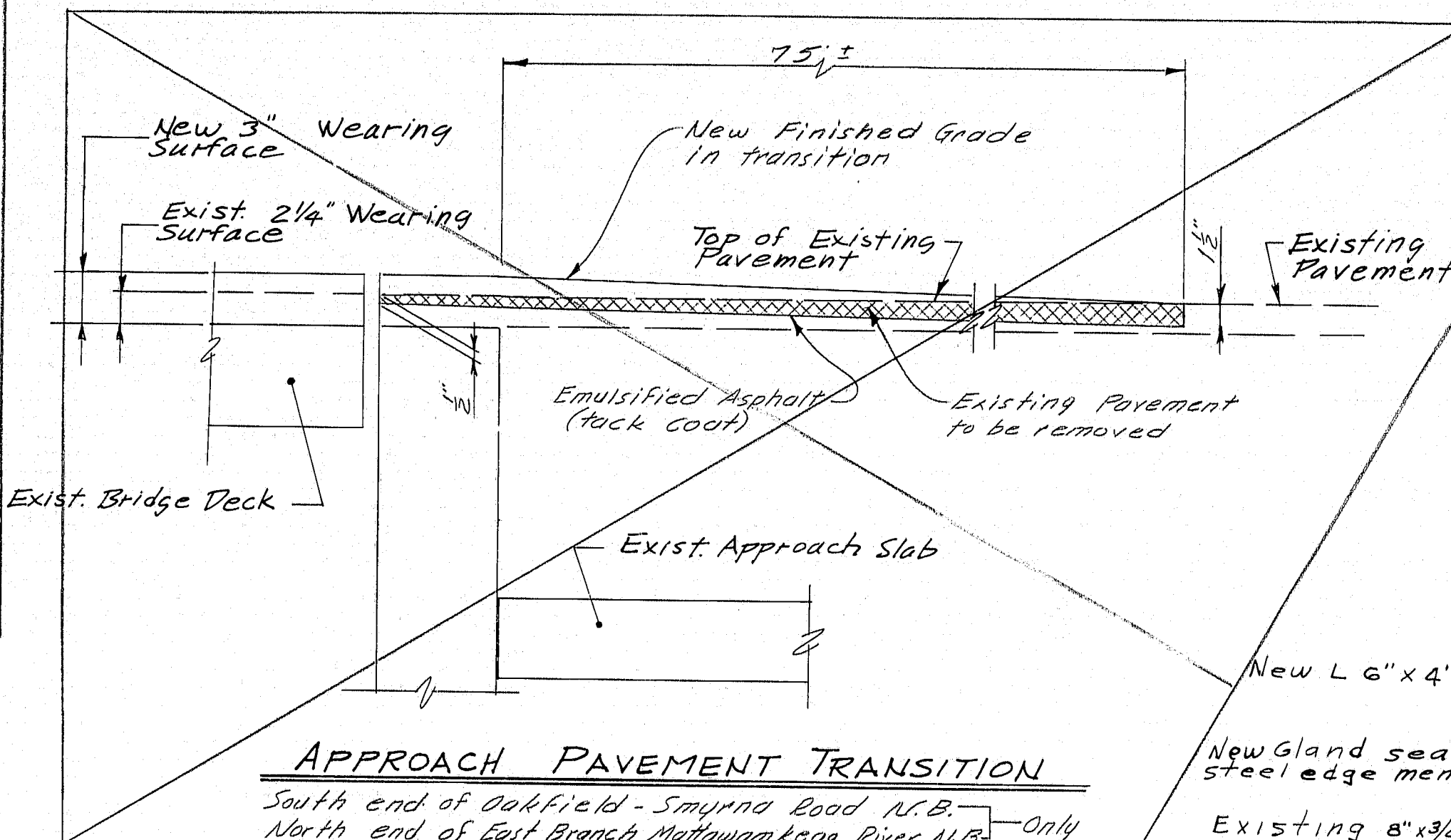
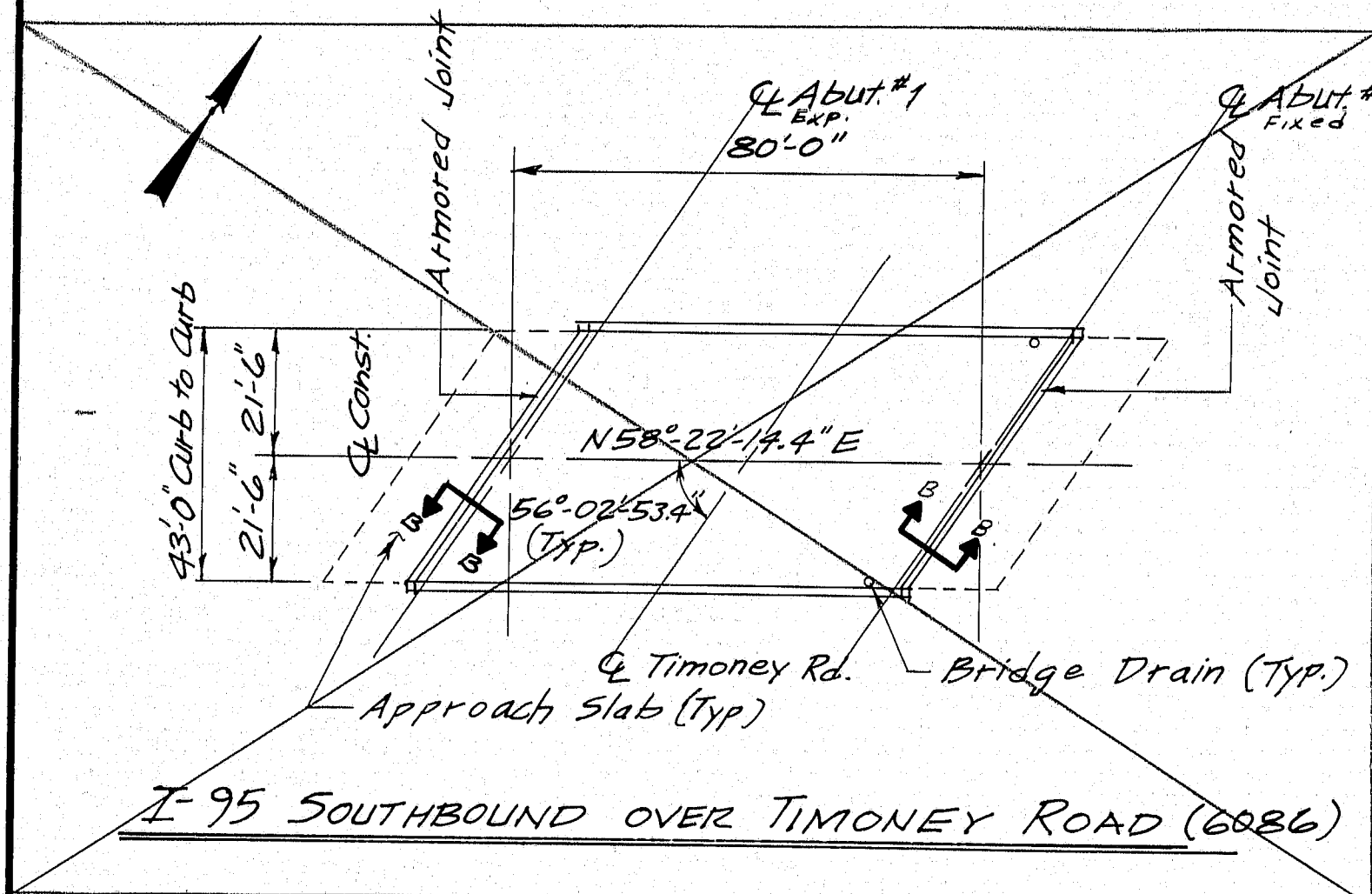
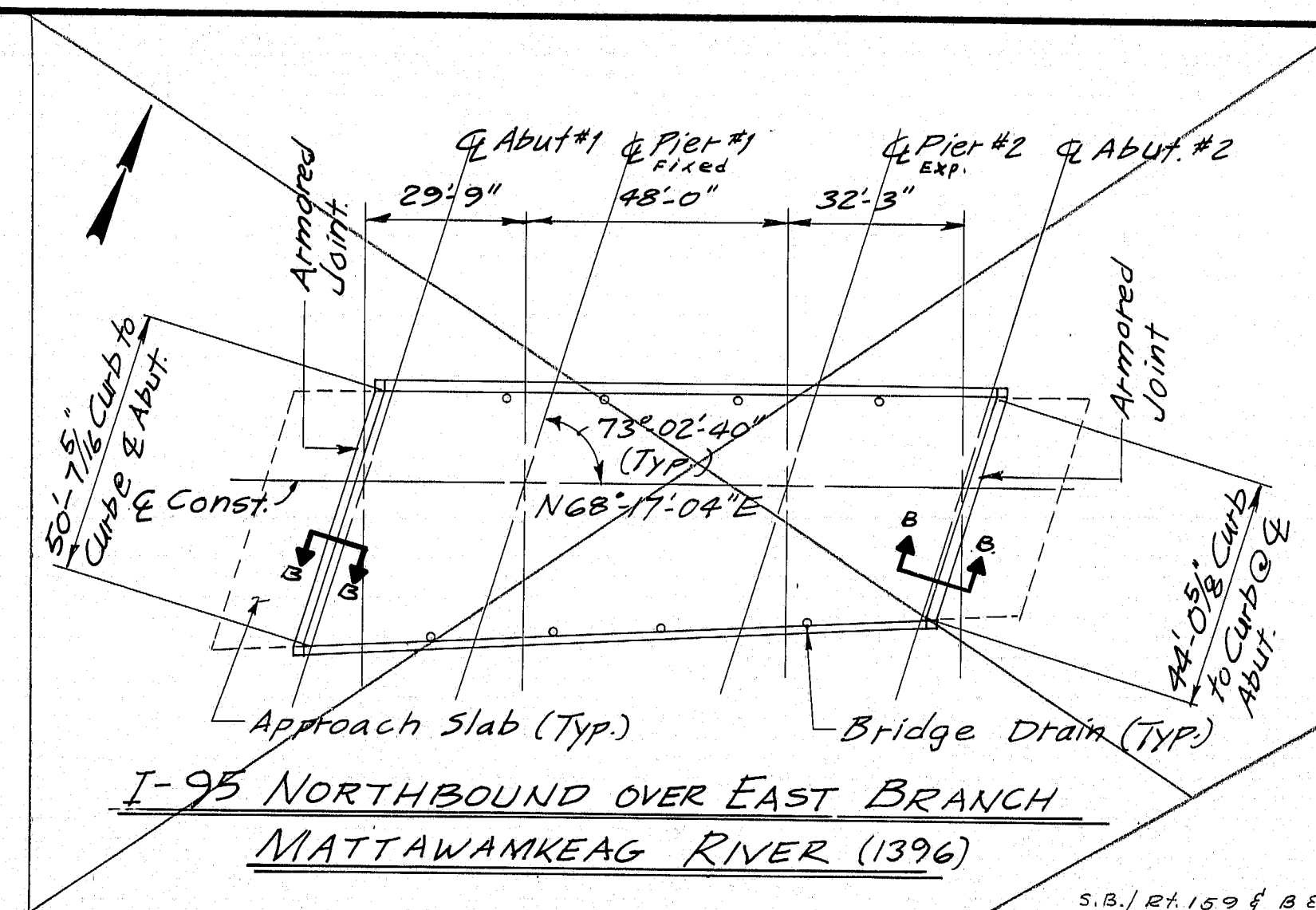
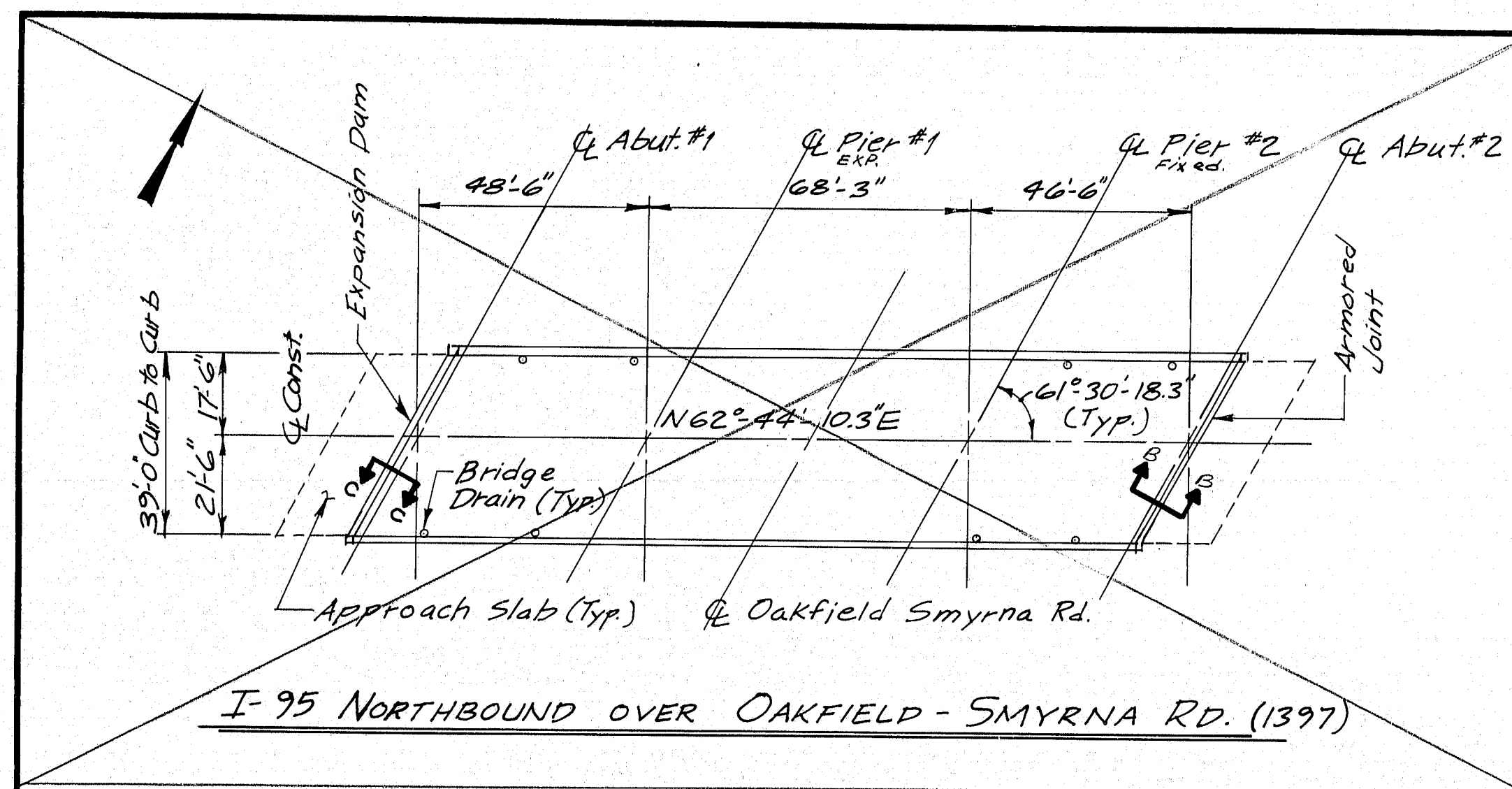
**97-195**



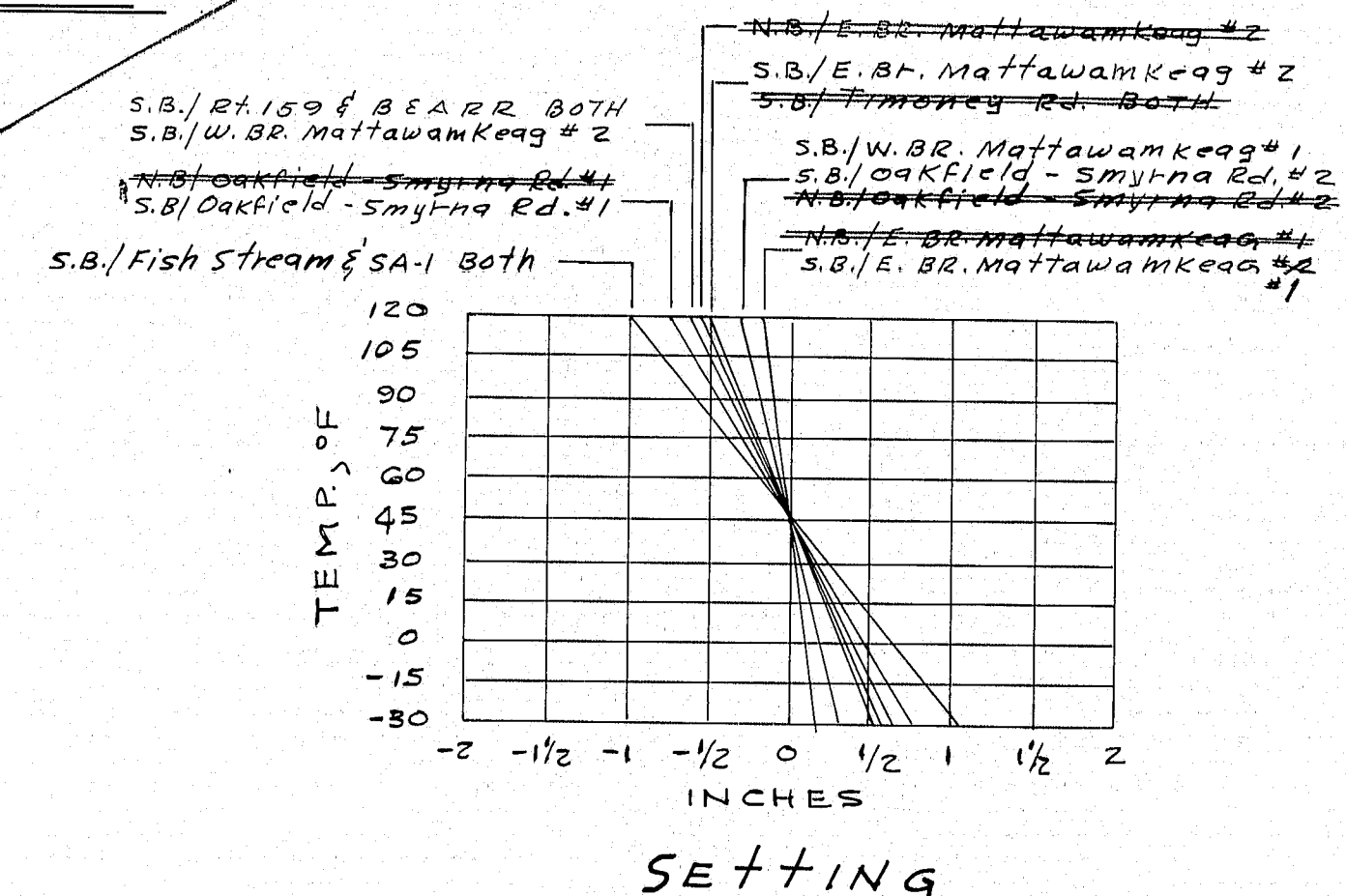






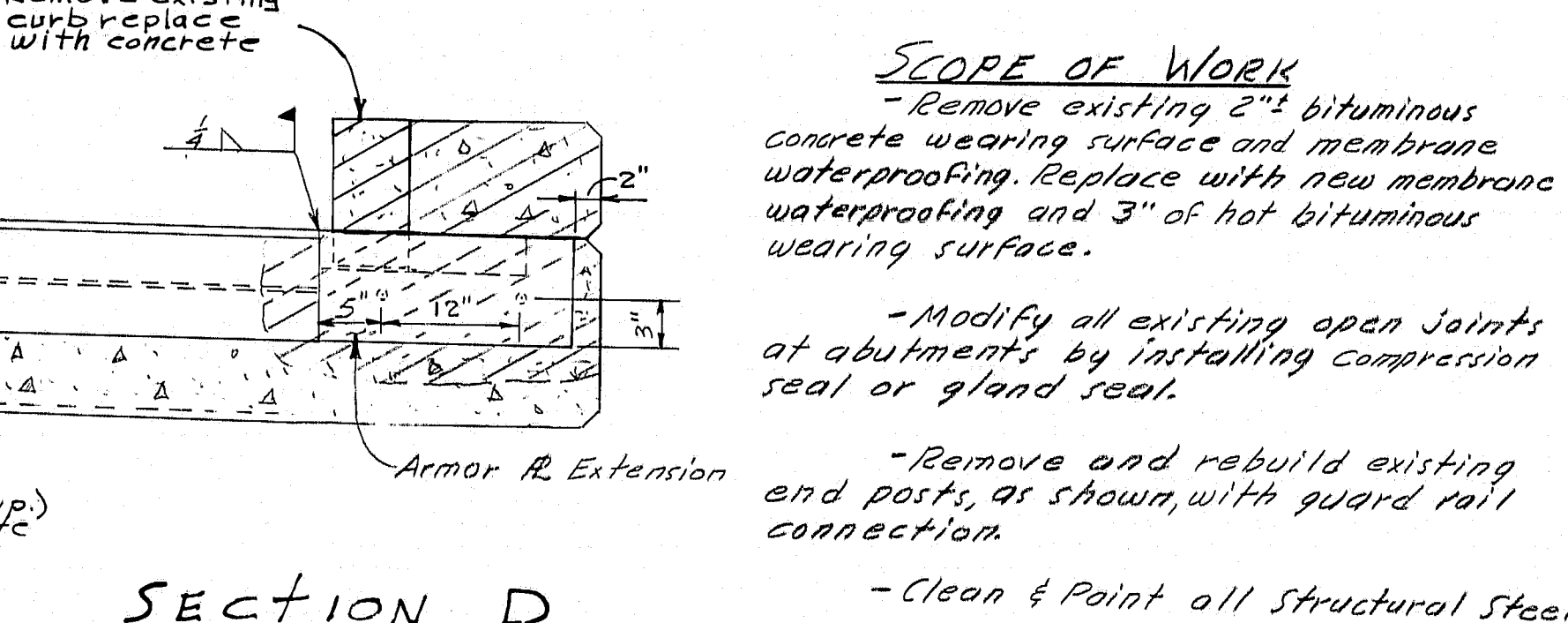
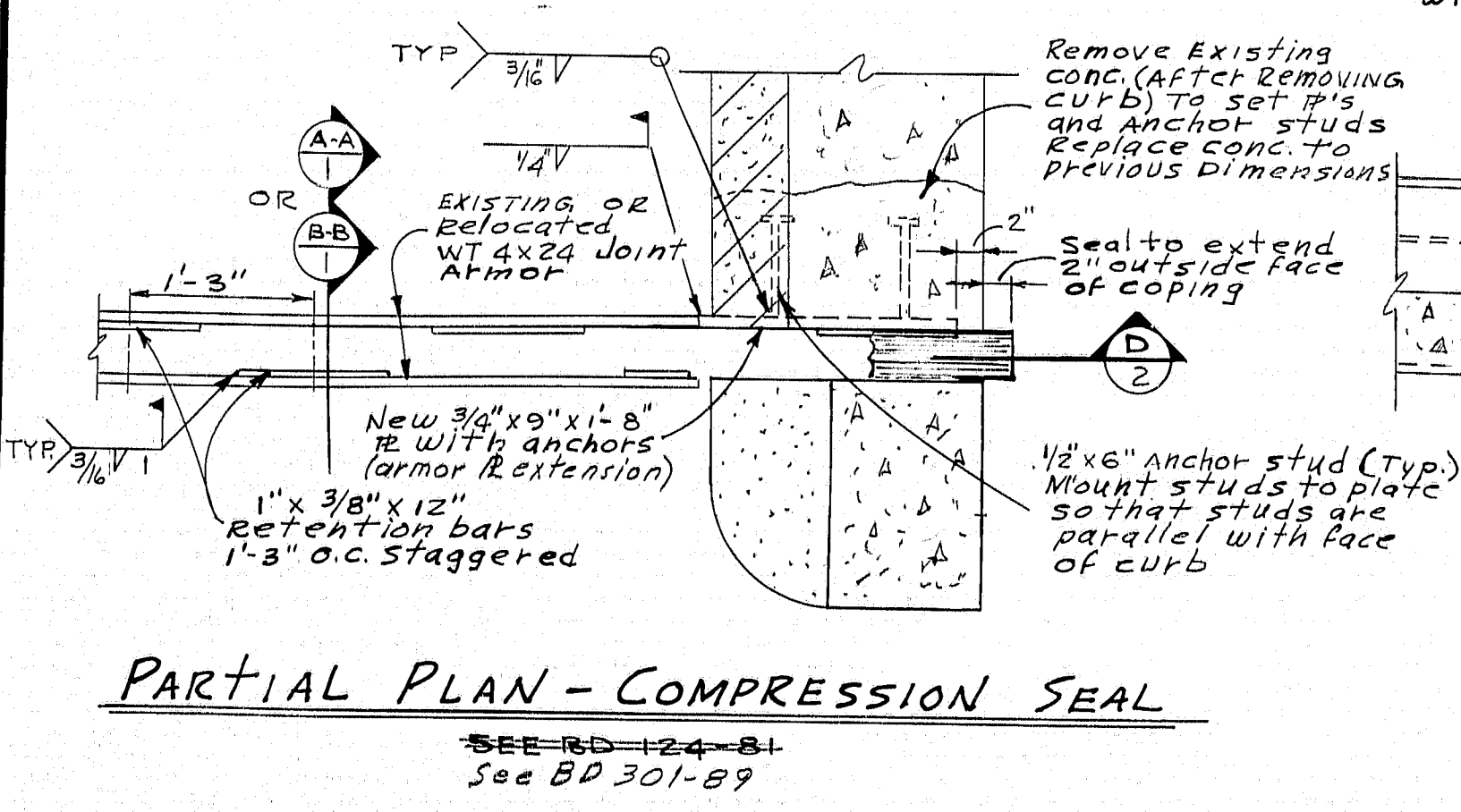


- | FORMA | STATE | PROJECT NUMBER | SHEET NO. | TOTAL SHEETS |
|-------|-------|----------------|-----------|--------------|
| 1     | MAINE | 105-47 (109)   | 27        | 34           |
- SEALS TO BE FURNISHED SHALL HAVE A MOVEMENT RATING OF:
    - 2" - SB/FISH STREAM & STATE AID-1 (BOTH)
    - 1 1/2" - SB/OAKFIELD/SMYRNA ROAD (#1)
    - 1 1/2" - SB/OAKFIELD/SMYRNA ROAD (#2)
    - 1 1/4" - SB/ROUTE 159 AND BARR (BOTH)
    - 1 1/4" - SB/W. BR. MATTAWAMKEAG (#2)
    - 1 1/4" - SB/E. BR. MATTAWAMKEAG (#2)
    - 1" - SB/E. BR. MATTAWAMKEAG (#1)
    - 1" - SB/TIMONEY ROAD (BOTH)
    - 5/8" - SB/W. BR. MATTAWAMKEAG (#1)
    - 5/8" - SB/OAKFIELD/SMYRNA ROAD (#2)
    - 5/8" - SB/OAKFIELD/SMYRNA ROAD (#1)
    - 3/8" - SB/E. BR. MATTAWAMKEAG (#1)
    - 3/8" - SB/E. BR. MATTAWAMKEAG (#2)
  - SEALS SHALL BE APPROVED BY THE ENGINEER PRIOR TO FABRICATION OF THE JOINT ARMOR.
  - COMPRESSION SEAL JOINT OPENINGS WILL VARY DEPENDING ON THE DIMENSIONS OF THE SEAL SELECTED BY THE CONTRACTOR. THE JOINT OPENING SHALL BE SET ACCORDING TO THE OPENING SHOWN ON THE APPROVED SHOP DETAIL DRAWINGS.
  - THE COMPRESSION SEAL ADJUSTMENT CHART SHOWS THE ADJUSTMENT NECESSARY TO ADJUST THE JOINT OPENING SHOWN ON THE SHOP DETAIL DRAWINGS (FOR COMPRESSION SEALS) OR IN THESE DRAWINGS (FOR GLAND SEALS) FOR TEMPERATURES OTHER THAN 45°F. ADJUSTMENT IS TO BE MEASURED PARALLEL TO THE CENTERLINE OF CONSTRUCTION.



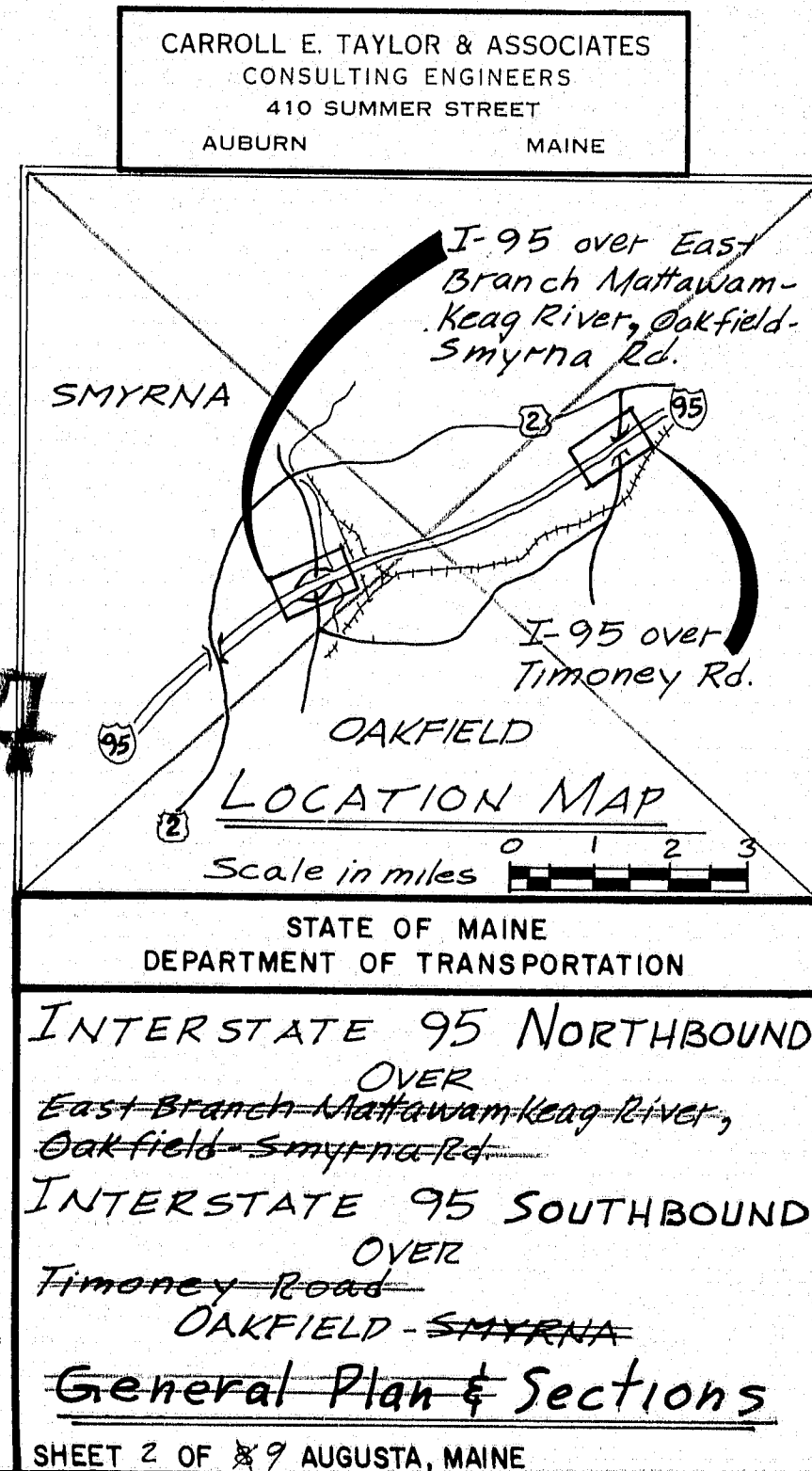
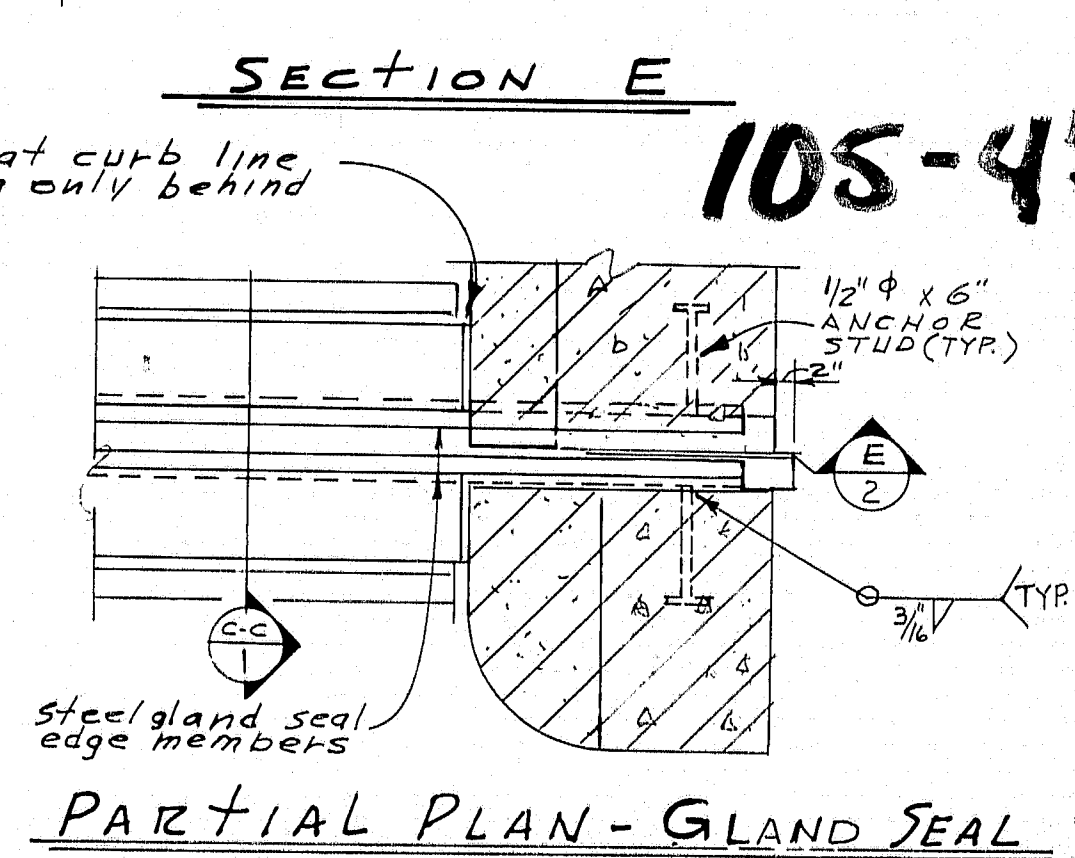
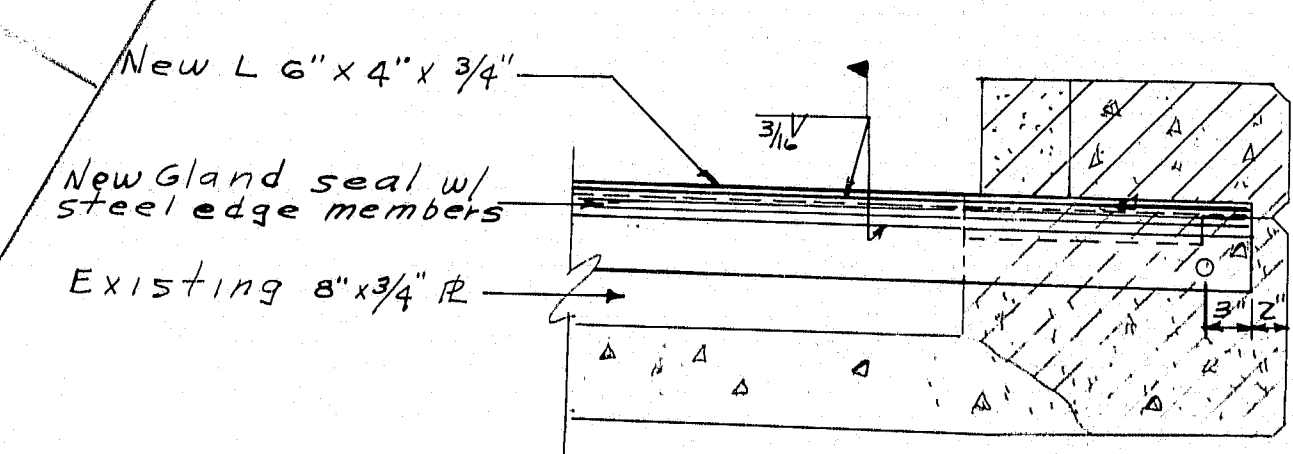
**OPTIONAL TREATMENT FOR GRANITE CURB END PIECE**

THE EXISTING GRANITE CURB END PIECE MAY BE REMOVED AND RESET, IF POSSIBLE, AT THE ADJUSTED LOCATION. EXISTING UNITS ARE SECURED WITH TWO ADG DOWELS GROUTED INTO 1 1/4" DIA. HOLES IN THE GRANITE. THEN SET INTO DRILLED HOLES IN THE CONCRETE BELOW. IF RE-USED, RESET THE DOWELS IN A SIMILAR MANNER. IF NOT RE-USED, PROVIDE NEW GRANITE END PIECES AS SHOWN IN THE DETAILS AT LEFT, AND ANCHOR IN A SIMILAR MANNER.

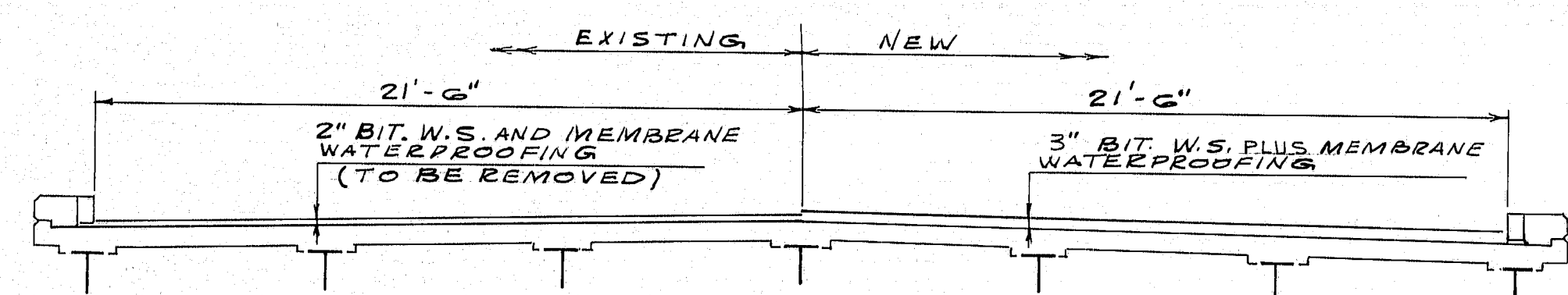


**SCOPE OF WORK**

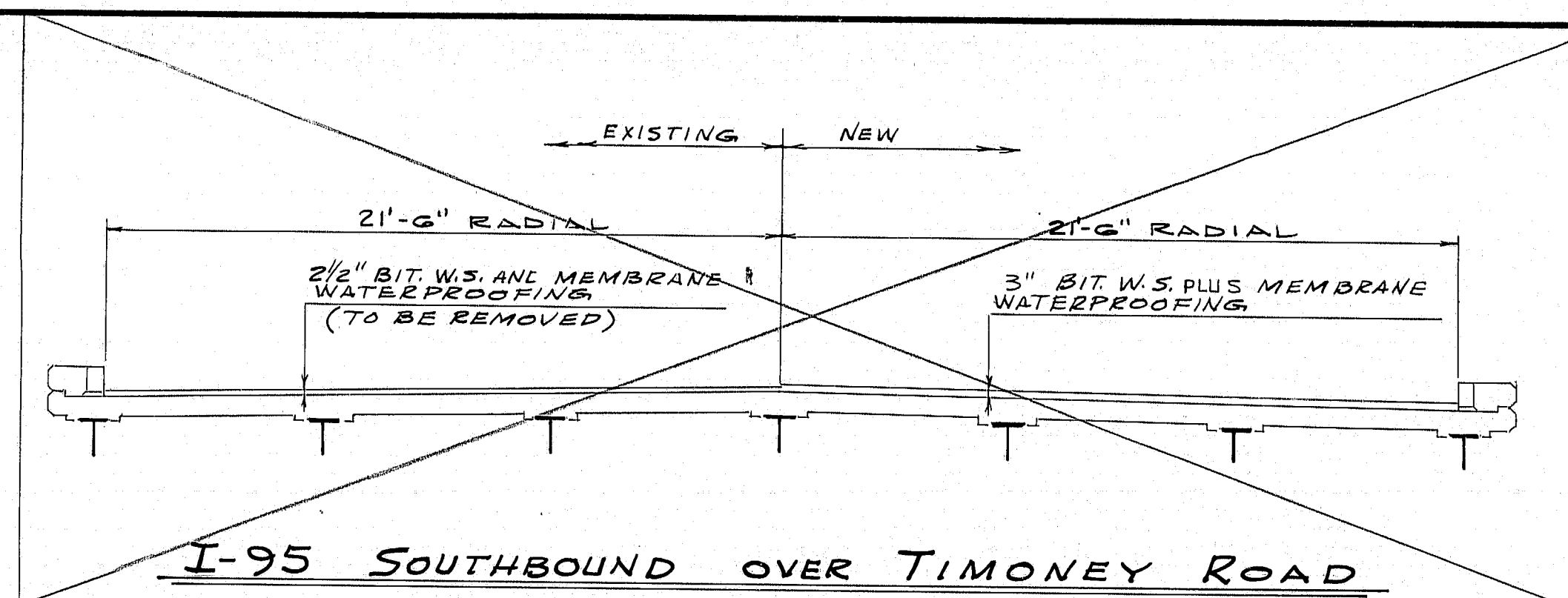
- Remove existing 2" bituminous concrete wearing surface and membrane waterproofing. Replace with new membrane waterproofing and 3" of hot bituminous wearing surface.
- Modify all existing open joints at abutments by installing compression seal or gland seal.
- Remove and rebuild existing end posts, as shown, with guard rail connection.
- Clean & Paint all Structural Steel



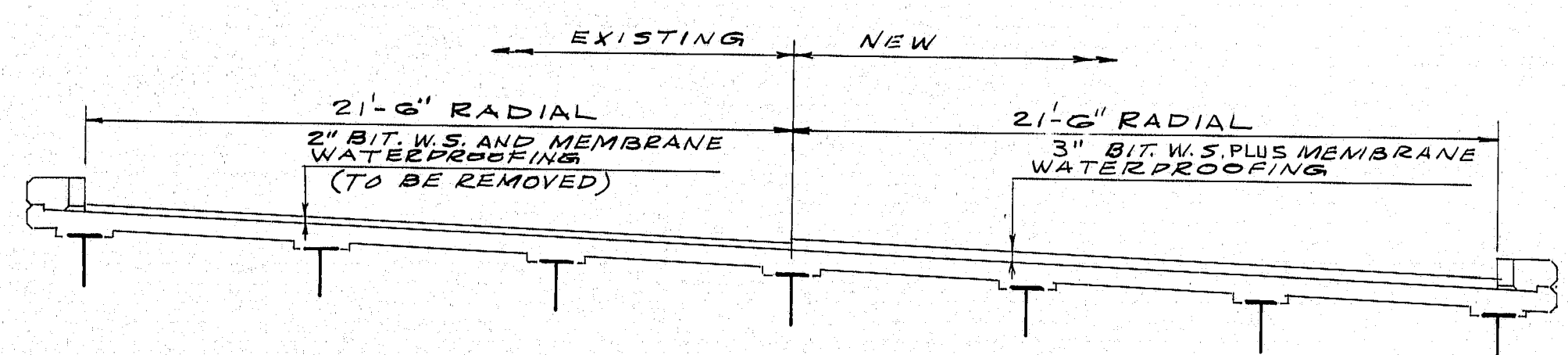




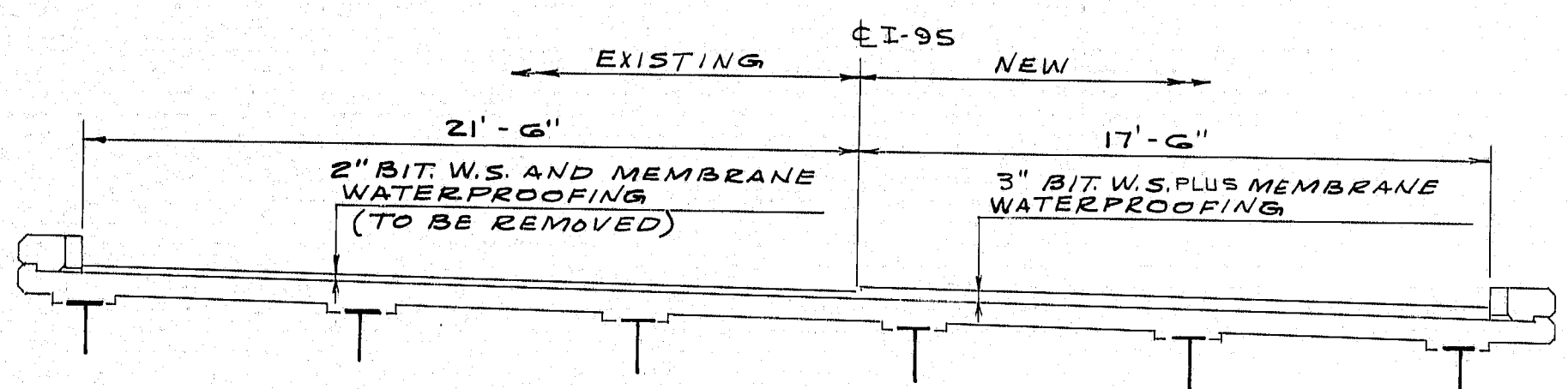
I-95 Southbound over Route 159 and B & ARR  
I-95 Southbound over Fish Stream and State Aid-1



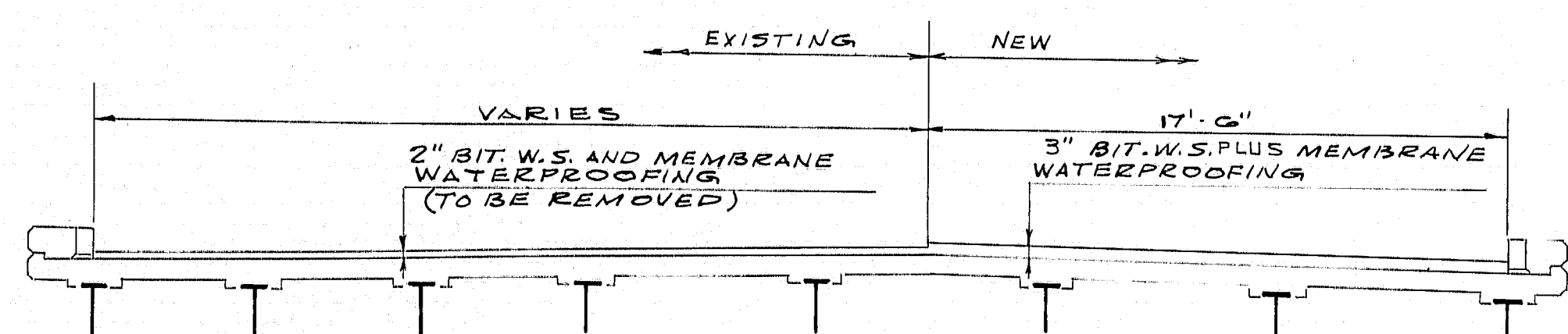
I-95 Southbound over Timoney Road



I-95 Southbound over West Branch Mattawamkeag River



I-95 Southbound over Oakfield - Smyrna Road (Shown)  
I-95 Northbound over Oakfield - Smyrna Road (Crown at E)



I-95 Southbound over East Branch Mattawamkeag River  
I-95 Northbound over East Branch Mattawamkeag River

PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAIL		
REVISIONS		
FIELD CHANGES		
PLANS		

BRUNING 12-12-82 47701

CONSTRUCTION NOTES

1. MAINTAIN ONE 12-FOOT MINIMUM TRAFFIC LANE AT ALL TIMES.
2. ALL WORK SHALL BE DONE BEHIND TEMPORARY CONCRETE BARRIERS.
3. THE TOP SURFACE OF THE EXISTING CONCRETE SLABS SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER.
4. DEPRESS THE BITUMINOUS WEARING SURFACE AROUND THE EXISTING BRIDGE DRAINS AS DIRECTED BY THE ENGINEER.
5. BECAUSE OF STAGED CONSTRUCTION, SOME EXPANSION JOINTS MAY REQUIRE CONSTRUCTION JOINTS. THESE SHALL BE AS APPROVED BY THE ENGINEER AS TO TYPE AND LOCATIONS.
6. ~~PAYMENT FOR DRILLING AND GROUTING ASSOCIATED WITH MODIFICATIONS OF THE JOINTS WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.~~
7. ~~PAYMENT FOR REMOVING AND RE-INSTALLING BRIDGE RAIL OR GUARDRAIL AS NEEDED TO ACCOMPLISH JOINT MODIFICATIONS, WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.~~
8. ~~PAYMENT FOR REMOVING AND REPLACING PORTIONS OF END POSTS WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.~~
9. PAYMENT FOR CUTTING, REMOVING, OR REPLACING GRANITE CURB WHERE CALLED FOR WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
10. REINFORCING STEEL SHALL HAVE A MINIMUM COVER OF 2 INCHES UNLESS NOTED OTHERWISE.
11. WHERE GRANITE CURB IS CALLED FOR TO BE REMOVED TO MODIFY A JOINT OR TO INSTALL A SEAL, ONLY THE SMALLEST AMOUNT NECESSARY TO ACCOMPLISH THE WORK SHALL BE REMOVED. IT IS PREFERABLE TO REMOVE CURB TO AN EXISTING JOINT, BUT IF A JOINT IS MORE THAN 3 FEET AWAY THE CURB SHALL BE SAW CUT IN THE FIELD. THE DECISION ABOUT WHETHER TO CUT OR NOT, AND WHERE, SHALL BE THE ENGINEER'S.
12. ~~THE REMOVAL OF THE EXISTING BITUMINOUS PAVEMENT ON THE APPROACHES SHALL BE INCIDENTAL TO ITEM 202-127.~~
13. The following labor and material shall be paid for under Item 606.173 Bridge Connection.
  - Removal of existing concrete end posts.
  - Removing and re-installing existing bridge rail posts.
  - Cutting existing bridge rail.
  - Concrete for new end posts.
  - All hardware for approach rail anchor.
  - W-Beam Terminal Connector (EE-S-19).
  - Remove, reset and add new guard rail posts.
  - Additional beam rail panels, with extra holes.
  - Additional holes in existing beam rail panels.
  - Protective Coating for Concrete.

14. Additional holes required in guard rail panels 2" x 8" may be made by drilling, punching, or any other method that produces a neat clean hole of the required size. Burning of holes will not be allowed.

105-48

For notes see sheet #5

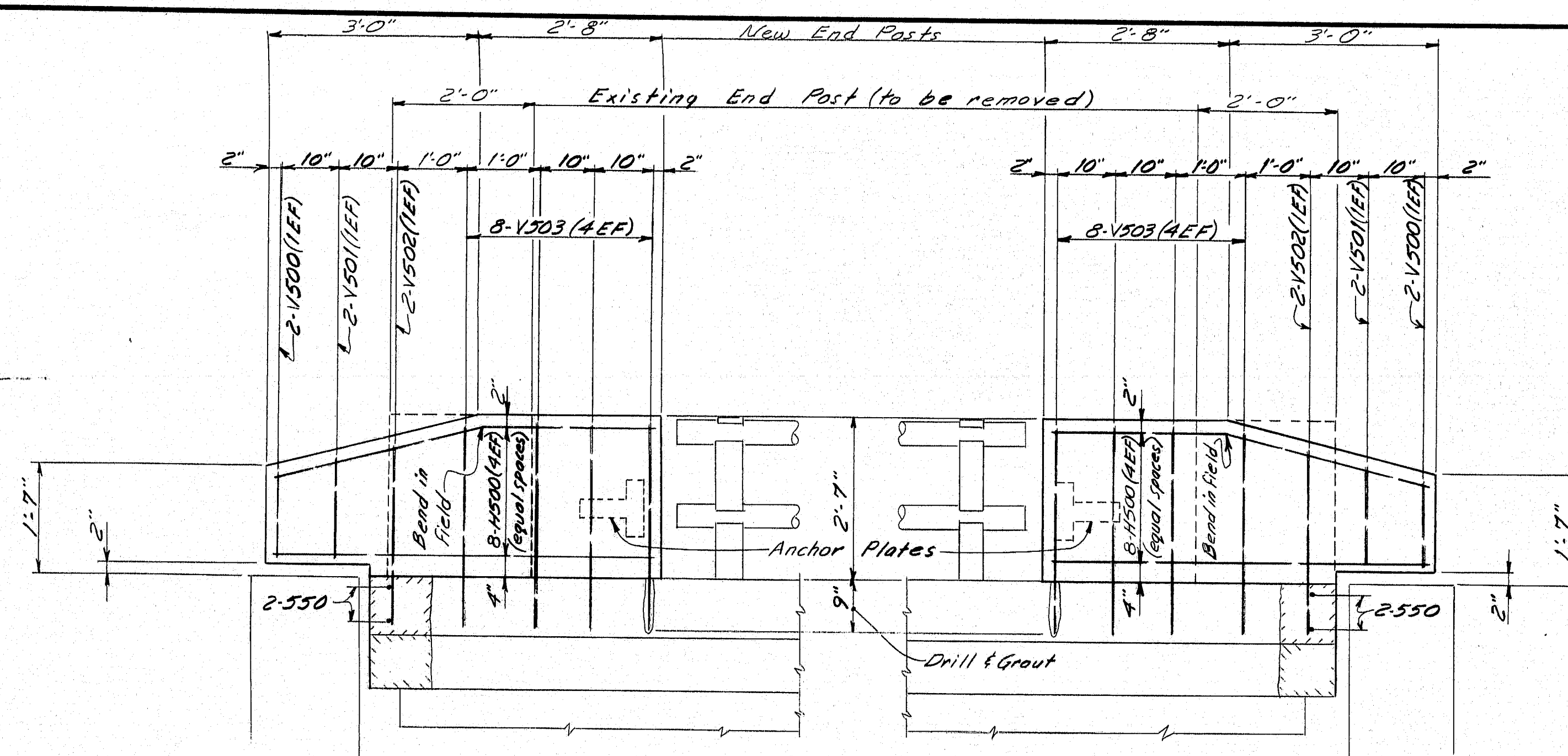
CARROLL E. TAYLOR & ASSOCIATES  
 CONSULTING ENGINEERS  
 410 SUMMER STREET  
 AUBURN MAINE

STATE OF MAINE  
 DEPARTMENT OF TRANSPORTATION

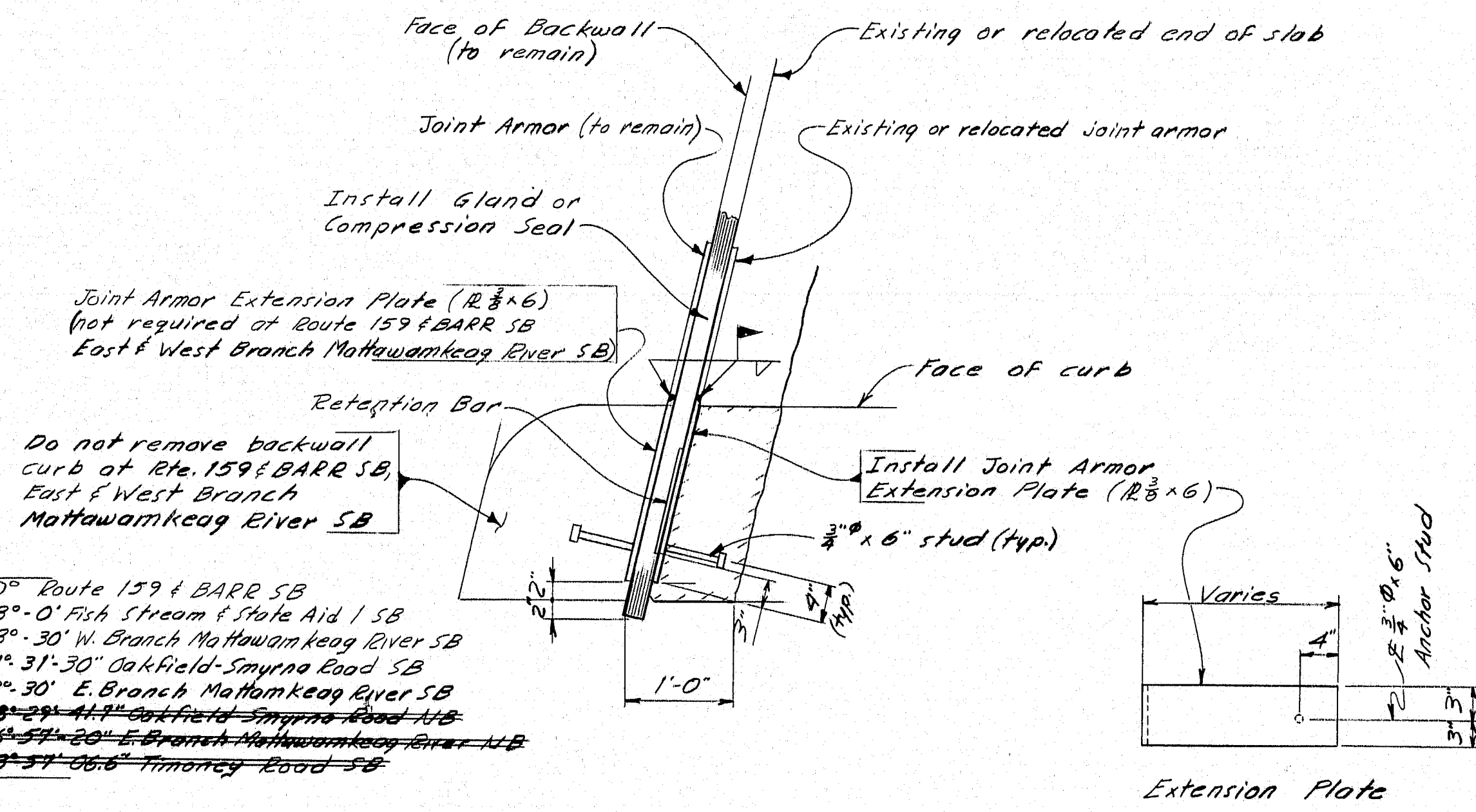
HALF SECTIONS & NOTES



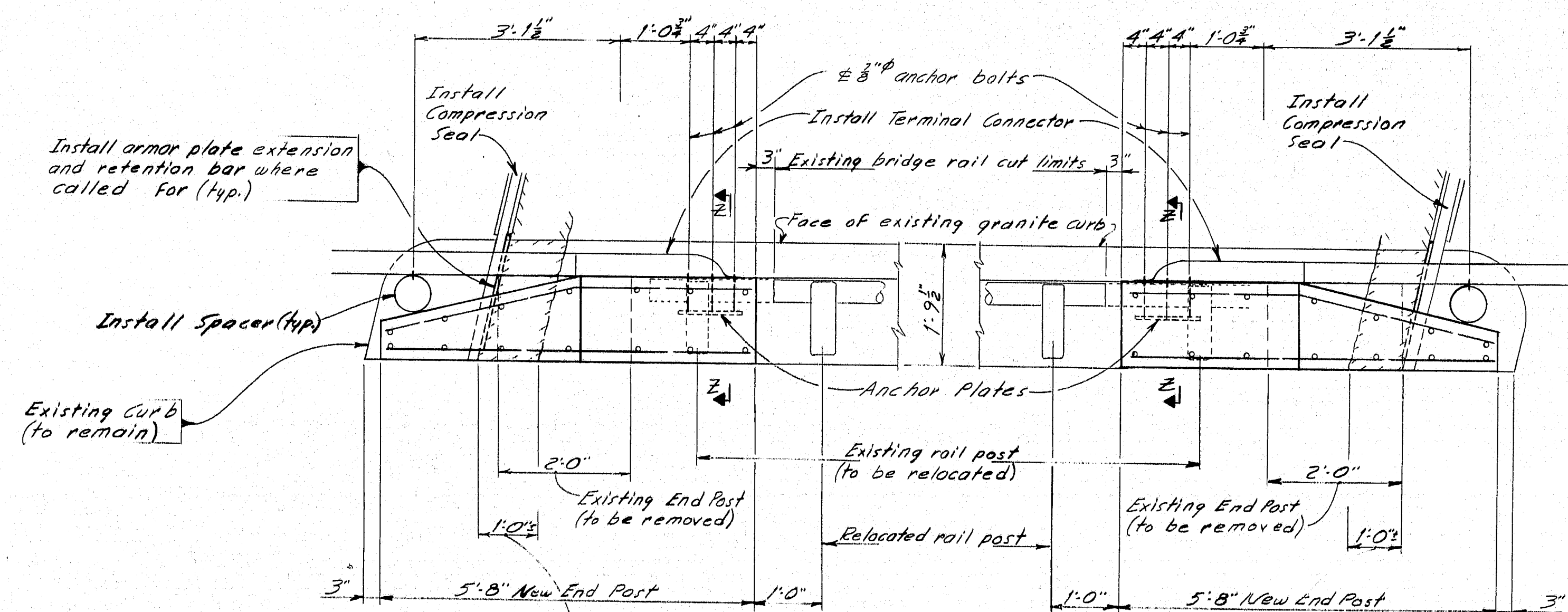
F.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	IP-95-9(109)	29	34



**ELEVATION**  
View shown: Route 159 and BARR SB - East Side, West side opposite hand  
W. Branch Mattawamkeag River SB - South side, North side opposite hand  
E. Branch Mattawamkeag River SB



**JOINT ARMOR EXTENSION PLATE DETAIL**



At Abut #1 W. Branch Mattawamkeag River SB and Abut #1 E. Branch Mattawamkeag River SB existing concrete and granite curb need not be removed to install compression seal if so directed.  
Remove portions of existing concrete and granite curb, concrete slab to limits shown. Cut back existing reinforcing steel only to the extent required to make modifications. Place additional reinforcing steel as shown. Install armor plate extension and compression seal. Rebuild concrete curb and slab to accommodate new seal. Abut #1 & #2 Rte. 159 & BARR SB  
Abut #2 W. Branch Mattawamkeag River SB  
Abut #2 E. Branch Mattawamkeag River SB

**PLAN**  
East Branch Mattawamkeag River shown, others similar

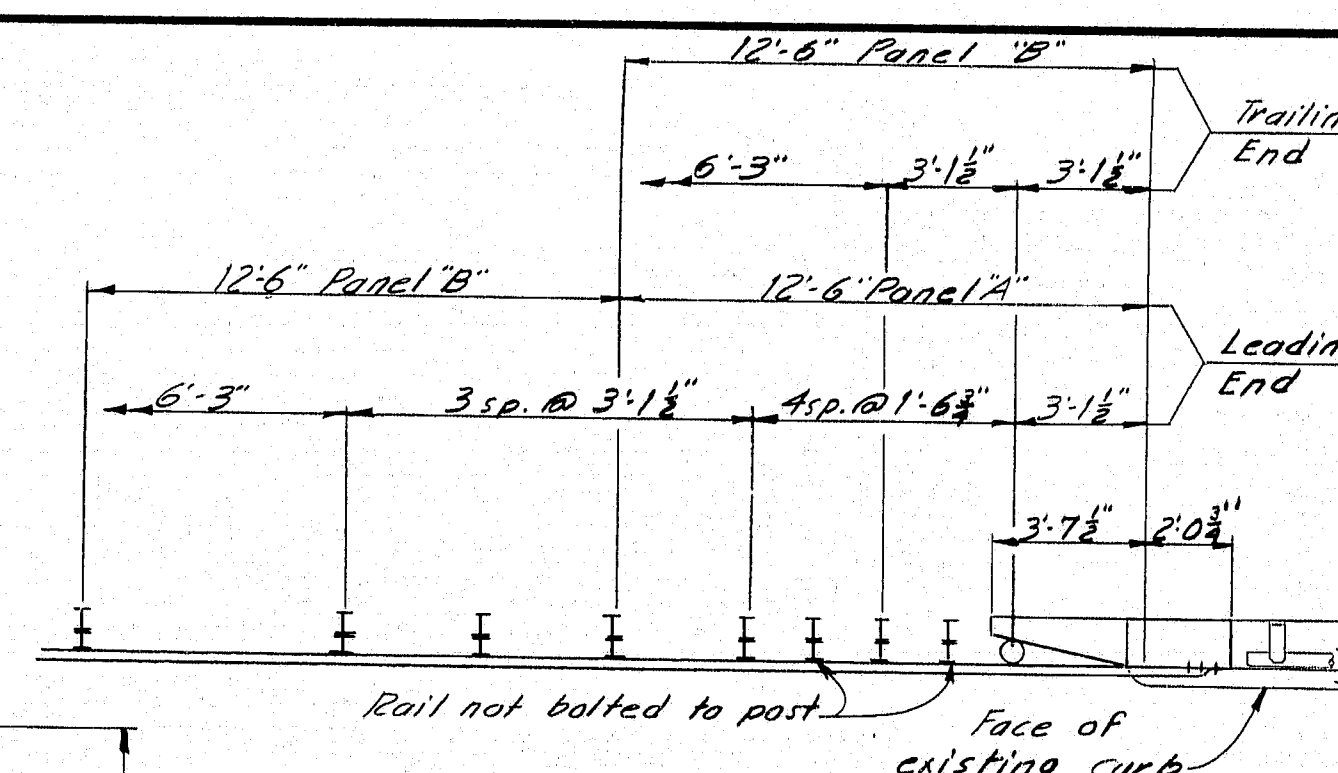
Work this sheet with:  
BD 201-89 Concrete End Post  
BD 301-89 Compression Seal  
BD 302-89 Gland Seal  
For notes see sheets #3 and #5

For Notes  
For Section Z-Z  
For Anchor Plate Detail  
For Anchor Bolt Detail  
For Spacer Detail  
For Guard Rail Layout

STATE OF MAINE DEPARTMENT OF TRANSPORTATION
<b>JOINT MODIFICATION</b>
<b>END POST &amp; RAIL ANCHOR DETAIL</b>
<b>INTERSTATE 95 OVER:</b>
Route 159 and B&A RR SB
W. Branch Mattawamkeag River SB
E. Branch Mattawamkeag River SB
SHEET 4 OF 9 AUGUSTA, MAINE

105-49



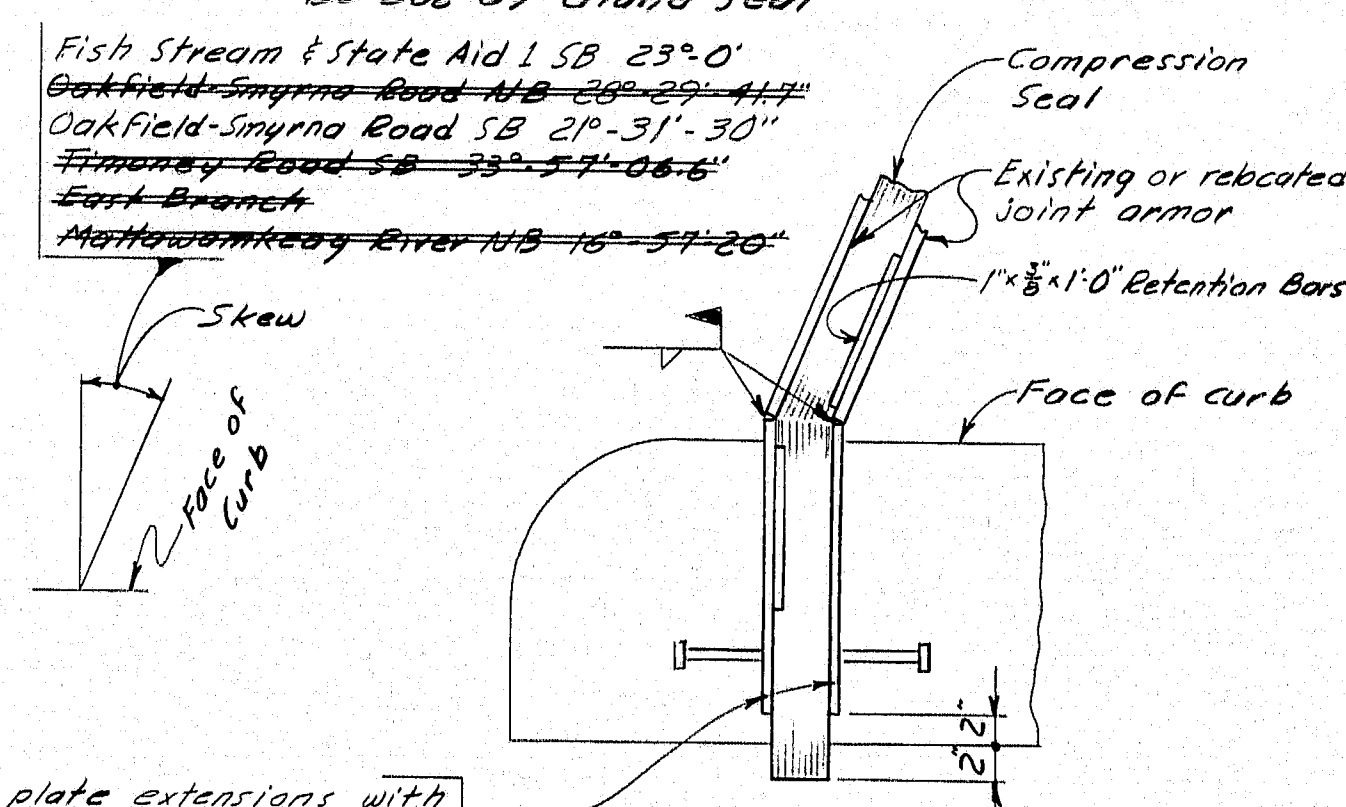
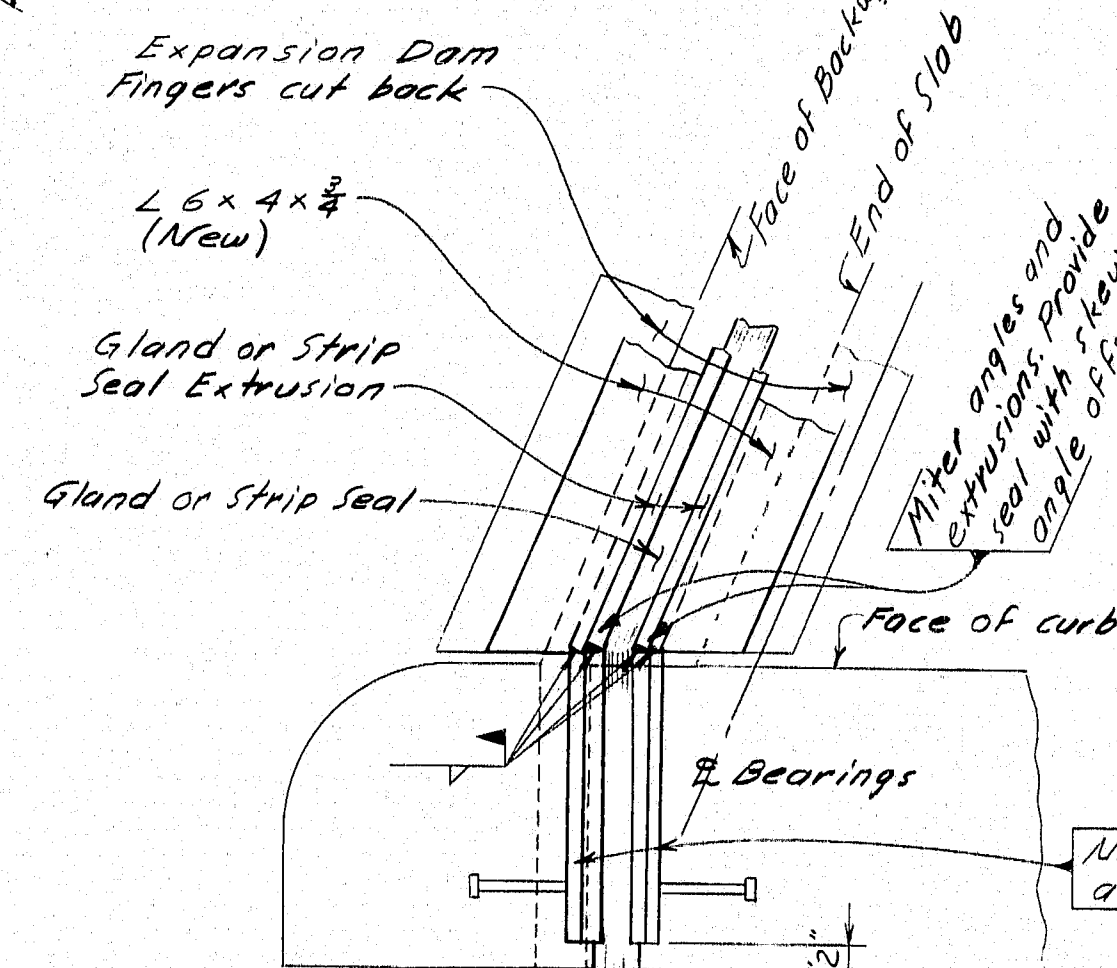


NOTES

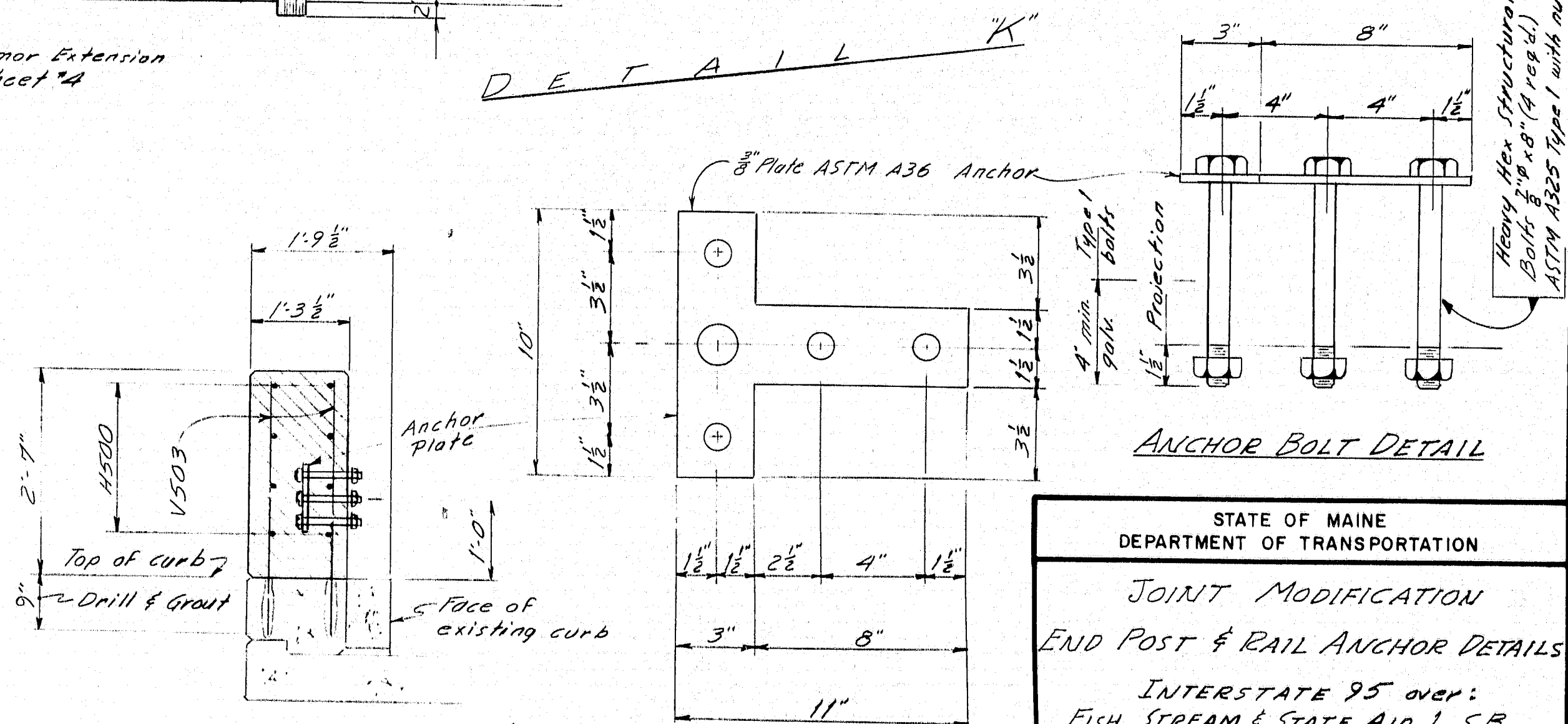
- NOTES**
- |   |       |            |    |    |
|---|-------|------------|----|----|
| 1 | MAINE | IR-95-9109 | 30 | 34 |
|---|-------|------------|----|----|
1. Reinforcing steel shall have a minimum cover of 2 inches unless otherwise indicated.
  2. Removing of existing reinforcing steel shall be at a minimum.
  3. Existing reinforcing steel to remain shall be cleaned as directed prior to placing new concrete.
  4. Mortar for grouting dowels shall contain an approved non-shrink additive. Payment for drilling and grouting of dowels shall be considered incidental to Item 503.13, Reinforcing Steel, Placing.
  5. ~~Additional holes required in guard rail post #1's may be made by drilling, punching, or any other method that produces a neat, clean hole of the required size. Burning of holes will not be allowed.~~
  6. After installation of guard rail is complete, upset the thread on the anchor bolts in three places around each bolt at the junction of the nut and the exposed thread, with a center punch or similar tool.

7. Work this sheet with:  
BD 201-89 Concrete End Post  
BD 301-89 Compression Seal  
BD 302-89 Gland Seal

~~Fish Stream & State Aid 1 SB 23° 0'~~  
~~Oakfield-Smyrna Road NB 28° 29' 41"~~  
~~Oakfield-Smyrna Road SB 21° 31' 30"~~  
~~Finney Road SB 33° 57' 06"~~  
~~Fort Branch~~  
~~Hallowmoke River NB 16° 54' 20"~~



DETAIL "K"



### ANCHOR BOLT DETAIL

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

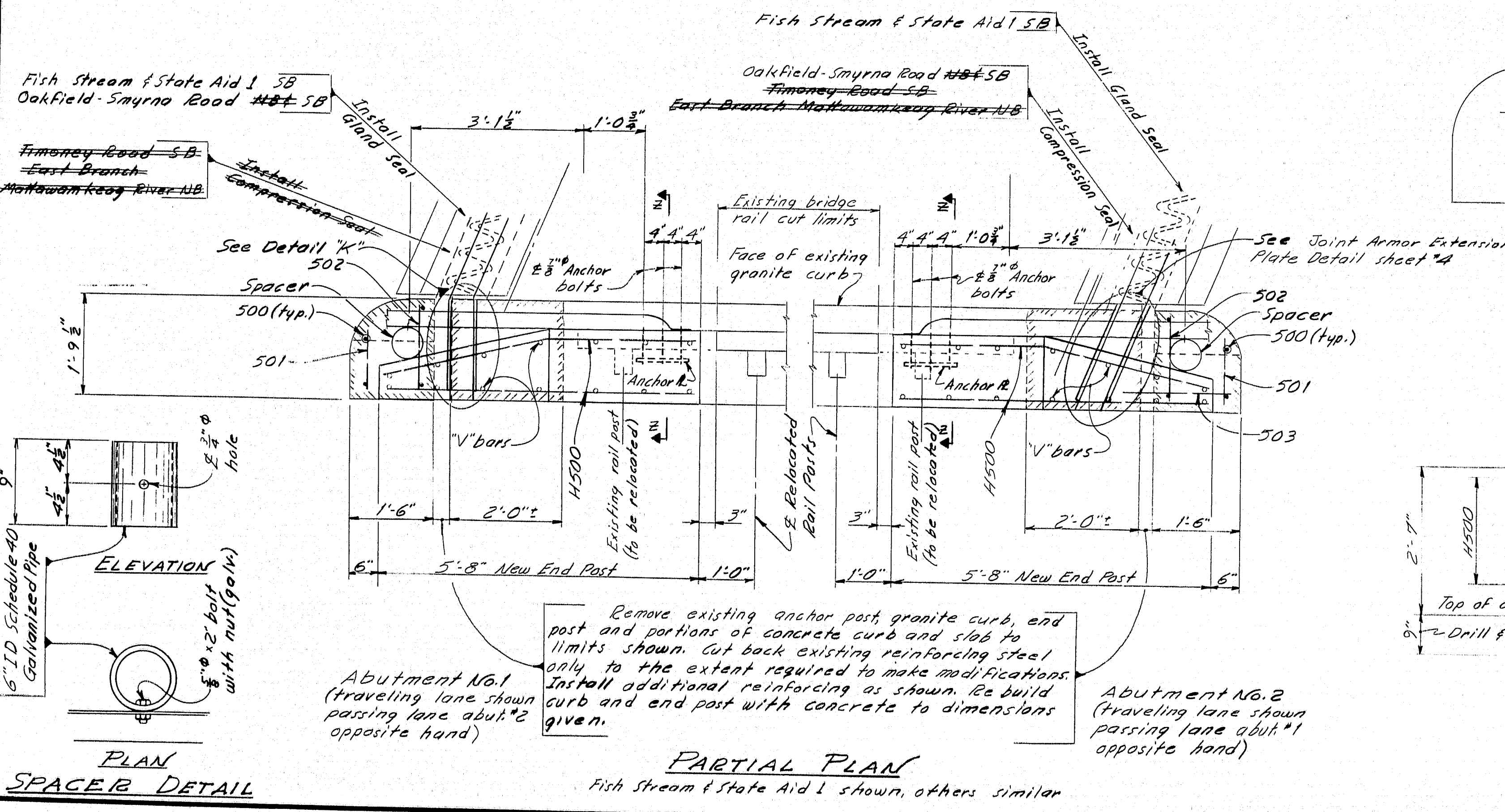
### JOINT MODIFICATION

END POST & RAIL ANCHOR DETAILS

INTERSTATE 95 over:  
FISH STREAM & STATE AID 1 SB  
OAKFIELD-SMYRNA ROAD ~~NB & SB~~  
~~TIMONEY ROAD SB~~  
EAST BRANCH MATTAHANNAKEG RIVER NB

SHEET 5 OF 8 AUGUSTA, MAINE

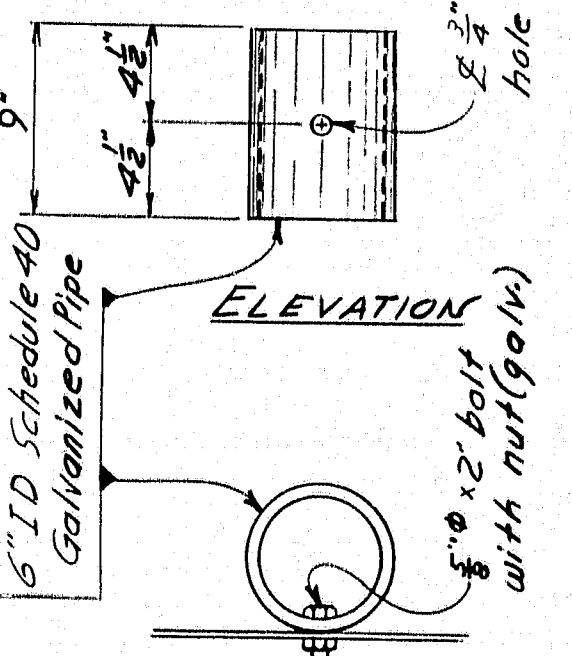
1-95-9(109) ISLAND FALLS - OAKFIELD



### PARTIAL PLAN

Fish Stream & State Aid 1 shown, others similar

PLAN  
SPACER DETAIL



PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	RTA	5-98
CHECKED		
REVISED		
FIELD CHANGES		

BRUNING 44-132 45710-1



PLANS  
DESIGN - DETAIL  
REVISIONS  
FIELD CHANGES

REINFORCING STEEL SCHEDULE																											
STRAIGHT BARS														BENT BARS													
MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	O	R	LOCATION	
E. BRANCH MATTAWAMKEAG RIVER SB				FISH STREAM & STATE AID - 1 SB				<del>OAKFIELD - SMYRNA ROAD N/B</del>																			
550	4	1'-5"	Curb	500	16	2'-2"	Dowels	500	16	2'-2"	Dowel																
				501	8	1'-2"	Curb	501	8	1'-2"	Curb																
V500	8	1'-2"	End Post	502	8	1'-5"	Curb	502	8	1'-5"	Curb																
V501	8	1'-5"		503	4	1'-11"	Curb	503	4	1'-11"	Curb																
V502	8	1'-8"	(dowel)																								
V503	32	3'-2"	End Post (dowel)	V500	8	1'-2"	End Post	V500	8	1'-2"	End Post																
				V501	8	1'-5"		V501	8	1'-5"																	
H500	32	5'-4"	End Post	V502	8	1'-8"	(dowel)	V502	8	1'-8"	(dowel)																
				V503	32	3'-2"	End Post (dowel)	V503	32	3'-2"	End Post (dowel)																
				H500	32	5'-4"	End Post	H500	32	5'-4"	End Post																
															</												

FWHA REV. NO.	STATE MAINE	PROJECT NUMBER 105-9(109)	SHEET NO. 21	TOTAL SHEETS 34
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**TYPE-BENDING DIAGRAMS**

All dimensions are out to out of reinf. bar  
Bending details and hooks shall conform to the recommendations of the current revision of ACI Standard 318.Δ  
Reinforcing Bar: ASTM A615 Grade 60

**GENERAL NOTES**

- First digit(s) following the letter of the Mark indicates size of reinf. bar.  
Mark (A502) bar size - #5  
Mark (P1001) bar size - #10  
Mark (S603) bar size - #6
- Each truss bar, Type B, may be replaced by two (2) straight bars (one top & one bottom) of the same bar size as the truss bar. Payment in either case shall be based on truss bars as scheduled on plans.

Δ New Bent Bar Type SJ  
Δ Revised ACI Standard

REVISIONS	DATE
STATE OF MAINE DEPARTMENT OF TRANSPORTATION	9-28-83 5-12-83

**REINFORCING STEEL SCHEDULE**  
INTERSTATE 95 OVER  
E. Branch Mattawamkeag River SB  
Oakfield - Smyrna Road SB  
W. Branch Mattawamkeag River SB  
Fish Stream & State Aid 1 SB  
Route 159 & BARR SB  
E. Branch Mattawamkeag River NB  
Oakfield - Smyrna Road NB  
Timoney Road SB

SHEET 6 OF 8 AUGUSTA, MAINE

105-51